



CITY OF MANHATTAN BEACH
DEPARTMENT OF COMMUNITY DEVELOPMENT
STAFF REPORT

DATE: October 09, 2024

TO: Planning Commission

FROM: Ryan Heise, Acting Community Development Director

THROUGH: Adam Finestone, AICP, Planning Manager
Ted Faturos, Senior Planner

BY: Johnathon Masi, Associate Planner

SUBJECT: Consideration of a Master Use Permit Amendment to modify existing conditions of approval concerning site access for Manhattan Village Shopping Center, and associated environmental determination finding the Project was adequately covered in the previously adopted Final Environmental Impact Report (SCH No. 2004061146; Resolution No. 14-0025) (Mark Cermak on behalf of RREEF AMERICA REIT II Corporation BBB)

RECOMMENDATION

Staff recommends that the Planning Commission: (1) conduct a public hearing; and (2) adopt the attached resolution approving a Master Use Permit Amendment, subject to conditions, and find the project consistent with a previously-certified Final Environmental Impact Report pursuant to the California Environmental Quality Act.

APPLICANT

Mark Cermak (on behalf of RREEF AMERICA REIT II Corporation BBB)
1200 Rosecrans Avenue, Unit 201
Manhattan Beach, CA 90266

BACKGROUND

On February 19, 2024, the Community Development Department received an application requesting a Master Use Permit Amendment to modify existing conditions of approval concerning site access for Manhattan Village Shopping Center located at 3200 N. Sepulveda Boulevard.

Prior Actions

On December 2, 2014, the City Council adopted Resolution No. 14-0025, certifying a Final Environmental Impact Report (EIR), and Resolution No. 14-0026, approving a Master Use Permit, Height Variance, and Sign Exception/Program, to allow for the remodeling and expansion of a portion of the Manhattan Village Shopping Center (MVSC). Resolution No. 14-0026 included conditions, added by the City Council at the hearing, requiring right-of-way dedication and construction of a right-turn pocket/deceleration lane from northbound Sepulveda Boulevard to eastbound 33rd Street.

On December 6, 2016, the City Council adopted Resolution No. 16-0081, approving Height Variances, an expansion of an existing clerestory and modification of the existing west mall entrances that already exceeded the maximum height, and increasing the height of the previously approved parapet walls from 22 feet to 28 feet.

On December 20, 2016, the City Council adopted the first addendum to the EIR and a modified site plan from the 2014 approval, consistent with a Director's determination that a Master Use Permit was not required, and a finding from an environmental consultant that the addendum was sufficient, and no further environmental documents or review were required.

On September 5, 2017, the City Council adopted Resolution No. 17-0119, adopting the second addendum to the previously approved EIR and further amending the Master Use Permit to refine certain conditions of approval imposed on the permit for the remodel and expansion of the MVSC.

On May 22, 2019, the Planning Commission adopted Resolution No. PC 19-06, approving a Use Permit to allow the construction of a new commercial building for a restaurant with on-sale service of distilled spirits, beer, and wine, located at 3110 N. Sepulveda Boulevard (currently BOA and Sushi Roku).

On October 9, 2019, the Planning Commission adopted Resolution No. PC 19-13, approving a Master Use Permit Amendment to modify certain conditions of approval previously imposed, and adopting a third addendum to the EIR.

On April 29, 2020, the Planning Commission adopted Resolution No. PC 20-05, approving a Sign Exception to allow off-premises and digital signage as part of a Sign Program.

Site Overview

The Project is located on a property which is currently occupied by The Manhattan Village Shopping Center (MVSC), an approximately 44-acre shopping center on the east side of Sepulveda Boulevard between Marine Avenue and Rosecrans Avenue. MVSC opened in the early 1980s and originally included an enclosed main mall building, several freestanding buildings, and two private streets that run parallel with Sepulveda Boulevard (Cedar Way and Carrlotta Way) to allow for traffic circulation.

The MVSC remodel project is largely complete, with all portions of Phase 1 and Phase 2 of the project completed, including all Conditions of Approval with the exception of those requiring dedication and improvement of a right-turn pocket/deceleration lane at the southeast corner of Sepulveda Boulevard and 33rd Street. In addition, the Sepulveda Bridge widening project was completed in 2023, which widened eastbound Sepulveda Boulevard to four lanes between Marine and Rosecrans avenues.

The Property is in the Community Commercial (CC) zoning district. The Property is in Area District II, and has a General Plan land use designation of Manhattan Village.

PROJECT OVERVIEW	
Location:	3200 N. Sepulveda Boulevard
Legal Description:	PM122-33-35 LOT 7-9.13-17, POR LOTS 10-11, 22-23 POR LOT 4 RF-140, POR CF 218*
General Plan Land Use:	Manhattan Village
Zoning Designation:	CC (Community Commercial)
Area District:	II
Neighboring Zoning & Land Uses	<u>North</u> : General Commercial (CG); City of El Segundo
	<u>South</u> : CG, Single-Family Residential (RS); carwash and single-family residences
	<u>East</u> : RPD, Single-family residential
	<u>West</u> : CG, urgent care, office

**See Attachment D for full Property Legal Description*

Governing Regulations

The Project is reviewed for compliance with applicable regulations, including the City's General Plan, Municipal Code, and Sepulveda Corridor Design Guidelines.

Manhattan Beach General Plan

The General Plan is a long-range policy document, adopted in 2003, that identifies the community's vision for its collective future and establishes the fundamental framework to guide decision-making about development, resource management, public safety, public services, and general community well-being. The General Plan contains a series of goals and policies that allow this vision to be implemented. All projects are reviewed to ensure alignment with the General Plan's goals and policies. General Plans contain required "elements," or chapters, including a Mobility Element which is used to guide the City's plans for multi-modal transportation. The City's Mobility Element was revised in 2018. The Project was evaluated for conformance with the following applicable Mobility Element goals and policies:

- Mobility Element Goal I-1: Provide a balanced, safe, and efficient multi-modal transportation system that serves the mobility needs of all community members, including children, seniors, and the disabled.
- Mobility Element Policy I-1.1: Review the safety and functioning of the street system on a regular basis to identify problems and develop solutions.
- Mobility Element Policy I-1.6: Require property owners, at the time of new construction or substantial remodeling to dedicate land for public improvements such as roadways, wider sidewalks and/or bicycle lanes, as appropriate and warranted by the project.

Manhattan Beach Municipal Code

Development projects and uses of land, including those that are subject to discretionary reviews such as a Use Permit, are regulated by Title 10 (Planning and Zoning) of the Manhattan Beach Municipal Code ("MBMC"). The overall purpose of the regulations contained in the MBMC is to protect and promote the public health, safety, and general welfare, and to implement the policies of the City of Manhattan Beach General Plan.

The Project was reviewed for compliance with the MBMC, including but not limited to, the following section:

- MBMC Chapter 10.84 – governs use permits which are required for use classifications typically having unusual site development features or operating characteristics

requiring special consideration so that they may be designed, located, and operated compatibly with uses on adjoining properties in the surrounding area.

PROJECT DESCRIPTION

Mark Cermak, on behalf of RREEF (the applicant, owner and developer of the MVSC), submitted an application for a Master Use Permit Amendment to modify previously-approved conditions of approval for the site. Specifically, the applicant requests the removal of the final paragraph of Condition of Approval No. 39, and Condition of Approval 50.t, which require dedication and improvement of a right-turn pocket/declaration lane from northbound Sepulveda Boulevard to eastbound 33rd Street. No physical changes or further development of the site are proposed.

DISCUSSION

Project Analysis

As noted above, Resolution No. 14-0026 included conditions, added by the City Council at the hearing, requiring right-of-way dedication and construction of a right-turn pocket/deceleration lane from northbound Sepulveda Boulevard to eastbound 33rd Street. Further discussion of the conditions are provided below.

Right-turn Pocket/Deceleration Lane

The applicant requests relief from the requirement to dedicate public right-of-way and construct a right-turn pocket/deceleration lane along northbound Sepulveda Boulevard for traffic entering the MVSC at 33rd Street. This requirement is included as part of Conditions of Approval Nos. 39 and 50.t to Resolution No. 14-0026. These conditions were added to the resolution at the request of City Council at the hearing, and were accepted by the project applicant at that time. It is important to note that no impact to traffic or circulation was identified at the intersection of northbound Sepulveda Boulevard to eastbound 33rd Street in the traffic analysis prepared for the MVSC remodel, and the right-turn pocket/deceleration lane was not part of the mitigation measures contained in the certified EIR. Additionally, the right-turn pocket/deceleration lane is not required based on any municipal code requirements.

In January 2024, the applicant prepared an updated transportation review (included with this staff report as part of Attachment F) related to the right-turn pocket/deceleration lane. That review indicated that recent traffic volumes and collision data do not warrant improvement of the right-turn pocket/deceleration lane at this time. In fact, the review indicated that traffic volumes at the intersection are less than anticipated by the traffic analysis prepared prior to the MVSC remodel. The City's traffic engineer reviewed the updated transportation review prepared by the applicant and concurs with its findings.

Caltrans Review

Resolution No. 14-0026 included conditions of approval which required the applicant to install various public improvements along Sepulveda Boulevard, and Marine and Rosecrans Avenues. Because it is under the jurisdiction of Caltrans, all improvements to Sepulveda Boulevard are subject to approval by Caltrans. With the exception of the conditions of approval requiring the installation of the right-turn pocket/deceleration lane at the southeast corner of Sepulveda Boulevard and 33rd Street, all conditions requiring improvements within the right-of-way on Sepulveda Boulevard were identified as being subject to Caltrans approval. Conditions related to the Sepulveda Boulevard/33rd Street right-turn pocket/deceleration lane did not include an express stipulation requiring Caltrans approval.

With the support of City staff, the applicant prepared and submitted a draft design of the required right-turn pocket/deceleration lane to Caltrans for preliminary review in January 2022. Caltrans issued comments on the submittal in October 2022. The applicant resubmitted revised plans in May 2023, and again in June 2023, with comments being issued by Caltrans in June 2023 and August 2023, respectively. The most significant comment received from Caltrans throughout the review process was related to the need for additional information justifying installation of the right-turn pocket/deceleration lane. One of Caltrans's primary metrics to justify improvements is how an improvement would help address congestion.

In an effort to justify the need for the right-turn pocket/deceleration lane, the applicant prepared the aforementioned updated transportation review in January 2024. As noted above, the updated review identified a decrease in traffic counts in 2023 compared to projected traffic volumes identified in the traffic study prepared prior to approval of the MVSC remodel in 2014. As such, it was determined by City staff and the applicant that further submittals to Caltrans would not provide the justification requested.

Irrevocable Offer to Dedicate

As a result of the phasing of the on- and off-site improvements required for the MVSC remodel project, the conditions of approval added to Resolution No. 14-0026 at the December 2, 2014, City Council meeting, required an irrevocable offer to dedicate land (IOD) for the future construction of the right-turn pocket/deceleration lane the southeast corner of Sepulveda Boulevard and 33rd Street. Because current traffic volumes do not warrant the need to install said improvement at this time, the applicant has requested to remove not only the condition requiring the improvements, but also the requirement to record an IOD for the right-turn pocket/deceleration lane from the conditions of approval.

As noted above, staff acknowledges that construction of the right-turn pocket/deceleration lane is not warranted at this time. However, should traffic volumes increase along Sepulveda

Boulevard in a manner that would justify the installation of the right-turn pocket/deceleration lane, the real property needed to construct it would have to be obtained from RREEF or their successor in interest. Because the applicant agreed to the dedication and improvement conditions as part of the project approval, and has exercised the rights and benefits granted by the approvals, City staff does not support the request to eliminate the requirement to record the IOD from the conditions of approval applied to the project as part of Resolution No. 14-0026. Compliance with the offer to dedicate requirement as envisioned by the conditions provides the City with the flexibility to more efficiently implement the right-turn pocket/deceleration lane improvement if and when warranted in the future.

It should be noted that recordation of the IOD does not require RREEF to install the right-turn pocket/deceleration lane; it only requires them to dedicate the real property for public right-of-way purposes when requested by the City. Future construction of the improvements would be at the expense of the City, however the right-of-way rights necessary for the improvement will have been obtained from RREEF. By requiring recordation of the IOD, the City has the right, but not the obligation, to obtain the real property in the future, which right would not be executed unless and until the improvements are needed. If the City were to obtain the real property at this time, it would be responsible for maintenance of said property. As such, the IOD is the preferred method to obtain the right-of-way if and when the right-turn pocket/deceleration lane is needed.

Consistency and Compliance with Governing Regulations

Manhattan Beach General Plan

As noted above, the Project is governed by the Manhattan Beach General Plan. As such, it was reviewed for consistency with applicable General Plan goals and policies. The Project has been determined to be consistent with the following goals and policies for the reasons described below:

- *Mobility Element Goal 1-1: Provide a balanced, safe, and efficient multi-modal transportation system that serves the mobility needs of all community members, including children, seniors, and the disabled.*

Based upon evidence in the record, namely the Traffic Impact Analysis prepared for the project prior to approval of the Master Use Permit in 2014 and the Updated Transportation Review prepared in May 2024, a right-turn pocket/deceleration lane from northbound Sepulveda Boulevard to eastbound 33rd Street is not warranted at this time and would not result in significant improvements to the safety and efficiency of the intersection. Maintaining the requirement for the applicant to record the IOD will allow the City to obtain the public right-of-way necessary to construct a right-turn lane if deemed necessary at a future date.

- *Mobility Element Policy I-1.1: Review the safety and functioning of the street system on a regular basis to identify problems and develop solutions.*

Removal of the conditions requiring construction of a right turn lane at northbound Sepulveda Boulevard to eastbound 33rd Street is warranted at this time. However, recordation of the IOD will allow staff to develop and implement solutions to safety and functionality problems should they be identified in the future.

- *Mobility Element Policy I-1.6: Require property owners, at the time of new construction or substantial remodeling to dedicate land for public improvements such as roadways, wider sidewalks and/or bicycle lanes, as appropriate and warranted by the project.*

Removing the condition requiring the applicant to construct the right turn lane would not alter the Project's consistency with the Mobility Element, and obtaining the IOD preserves flexibility to implement improvements in the future if warranted.

Manhattan Beach Municipal Code

Pursuant to Section 10.84.020, the Planning Commission is the decision-making authority for use permits and amendments thereto. Per Section 10.84.060, the Planning Commission, as the decision-making authority, must make certain findings for approval of a use permit. An analysis of those findings is provided in the *Required Findings* section below.

Interdepartmental Review

The Traffic and Building Divisions, along with the Public Works and Fire Departments reviewed the request. The Traffic Division and Public Works department are supportive of the request to remove conditions requiring construction of the right-turn pocket/deceleration lane at the southeast corner of Sepulveda Boulevard and 33rd Street for the reasons noted in the staff report. They have, however requested that the City retain conditions requiring recordation of an IOD for the potential future construction of the right-turn pocket/deceleration lane if and when it becomes necessary.

Required Findings

Based upon substantial evidence in the record, staff believes that the Project, as proposed and conditioned, meets the required findings for a use permit.

Use Permit

1. *The proposed location of the use is in accord with the objectives of this title and the purposes of the district in which the site is located.*

The project is in the City's CC (Community Commercial) zoning district. The CC zone allows for planned commercial centers, such as Manhattan Village, which contain a wide variety of commercial establishments, including businesses selling home furnishings, apparel, durable goods, and specialty items and generally having a City-wide market area. Support facilities such as entertainment and eating-and-drinking establishments are permitted, subject to certain limitations to avoid adverse effects on adjacent uses. The Project proposes no change to the site's allowed land uses.

2. *The proposed location of the use and the proposed conditions under which the uses would be operated or maintained will be consistent with the General Plan; will not be detrimental to the public health, safety or welfare of persons residing or working on the proposed project site or in or adjacent to the neighborhood of such uses; and will not be detrimental to properties or improvements in the vicinity or to the general welfare of the city.*

The General Plan land use designation for the Property is Manhattan Village. Commercial uses in Manhattan Village are generally regional serving, including shopping centers, large department and specialty stores, establishments. and entertainment and restaurant. The Project proposes no change to the site's allowed land uses. Rather, the Project proposes removing conditions requiring a right-turn pocket/deceleration lane for northbound traffic entering the MVSC from Sepulveda Boulevard at 33rd Street. The Updated Transportation Review indicates there is no need for said right-turn pocket/deceleration lane at this time; thus the removal of the condition requiring it will not detrimentally impact properties or improvements in the vicinity or the general welfare of the city. Further, obtaining the IOD preserves the City's flexibility to address any impacts in the future should the need for such improvements arise.

3. *The proposed use will comply with the provisions of this title, including any specific condition required for the proposed uses in the district in which they would be located.*

Resolution No. 14-0026 provided conditions of approval specific to the remodel of the MVSC. Evidence in the record indicates that construction of a right-turn pocket/deceleration lane from northbound Sepulveda Boulevard to eastbound 33rd Street is not warranted at this time. Through the removal of conditions requiring the right-turn pocket/deceleration lane, and with implementation of the IOD condition, the project will be in compliance with all conditions of approval.

4. *The proposed use will not adversely impact nor be adversely impacted by nearby properties. Potential impacts are related but not necessarily limited to traffic, parking,*

noise, vibration, odors, resident security and personal safety, and aesthetics, or create demands exceeding the capacity of public services and facilities which cannot be mitigated.

The Updated Transportation Review found that construction of a right-turn pocket/deceleration lane from northbound Sepulveda Boulevard to eastbound 33rd Street is not warranted at this time based on current traffic and circulations patterns, and accident data. Retaining the requirement to record an IOD, will ensure that the City can install said right-turn pocket/deceleration lane if deemed necessary in the future.

ENVIRONMENTAL DETERMINATION

The City has reviewed the proposed project for compliance with the California Environmental Quality Act (CEQA) and has determined that the proposed project was adequately covered, and is fully within the scope of the previously certified Final Environmental Impact Report, Resolution No. 14-0025. Thus, no further environmental review or documentation is necessary.

PUBLIC NOTIFICATION AND COMMENT

A public notice for the October 9, 2024, public hearing was published in The Beach Reporter, mailed to all property owners within a 500-foot radius of the Property, and posted at City Hall, on September 27, 2024. As of the writing of this report, staff has not received any public comments.

CONCLUSION AND RECOMMENDATION

Based on the analysis included in this report, staff supports the applicant's request to be relieved from the requirement to install a right-turn pocket/deceleration lane at the southeast corner of Sepulveda Boulevard and 33rd Street, but does not support the applicant's request to eliminate the requirement to record an IOD over the real property needed to construct said improvement in the future. As such, staff recommends that the Planning Commission conduct a public hearing and adopt the attached draft resolution, modifying the condition of approval applied to Resolution No. 14-0026 related to the right-turn pocket/deceleration lane and IOD, and finding that the proposed project was adequately analyzed in the previously-certified Final Environmental Impact Report for the project certified through adoption of Resolution No. 14-0025.

ATTACHMENTS:

- A. Draft Planning Commission Resolution No. PC 24-XX
- B. Resolution No. 14-0026
- C. Vicinity Map
- D. Property Legal Description
- E. Applicant's Written Materials

- F. Updated Transportation Review - May 4, 2024
- G. Draft Irrevocable Offer to Dedicate

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RESOLUTION NO. PC 24-XX**A RESOLUTION OF THE MANHATTAN BEACH PLANNING COMMISSION APPROVING A MASTER USE PERMIT AMENDMENT TO REMOVE CONDITIONS OF APPROVAL RELATED TO CONSTRUCTION OF CERTAIN PUBLIC IMPROVEMENTS, AND FINDING THAT THE PROPOSED PROJECT IS WITHIN THE SCOPE OF AND WAS ADEQUATELY ANALYZED IN A PREVIOUSLY-CERTIFIED FINAL ENVIRONMENTAL IMPACT REPORT****THE MANHATTAN BEACH PLANNING COMMISSION DOES HEREBY RESOLVE AS FOLLOWS:**

SECTION 1. On December 2, 2014, the City Council adopted Resolution No. 14-0025, certifying a Final Environmental Impact Report (“EIR”) and Resolution No.14-0026, approving a Master Use Permit, Height Variance, Sign Exception/Program, to allow for the remodeling and expansion of a portion of the Manhattan Village Shopping Center (“MVSC”), subject to approvals accepted by the applicant. On December 6, 2016, the City Council adopted Resolution No. 16-0081, approving Height Variances, an expansion of an existing clerestory and modification of the existing west mall entrances that already exceeded the maximum height, and increasing the height of the previously approved parapet walls from 22 feet to 28 feet. On December 20, 2016, the City Council adopted the first addendum to the EIR and a modified site plan from the 2014 approval, consistent with a Director’s determination that a Master Use Permit was not required, and a finding from an environmental consultant that the addendum was sufficient, and no further environmental documents or review were required. On September 5, 2017, the City Council adopted Resolution No. 17-0119, adopting the second addendum to the EIR and further amending the Master Use Permit to refine certain conditions of approval imposed on the permit for the remodel and expansion of the MVSC. On May 22, 2019, the Planning Commission adopted Resolution No. PC 19-06, approving a Use Permit to allow the construction of a new commercial building for a restaurant with on-sale service of distilled spirits, beer, and wine, located at 3110 N. Sepulveda Boulevard (currently BOA and Sushi Roku). On October 9, 2019, the Planning Commission adopted Resolution No. PC 19-13, approving a Master Use Permit Amendment to modify certain conditions of approval previously imposed, and adopting a third addendum to the EIR. On April 29, 2020, the Planning Commission adopted Resolution No. PC 20-05, approving a Sign Exception to allow off-premises and digital signage as part of a Sign Program.

SECTION 2. On February 19, 2024, the Community Development Department received an application requesting a Master Use Permit Amendment to modify existing conditions of approval concerning site access for Manhattan Village Shopping Center (“Project”) located at 2600-3600 N. Sepulveda Boulevard and 1180-1200 Rosecrans Avenue (“Property”). Within the Property, the Project would impact the southwest corner of the intersection of North Sepulveda Boulevard and 33rd Street. Pursuant to the Manhattan Beach Municipal Code (“MBMC”), the Project requires a Master Use Permit Amendment.

SECTION 3. Use permits and amendments thereto are governed by Chapter 10.84 of the Manhattan Beach Municipal Code (MBMC). Section 10.84.010 of the MBMC specifies that, “[U]se permits are required for use classifications typically having unusual site development features or operating characteristics requiring special consideration so that they may be designed, located, and operated compatibly with uses on adjoining properties and in the surrounding area.” Section 10.84.105.D of the MBMC states that an amendment to a master use permit is required any time modifications to existing conditions of approval are requested. The Applicant’s request is to modify existing conditions of approval which require right-of-way dedication and construction of a right-turn pocket/deceleration lane at northbound Sepulveda Boulevard to eastbound 33rd Street.

SECTION 4. On October 9, 2024, the Planning Commission conducted a duly noticed public hearing to consider the Project, during which the Planning Commission received a presentation by staff and testimony from the Applicant, and provided an opportunity for the public to provide evidence and testimony. The Planning Commission also received and reviewed written testimony received by the City prior to the public hearing.

SECTION 5. The Project was adequately covered by, and is within the scope of the previously certified Final Environmental Impact Report, per Resolution No. 14-0025. Thus, no further environmental review

or documentation under the California Environmental Quality Act is necessary.

SECTION 6. The record of the public hearing indicates:

- A. The legal description of the Property is included as Exhibit “A” to this resolution.
- B. The Property is located in Area District II and is zoned Community Commercial (CC), with a General Plan land use designation of Manhattan Village. The Property is surrounded by CG zoned properties to the north, south, and west, and RPD zoned properties to the east.
- C. The Property is currently governed by a master use permit approved by City Council in 2014, as amended.
- D. Traffic studies have been conducted which demonstrate that a right-turn pocket/deceleration lane on northbound Sepulveda Boulevard at 33rd Street is not warranted at this time. Further, no impact to the intersection was identified in the Final EIR which would have required the right-turn pocket/deceleration lane as a mitigation measure.
- E. The applicant, with city staff’s assistance, has made multiple submittals to Caltrans to obtain authorization to install the right-turn pocket/deceleration lane. Caltrans has stated that the no evidence has been presented which demonstrates the need for said improvement at this time.
- F. The Project is consistent with following General Plan goals and policies, as described in the staff report prepared for the Project:

Mobility Element Goal I-1: Provide a balanced, safe, and efficient multi-modal transportation system that serves the mobility needs of all community members, including children, seniors, and the disabled.

Based upon evidence in the record, namely the Traffic Impact Analysis prepared for the project prior to approval of the Master Use Permit in 2014 and the Updated Transportation Review prepared in May 2024, a right-turn pocket/deceleration lane from northbound Sepulveda Boulevard to eastbound 33rd Street is not warranted at this time and would not result in significant improvements to the safety and efficiency of the intersection. Maintaining the requirement for the applicant to record the IOD will allow the City to obtain the public right-of-way necessary to construct a right-turn lane if deemed necessary at a future date.

Mobility Element Policy I-1.1: Review the safety and functioning of the street system on a regular basis to identify problems and develop solutions.

Removal of the conditions requiring construction of a right turn lane at northbound Sepulveda Boulevard to eastbound 33rd Street is warranted at this time. However, recordation of the IOD will allow staff to develop and implement solutions to safety and functionality problems should they be identified in the future.

Mobility Element Policy I-1.6: Require property owners, at the time of new construction or substantial remodeling to dedicate land for public improvements such as roadways, wider sidewalks and/or bicycle lanes, as appropriate and warranted by the project.

Removing the condition requiring the applicant to construct the right turn lane would not alter the Project’s consistency with the Mobility Element, and obtaining the IOD preserves flexibility to implement improvements in the future if warranted.

SECTION 7. Based upon substantial evidence in the record, and pursuant to Section 10.84.060 of

the MBMC, the Planning Commission hereby makes the following findings related to the Master Use Permit Amendment:

A. *The proposed location of the use is in accord with the objectives of this title and the purposes of the district in which the site is located for the following reasons:*

The project is in the City's CC (Community Commercial) zoning district. The CC zone allows for planned commercial centers, such as Manhattan Village, which contain a wide variety of commercial establishments, including businesses selling home furnishings, apparel, durable goods, and specialty items and generally having a City-wide market area. Support facilities such as entertainment and eating-and-drinking establishments are permitted, subject to certain limitations to avoid adverse effects on adjacent uses. The Project proposes no change to the site's allowed land uses.

B. *The proposed location of the use and the proposed conditions under which the uses would be operated or maintained will be consistent with the General Plan; will not be detrimental to the public health, safety or welfare of persons residing or working on the proposed project site or in or adjacent to the neighborhood of such uses; and will not be detrimental to properties or improvements in the vicinity or to the general welfare of the city for the following reasons:*

The General Plan land use designation for the Property is Manhattan Village. Commercial uses in Manhattan Village are generally regional serving, including shopping centers, large department and specialty stores, establishments, and entertainment and restaurant. The Project proposes no change to the site's allowed land uses. Rather, the Project proposes removing conditions requiring a right-turn pocket/deceleration lane for northbound traffic entering the MVSC from Sepulveda Boulevard at 33rd Street. The Updated Transportation Review indicates there is no need for said right-turn pocket/deceleration lane at this time; thus the removal of the condition requiring it will not detrimentally impact properties or improvements in the vicinity or the general welfare of the city. Further, obtaining the IOD preserves the City's flexibility to address any impacts in the future should the need for such improvements arise.

C. *The proposed use will comply with the provisions of this title, including any specific condition required for the proposed uses in the district in which they would be located for the following reasons:*

Resolution No. 14-0026 provided conditions of approval specific to the remodel of the MVSC. Evidence in the record indicates that construction of a right-turn pocket/deceleration lane from northbound Sepulveda Boulevard to eastbound 33rd Street is not warranted at this time. Through the removal of conditions requiring the right-turn pocket/deceleration lane, and with implementation of the IOD condition, the project will be in compliance with all conditions of approval.

D. *The proposed use will not adversely impact or be adversely impacted by nearby properties. Potential impacts are related but not necessarily limited to traffic, parking, noise, vibration, odors, resident security and personal safety, and aesthetics, or create demands exceeding the capacity of public services and facilities which cannot be mitigated for the following reasons:*

The Updated Transportation Review found that construction of a right-turn pocket/deceleration lane from northbound Sepulveda Boulevard to eastbound 33rd Street is not warranted at this time based on current traffic and circulations patterns, and accident data. Retaining the requirement to record an IOD, will ensure that the City can install said right-turn pocket/deceleration lane if deemed necessary in the future.

SECTION 8. Based upon the foregoing, the Planning Commission hereby APPROVES the Project, subject to the conditions below.

General

1. All conditions of approval contained in Resolution No. 14-0026, and in all other entitlements issued for the Property, shall remain in full force and in effect, unless superseded by the conditions of this resolution.
2. The last paragraph of Condition of Approval No. 39 in Resolution No. 14-0026, is hereby amended to read as follows, with all other provisions remaining in full force and effect without amendment:

RREEF shall record an irrevocable offer to dedicate (“IOD”) land for public right-of-way purposes at the southeast corner of Sepulveda Boulevard and 33rd Street to accommodate improvements for a future right turn pocket/deceleration lane from northbound Sepulveda Boulevard to eastbound 33rd Street. The IOD shall include all land necessary to accommodate improvements shown in the Design Engineering Evaluation Report submitted to Caltrans in May 2023, to the satisfaction of the City Engineer.

3. Condition of Approval No. 50.t, in Resolution No. 14-0026 is hereby deleted and of no further force or effect.
4. Any questions of intent or interpretation of any condition will be reviewed by the Community Development Director to determine if further Planning Commission review and action is required.
5. At any time in the future, the Planning Commission or City Council may review the Master Use Permit and any amendments thereto for the purpose of revocation or modification in accordance with the requirements of the Manhattan Beach Municipal Code (“MBMC”) Chapter 10.104. Modification may consist of conditions deemed reasonable to mitigate or alleviate impacts to adjacent land uses.
6. Community Development Department staff shall be allowed to inspect the Property at any time to determine compliance with conditions imposed and MBMC requirements.

Procedural

7. Terms and Conditions are Perpetual; Recordation of Covenant. The provisions, terms and conditions set forth herein are perpetual, and are binding on the owner, its successors-in-interest, and, where applicable, all tenants and lessees of the site. Further, the owner shall submit the covenant, prepared and approved by the City, indicating its consent to the conditions of approval of this Resolution, and the City shall record the covenant with the Office of the County Clerk/Recorder of Los Angeles. Owner shall deliver the executed covenant, and all required recording and related fees, to the Community Development Department within 45 calendar days of receipt of a signed copy of this Resolution. Notwithstanding the foregoing, the Director may, upon a request by owner, grant an extension to the 45-day time limit. The Project approval shall not become effective until the covenant is recorded.

SECTION 9. Indemnity, Duty to Defend and Obligation to Pay Judgments and Defense Costs, Including Attorneys’ Fees, Incurred by the City. The operator and owner (operator/owner) shall defend, indemnify, and hold harmless the City, its elected officials, officers, employees, volunteers, agents, and those City agents serving as independent contractors in the role of City officials (collectively “Indemnitees”) from and against any claims, damages, actions, causes of actions, lawsuits, suits, proceedings, losses, judgments, costs, and expenses (including, without limitation, attorneys’ fees or court costs) in any manner arising out of or incident to this approval, related entitlements, or the City’s environmental review thereof. The operator/owner shall pay and satisfy any judgment, award or decree that may be rendered against City or the other Indemnitees in any such suit, action, or other legal proceeding. The City shall promptly notify the operator/owner of any claim, action, or proceeding and the City shall reasonably cooperate in the defense. If the City fails to promptly notify the operator/owner of any claim, action, or proceeding, or if the City fails to reasonably cooperate in the defense, the operator/owner shall not thereafter be responsible to defend, indemnify, or hold harmless the City or the Indemnitees. The City shall have the right to select counsel of its choice. The operator/owner shall reimburse the City, and the other Indemnitees, for any and all legal expenses and costs incurred by each of them in connection therewith or in enforcing the indemnity herein provided. Nothing in this Section shall be construed to

require the operator/owner to indemnify Indemnitees for any Claim arising from the sole negligence or willful misconduct of the Indemnitees. In the event such a legal action is filed challenging the City's determinations herein or the issuance of the approval, the City shall estimate its expenses for the litigation. The operator/owner shall deposit said amount with the City or enter into an agreement with the City to pay such expenses as they become due.

SECTION 10. The Planning Commission's decision is based upon each of the totally independent and separate grounds stated herein, each of which stands alone as a sufficient basis for its decision. Further, this approval is limited to the specific amendments contemplated herein, and shall not be construed as a new approval of the project or new adoption of any previously imposed condition of approval unless specifically amended herein. Any appeal or challenge of this approval shall be limited only to the specific amendments approved herein.

SECTION 11. This Resolution shall become effective when all time limits for appeal as set forth in MBMC Chapter 10.100 have expired.

SECTION 12. The Secretary of the Planning Commission shall certify to the adoption of this Resolution and shall forward a copy of this Resolution to the Applicant. The Secretary shall make this resolution readily available for public inspection.

[Continued on following page]

SECTION 13. This Master Use Permit amendment shall lapse two years after its date of approval, unless implemented or extended pursuant to 10.84.090 of the MBMC.

October 9, 2024

Kristin Sistos
Planning Commission Chair

I hereby certify that the following is a full, true, and correct copy of the Resolution as **ADOPTED** by the Planning Commission at its regular meeting on **OCTOBER 9, 2024**, and that said Resolution was adopted by the following vote:

AYES:

NOES:

ABSTAIN:

ABSENT:

Adam Finestone, AICP
Secretary to the Planning Commission

Tatiana Maury
Recording Secretary

EXHIBIT A

Manhattan Village Shopping Center MUP Amendment

LEGAL DESCRIPTION

(CHICAGO TITLE COMPANY ORDER NO. 00197412-987-OC1-K27 DATED AUGUST 24, 2023)

THE LAND REFERRED TO HEREIN BELOW IS SITUATED IN THE CITY OF MANHATTAN BEACH, IN THE COUNTY OF LOS ANGELES, STATE OF CALIFORNIA, AND IS DESCRIBED AS FOLLOWS:

PARCEL 1:

THAT PORTION OF LOT 4 IN SECTION 19, TOWNSHIP 3 SOUTH, RANGE 14 WEST, SAN BERNARDINO BASE AND MERIDIAN, IN THE CITY OF MANHATTAN BEACH, COUNTY OF LOS ANGELES, STATE OF CALIFORNIA, AS SHOWN ON PARTITION MAP SHOWING PROPERTY FORMERLY OF REDONDO LAND COMPANY, SUBDIVIDED BY JAMES F. TOWELL, C.A. EDWARDS AND P.P. WILCOX, COMMISSIONER, SURVEYED AUGUST, 1897, BY L. FRIEL AND FILED IN THE OFFICE OF THE COUNTY RECORDER OF SAID COUNTY ON SEPTEMBER 3, 1897 DESCRIBED AS FOLLOWS:

BEGINNING AT THE NORTHWEST CORNER OF SAID SECTION 19; THENCE SOUTH 0' 04' 16" EAST ALONG THE WEST LINE THEREOF, 77.04 FEET; THENCE NORTH 89' 55' 44" EAST PERPENDICULAR TO SAID WEST LINE 20.00 FEET TO THE TRUE POINT OF BEGINNING; THENCE SOUTH 0' 04' 16" EAST PARALLEL TO SAID WEST LINE 415.97 FEET TO A POINT IN THE SOUTH LINE OF SAID LOT 4; THENCE NORTH 89' 58' 45" EAST ALONG SAID SOUTH LINE 48.35 FEET, TO A POINT IN THE NORTHWESTERLY LINE OF THE 100 FOOT WIDE RIGHT OF WAY OF THE SOUTHERN CALIFORNIA RAILWAY COMPANY PER BOOK D-508 PAGE 76, OFFICIAL RECORDS OF SAID COUNTY, SAID POINT BEING A POINT IN A CURVE CONCAVE SOUTHEASTERLY AND HAVING A RADIUS OF 5779.65 FEET, A RADIAL LINE PASSING THROUGH SAID POINT BEARS NORTH 38' 19' 56" WEST; THENCE NORTHEASTERLY ALONG SAID CURVE AN ARC LENGTH OF 626.67 FEET THROUGH A CENTRAL ANGLE OF 6' 12' 45"; THENCE TANGENT TO SAID CURVE AND CONTINUING ALONG SAID NORTHWESTERLY LINE OF SAID RIGHT OF WAY NORTH 57' 53' 02" EAST 154.19 FEET TO A POINT IN THE SOUTHERLY LINE OF THE NORTHERLY 50.00 FEET OF SAID SECTION 19, SAID POINT ALSO BEING A POINT IN THE SOUTHERLY LINE OF ROSECRANS AVENUE, 100 FEET WIDE AS SAID AVENUE EXISTING ON NOVEMBER 29, 1979; THENCE SOUTH 89' 58' 45" WEST ALONG SAID SOUTHERLY LINE 664.17 FEET TO THE BEGINNING OF A TANGENT CURVE CONCAVE SOUTHEASTERLY AND HAVING A RADIUS OF 27.00 FEET; THENCE WESTERLY, SOUTHWESTERLY AND SOUTHERLY ALONG SAID CURVE AN ARC LENGTH OF 42.43 FEET THROUGH A CENTRAL ANGLE OF 90' 03' 01" TO THE TRUE POINT OF BEGINNING.

EXCEPTING THEREFROM, THAT PORTION OF SAID LAND CONVEYED TO THE CITY OF MANHATTAN BEACH, A MUNICIPAL CORPORATION, BY DEEDS RECORDED OCTOBER 2, 1997 AS INSTRUMENT NO. 97-1521451 AND AS INSTRUMENT NO. 97-1521452, BOTH OF OFFICIAL RECORDS ALSO EXCEPTING THEREFROM THAT PORTION OF SAID LOT 4 DESCRIBED AS FOLLOWS:

BEGINNING AT THE NORTHWEST CORNER OF SAID SECTION 19; THENCE SOUTH 0' 04' 16" EAST ALONG THE WEST LINE THEREOF, 77.04 FEET; THENCE NORTH 89' 55' 44" EAST PERPENDICULAR TO SAID WEST LINE 20.00 FEET; THENCE SOUTH 0' 04' 16" EAST PARALLEL TO SAID WEST LINE 415.97 FEET TO A POINT IN THE SOUTH LINE OF SAID LOT 4; THENCE NORTH 89' 58' 45" EAST ALONG SAID SOUTH LINE 48.35 FEET TO A POINT IN THE NORTHWESTERLY LINE OF THE 100 FOOT WIDE RIGHT OF WAY OF THE SOUTHERN CALIFORNIA RAILWAY COMPANY PER BOOK D-508 PAGE 76, OFFICIAL RECORDS OF SAID COUNTY, SAID POINT BEING A POINT IN A CURVE CONCAVE SOUTHEASTERLY AND HAVING A RADIUS OF 5779.65 FEET, A RADIAL LINE PASSING THROUGH SAID POINT BEARS NORTH 38' 19' 43" WEST; THENCE NORTHEASTERLY ALONG SAID CURVE AN ARC LENGTH OF 532.95 FEET THROUGH A CENTRAL ANGLE OF 5' 17' 00" TO THE TRUE POINT OF BEGINNING; THENCE CONTINUING ALONG SAID NORTHWESTERLY LINE AND SAID CURVE AN ARC LENGTH OF 93.72 FEET THROUGH A CENTRAL ANGLE OF 0' 55' 45"; THENCE TANGENT TO SAID CURVE AND CONTINUING ALONG SAID NORTHWESTERLY LINE OF SAID RIGHT OF WAY

City of Manhattan Beach Environmental Information Form
And Master Application Form
Manhattan Village Shopping Center MUP Amendment

NORTH 57° 53' 02" EAST 154.19 FEET TO A POINT IN THE SOUTHERLY LINE OF THE NORTHERLY 50.00 FEET OF SAID SECTION 19, SAID POINT ALSO BEING A POINT IN THE SOUTHERLY LINE OF ROSECRANS AVENUE, 100 FEET WIDE AS SAID AVENUE EXISTING ON NOVEMBER 29, 1979; THENCE SOUTH 89° 58' 45" WEST ALONG SAID SOUTHERLY LINE 209.57 FEET; THENCE LEAVING SAID SOUTHERLY LINE OF ROSECRANS AVENUE SOUTH 132.37 TO THE TRUE POINT OF BEGINNING.

SAID DESCRIPTION DESCRIBED AS "EXHIBIT A LEGAL DESCRIPTION FOR PARCEL 2" OF THE CERTIFICATE OF LOT LINE ADJUSTMENT RECORDED SEPTEMBER 13, 2018 AS INSTRUMENT NO. 20180943008, OF OFFICIAL RECORDS.

PARCEL 2:

PARCELS 1, 3 THROUGH 9 INCLUSIVE, AND PARCELS 13, 15, 16, 18 THROUGH 21 INCLUSIVE, IN THE CITY OF MANHATTAN BEACH, COUNTY OF LOS ANGELES, STATE OF CALIFORNIA, AS SHOWN ON PARCEL MAP NO. 12219, FILED IN BOOK 122 PAGES 33 THROUGH 35 INCLUSIVE OF PARCEL MAPS, IN THE OFFICE OF THE COUNTY RECORDER OF SAID COUNTY.

EXCEPT ALL OIL, GAS AND OTHER HYDROCARBONS, GEOTHERMAL RESOURCES AS DEFINED IN SECTION 6903 OF THE CALIFORNIA PUBLIC RESOURCES CODE AND ALL OTHER MINERALS, WHETHER SIMILAR TO THOSE HEREIN SPECIFIED OR NOT, WITHIN OR THAT MAY BE PRODUCED FROM THE PROPERTY; PROVIDED, HOWEVER THAT ALL RIGHTS AND INTEREST IN THE SURFACE OF THE PROPERTY HAVE BEEN CONVEYED TO GRANTEE, NO RIGHTS OR INTEREST OF ANY KIND THEREIN, EXPRESS OR IMPLIED, BEING EXCEPTED OR RESERVED TO GRANTOR EXCEPT AS THEREINAFTER EXPRESSLY SET FORTH THEREIN.

ALSO EXCEPT THE SOLE AND EXCLUSIVE RIGHT FROM TIME TO TIME TO DRILL AND MAINTAIN WELLS OR OTHER WORKS INTO OR THROUGH PROPERTY BELOW A DEPTH OF 500 FEET AND TO PRODUCE, INJECT, STORE AND REMOVE FROM OR THROUGH SUCH WELLS OR WORKS, OIL, GAS AND OTHER SUBSTANCES OF WHATEVER NATURE, INCLUDING THE RIGHT TO PERFORM ANY AND ALL OPERATIONS DEEMED NECESSARY OR CONVENIENT FOR THE EXERCISE OF SUCH RIGHTS, AS RESERVED BY CHEVRON U.S.A., INC., A CALIFORNIA CORPORATION, RECORDED APRIL 19, 1979 AS INSTRUMENT' NO. 79-424732 OF OFFICIAL RECORDS, AS TO THAT PORTION OF SAID LAND, ACQUIRED BY DEED RECORDED APRIL 2, 1923 IN BOOK 1993 PAGE 351, OFFICIAL RECORDS.

PARCEL 3:

PARCEL 2, IN THE CITY OF MANHATTAN BEACH, COUNTY OF LOS ANGELES, STATE OF CALIFORNIA, AS SHOWN ON PARCEL MAP NO. 12219, FILED IN BOOK 122 PAGES 33 THROUGH 35 INCLUSIVE OF PARCEL MAPS, IN THE OFFICE OF THE COUNTY RECORDER OF SAID COUNTY.

TOGETHER WITH THAT PORTION OF PARCEL 22 OF SAID PARCEL MAP NO. 12219 DESCRIBED AS FOLLOWS:

BEGINNING AT THE SOUTHWEST CORNER OF SAID PARCEL 2, THENCE ALONG THE WESTERLY PROLONGATION OF THE SOUTH LINE OF SAID PARCEL 2, S89°56'03"W, 17.50 FEET; THENCE N0°03'57"W, 66.00 FEET TO THE SOUTH LINE OF PARCEL 3 OF SAID PARCEL MAP NO. 12219; THENCE ALONG SAID SOUTH LINE, N89°56'03"E, 17.50 FEET TO THE NORTHWEST CORNER OF SAID PARCEL 2; THENCE ALONG THE WEST LINE OF SAID PARCEL 2, S0°03'57"E, 66.00 FEET TO THE POINT OF BEGINNING.

ALSO TOGETHER WITH THAT PORTION OF PARCEL 22 OF SAID PARCEL MAP NO. 12219 DESCRIBED AS FOLLOWS:

City of Manhattan Beach Environmental Information Form
And Master Application Form
Manhattan Village Shopping Center MUP Amendment

BEGINNING AT THE SOUTHEAST CORNER OF SAID PARCEL 2, THENCE ALONG THE EASTERLY PROLONGATION OF THE SOUTH LINE OF SAID PARCEL 2, N89°56'03"E, 5.86 FEET; THENCE N0°03'57"W, 59.99 FEET; THENCE S89°56'03"W, 3.30 FEET; THENCE N0°03'57"W, 6.01 TO THE SOUTH LINE OF PARCEL 3 OF SAID PARCEL MAP NO. 12219; THENCE ALONG SAID SOUTH LINE, S89°56'03"W, 2.56 FEET TO THE NORTHEAST CORNER OF SAID PARCEL 2; THENCE ALONG THE EAST LINE OF SAID PARCEL 2, S0°03'57"E, 66.00 FEET TO THE POINT OF BEGINNING.

SAID DESCRIPTION DESCRIBED AS "EXHIBIT A LEGAL DESCRIPTION FOR PARCEL 2" OF THE CERTIFICATE OF LOT LINE ADJUSTMENT RECORDED NOVEMBER 29, 2018 AS INSTRUMENT NO. 20181203208, OF OFFICIAL RECORDS.

ALSO EXCEPT ALL OIL, GAS AND OTHER HYDROCARBONS, GEOTHERMAL RESOURCES AS DEFINED IN SECTION 6903 OF THE CALIFORNIA PUBLIC RESOURCES CODE AND ALL OTHER MINERALS, WHETHER SIMILAR TO THOSE HEREIN SPECIFIED OR NOT, WITHIN OR THAT MAY BE PRODUCED FROM THE PROPERTY; PROVIDED, HOWEVER THAT ALL RIGHTS AND INTEREST IN THE SURFACE OF THE PROPERTY HAVE BEEN CONVEYED TO GRANTEE, NO RIGHTS OR INTEREST OF ANY KIND THEREIN, EXPRESS OR IMPLIED, BEING EXCEPTED OR RESERVED TO GRANTOR EXCEPT AS THEREINAFTER EXPRESSLY SET FORTH THEREIN.

ALSO EXCEPT THE SOLE AND EXCLUSIVE RIGHT FROM TIME TO TIME TO DRILL AND MAINTAIN WELLS OR OTHER WORKS INTO OR THROUGH PROPERTY BELOW A DEPTH OF 500 FEET AND TO PRODUCE, INJECT, STORE AND REMOVE FROM OR THROUGH SUCH WELLS OR WORKS, OIL, GAS AND OTHER SUBSTANCES OF WHATEVER NATURE, INCLUDING THE RIGHT TO PERFORM ANY AND ALL OPERATIONS DEEMED NECESSARY OR CONVENIENT FOR THE EXERCISE OF SUCH RIGHTS, AS RESERVED BY CHEVRON U.S.A., INC., A CALIFORNIA CORPORATION, RECORDED APRIL 19, 1979 AS INSTRUMENT NO. 79-424732 OF OFFICIAL RECORDS, AS TO THAT PORTION OF SAID LAND, ACQUIRED BY DEED RECORDED APRIL 2, 1923 IN BOOK 1993 PAGE 351, OFFICIAL RECORDS.

PARCEL 4:

PARCEL 11, IN THE CITY OF MANHATTAN BEACH, COUNTY OF LOS ANGELES, STATE OF CALIFORNIA, AS SHOWN ON PARCEL MAP NO. 12219, FILED IN BOOK 122 PAGES 33 THROUGH 35 INCLUSIVE OF PARCEL MAPS, IN THE OFFICE OF THE COUNTY RECORDER OF SAID COUNTY.

EXCEPTING THEREFROM THAT PORTION OF PARCEL 11, IN THE CITY OF MANHATTAN BEACH, COUNTY OF LOS ANGELES, STATE OF CALIFORNIA, AS SHOWN ON PARCEL MAP NO. 12219, FILED IN BOOK 122 PAGES 33 THROUGH 35 INCLUSIVE OF PARCEL MAPS, IN THE OFFICE OF THE COUNTY RECORDER OF SAID COUNTY, DESCRIBED AS FOLLOWS:

BEGINNING AT THE SOUTHEAST CORNER OF SAID PARCEL 11, THENCE ALONG THE EASTERLY LINE OF SAID PARCEL 11, N0°03'58"W 56.54 FEET TO THE BEGINNING OF A TANGENT CURVE CONCAVE TO THE SOUTHEAST HAVING A RADIUS OF 278.00 FEET; THENCE NORTHERLY THROUGH A CENTRAL ANGLE OF 10°53'16", AN ARC LENGTH OF 52.83 FEET; THENCE LEAVING SAID EASTERLY LINE, S89°57'42"W 92.94 FEET; THENCE N0°00'23"W 56.93 FEET; THENCE N29°26'38"W 57.53 FEET TO THE NORTHWESTERLY LINE OF SAID PARCEL 11; THENCE ALONG THE LINES OF SAID PARCEL 11, S57°53'02"W 193.37 FEET; THENCE S0°03'57"E 113.53 FEET; THENCE N89°56'03"E 280.00 FEET TO THE POINT OF BEGINNING.

SAID DESCRIPTION DESCRIBED AS "EXHIBIT A LEGAL DESCRIPTION FOR PARCEL 3" OF THE CERTIFICATE OF LOT LINE ADJUSTMENT RECORDED SEPTEMBER 13, 2018 AS INSTRUMENT NO. 20180943008, OF OFFICIAL RECORDS.

**City of Manhattan Beach Environmental Information Form
And Master Application Form
Manhattan Village Shopping Center MUP Amendment**

ALSO EXCEPT ALL OIL, GAS AND OTHER HYDROCARBONS, GEOTHERMAL RESOURCES AS DEFINED IN SECTION 6903 OF THE CALIFORNIA PUBLIC RESOURCES CODE AND ALL OTHER MINERALS, WHETHER SIMILAR TO THOSE HEREIN SPECIFIED OR NOT, WITHIN OR THAT MAY BE PRODUCED FROM THE PROPERTY; PROVIDED, HOWEVER THAT ALL RIGHTS AND INTEREST IN THE SURFACE OF THE PROPERTY HAVE BEEN CONVEYED TO GRANTEE, NO RIGHTS OR INTEREST OF ANY KIND THEREIN, EXPRESS OR IMPLIED, BEING EXCEPTED OR RESERVED TO GRANTOR EXCEPT AS THEREINAFTER EXPRESSLY SET FORTH THEREIN.

ALSO EXCEPT THE SOLE AND EXCLUSIVE RIGHT FROM TIME TO TIME TO DRILL AND MAINTAIN WELLS OR OTHER WORKS INTO OR THROUGH PROPERTY BELOW A DEPTH OF 500 FEET AND TO PRODUCE, INJECT, STORE AND REMOVE FROM OR THROUGH SUCH WELLS OR WORKS, OIL, GAS AND OTHER SUBSTANCES OF WHATEVER NATURE, INCLUDING THE RIGHT TO PERFORM ANY AND ALL OPERATIONS DEEMED NECESSARY OR CONVENIENT FOR THE EXERCISE OF SUCH RIGHTS, AS RESERVED BY CHEVRON U.S.A., INC., A CALIFORNIA CORPORATION, RECORDED APRIL 19, 1979 AS INSTRUMENT' NO. 79-424732 OF OFFICIAL RECORDS, AS TO THAT PORTION OF SAID LAND, ACQUIRED BY DEED RECORDED APRIL 2, 1923 IN BOOK 1993 PAGE 351, OFFICIAL RECORDS.

PARCEL 5:

THAT PORTION OF PARCEL 23, IN THE CITY OF MANHATTAN BEACH, COUNTY OF LOS ANGELES, STATE OF CALIFORNIA, AS SHOWN ON PARCEL MAP NO. 12219, FILED IN BOOK 122 PAGES 33 THROUGH 35 INCLUSIVE OF PARCEL MAPS, IN THE OFFICE OF THE COUNTY RECORDER OF SAID COUNTY DESCRIBED AS FOLLOWS:

THAT PORTION OF PARCEL 1, IN THE CITY OF MANHATTAN BEACH, COUNTY OF LOS ANGELES, STATE OF CALIFORNIA, AS SHOWN ON LOT LINE ADJUSTMENT RECORDED AUGUST 2, 2018 AS INSTRUMENT NO. 20180779112, IN THE OFFICE OF THE COUNTY RECORDER OF SAID COUNTY BEING DESCRIBED AS FOLLOWS:

BEGINNING AT THE EASTERLY TERMINUS OF THAT CERTAIN COURSE IN PARCEL 2 OF SAID INSTRUMENT NO. 20180779112, HAVING A BEARING OF N89°56'03"E, A DISTANCE OF 49.81 FEET; THENCE ALONG THE LINES OF SAID PARCEL 2, S89°56'03"W 49.81 FEET; THENCE N0°05'28"W 109.09 FEET; THENCE S89°54'32"W 266.25 FEET; THENCE S0°05'28"W 105.52 FEET; THENCE LEAVING SAID PARCEL 2, WEST 61.23 FEET TO THE TRUE POINT OF BEGINNING; THENCE WEST 194.46 FEET; THENCE S45°00'00"W 9.90 FEET; THENCE SOUTH 317.30 FEET; THENCE WEST 73.76 FEET; THENCE SOUTH 174.69 FEET; THENCE EAST 70.37 FEET; THENCE NORTH 61.65 FEET; THENCE S89°48'25"E 204.85 FEET; THENCE NORTH 438.02 FEET TO THE TRUE POINT OF BEGINNING.

SAID DESCRIPTION DESCRIBED AS "EXHIBIT A LEGAL DESCRIPTION FOR PARCEL 4" OF THE CERTIFICATE OF LOT LINE ADJUSTMENT RECORDED SEPTEMBER 13, 2018 AS INSTRUMENT NO. 20180943008, OF OFFICIAL RECORDS.

EXCEPT ALL OIL, GAS AND OTHER HYDROCARBONS, GEOTHERMAL RESOURCES AS DEFINED IN SECTION 6903 OF THE CALIFORNIA PUBLIC RESOURCES CODE AND ALL OTHER MINERALS, WHETHER SIMILAR TO THOSE HEREIN SPECIFIED OR NOT, WITHIN OR THAT MAY BE PRODUCED FROM THE PROPERTY; PROVIDED, HOWEVER THAT ALL RIGHTS AND INTEREST IN THE SURFACE OF THE PROPERTY HAVE BEEN CONVEYED TO GRANTEE, NO RIGHTS OR INTEREST OF ANY KIND THEREIN, EXPRESS OR IMPLIED, BEING EXCEPTED OR RESERVED TO GRANTOR EXCEPT AS THEREINAFTER EXPRESSLY SET FORTH THEREIN.

ALSO EXCEPT THE SOLE AND EXCLUSIVE RIGHT FROM TIME TO TIME TO DRILL AND MAINTAIN WELLS OR OTHER WORKS INTO OR THROUGH PROPERTY BELOW A DEPTH OF 500 FEET AND TO PRODUCE, INJECT, STORE AND REMOVE FROM OR THROUGH SUCH WELLS OR WORKS, OIL, GAS

City of Manhattan Beach Environmental Information Form
And Master Application Form
Manhattan Village Shopping Center MUP Amendment

AND OTHER SUBSTANCES OF WHATEVER NATURE, INCLUDING THE RIGHT TO PERFORM ANY AND ALL OPERATIONS DEEMED NECESSARY OR CONVENIENT FOR THE EXERCISE OF SUCH RIGHTS, AS RESERVED BY CHEVRON U.S.A., INC., A CALIFORNIA CORPORATION, RECORDED APRIL 19, 1979 AS INSTRUMENT' NO. 79-424732 OF OFFICIAL RECORDS, AS TO THAT PORTION OF SAID LAND, ACQUIRED BY DEED RECORDED APRIL 2, 1923 IN BOOK 1993 PAGE 351, OFFICIAL RECORDS.

PARCEL 6:

PARCEL 1, IN THE CITY OF MANHATTAN BEACH, COUNTY OF LOS ANGELES, STATE OF CALIFORNIA, AS SHOWN ON LOT LINE ADJUSTMENT RECORDED SEPTEMBER 13, 2018 AS INSTRUMENT NO. 20180943008, IN THE OFFICE OF THE COUNTY RECORDER OF SAID COUNTY.

TOGETHER WITH PARCEL 1, IN THE CITY OF MANHATTAN BEACH, COUNTY OF LOS ANGELES, STATE OF CALIFORNIA, AS SHOWN ON LOT LINE ADJUSTMENT RECORDED NOVEMBER 29, 2018 AS INSTRUMENT NO. 20181203208, IN THE OFFICE OF THE COUNTY RECORDER OF SAID COUNTY.

TOGETHER WITH PARCEL 14, IN THE CITY OF MANHATTAN BEACH, COUNTY OF LOS ANGELES, STATE OF CALIFORNIA, AS SHOWN ON PARCEL MAP NO. 12219, FILED IN BOOK 122 PAGES 33 THROUGH 35 INCLUSIVE OF PARCEL MAPS, IN THE OFFICE OF THE COUNTY RECORDER OF SAID COUNTY.

TOGETHER WITH PARCEL 17, IN THE CITY OF MANHATTAN BEACH, COUNTY OF LOS ANGELES, STATE OF CALIFORNIA, AS SHOWN ON PARCEL MAP NO. 12219, FILED IN BOOK 122 PAGES 33 THROUGH 35 INCLUSIVE OF PARCEL MAPS, IN THE OFFICE OF THE COUNTY RECORDER OF SAID COUNTY.

EXCEPTING THEREFROM THAT PORTION OF THE ABOVE DESCRIBED LAND, DESCRIBED AS FOLLOWS:

BEGINNING AT THE NORTHEAST CORNER OF PARCEL 16 OF SAID PARCEL MAP NO. 12219; THENCE SOUTH 0°03'57" EAST 117.50 FEET TO THE SOUTHEAST CORNER OF SAID PARCEL 16; THENCE SOUTH 44°24'03" EAST 67.33 FEET TO THE TRUE POINT OF BEGINNING; THENCE EAST 76.17 FEET; THENCE NORTH 65.93 FEET; THENCE NORTH 89°59'50" WEST 76.17 FEET; THENCE SOUTH 65.94 FEET TO THE TRUE POINT OF BEGINNING.

ALSO EXCEPTING THEREFROM THAT PORTION OF THE ABOVE DESCRIBED LAND, DESCRIBED AS FOLLOWS:

BEGINNING AT THE NORTHEAST CORNER OF PARCEL 16 OF SAID PARCEL MAP NO. 12219; THENCE SOUTH 44°24'03" EAST 117.50 FEET TO THE SOUTHEAST CORNER OF SAID PARCEL 16; THENCE SOUTH 44°24'03" EAST 67.33 FEET; THENCE EAST 76.17 FEET TO THE TRUE POINT OF BEGINNING; THENCE NORTH 65.93 FEET; THENCE NORTH 89°59'50" WEST 5.46 FEET; THENCE, NORTH 186.50 FEET; THENCE WEST 31.43 FEET; THENCE NORTH 184.33 FEET; THENCE NORTH 89°59'50" WEST 39.28 FEET; THENCE NORTH 150.93 FEET; THENCE EAST 277.64 FEET; THENCE SOUTH 0°00'10" WEST 122.34 FEET; THENCE SOUTH 11°33'20" WEST 474.99 FEET; THENCE WEST 106.31 FEET TO THE TRUE POINT OF BEGINNING.

ALSO EXCEPTING THEREFROM THAT PORTION OF THE ABOVE DESCRIBED LAND, DESCRIBED AS FOLLOWS:

BEGINNING AT THE NORTHWEST CORNER OF PARCEL 19 OF SAID PARCEL MAP NO. 12219; THENCE SOUTH 0°03'57" EAST 100.00 FEET TO THE SOUTHWEST CORNER OF SAID PARCEL 19; THENCE SOUTH 0°03'57" EAST 61.00 FEET TO THE TRUE POINT OF BEGINNING; THENCE SOUTH 0°03'57"

City of Manhattan Beach Environmental Information Form
And Master Application Form
Manhattan Village Shopping Center MUP Amendment

EAST 100.00 FEET; THENCE NORTH 89°56'03" EAST 99.00 FEET; THENCE NORTH 0°03'57" WEST 100.00 FEET; THENCE SOUTH 89°56'03" WEST 99.00 FEET TO THE TRUE POINT OF BEGINNING.

ALSO EXCEPTING THEREFROM THAT PORTION OF SAID LAND, ALL THAT PORTION OF THE REAL PROPERTY CONVEYED TO THE CITY OF MANHATTAN BEACH PER THAT CERTAIN INSTRUMENT ENTITLED "GRANT DEED-DEED OF DEDICATION" RECORDED JUNE 25, 2002 AS INSTRUMENT NO. 02-1439469, OF OFFICIAL RECORDS.

ALSO EXCEPTING THEREFROM THAT PORTION OF SAID PARCEL 1 LOT LINE ADJUSTMENT RECORDED NOVEMBER 29, 2018 AS INSTRUMENT NO. 20181203208, THAT PORTION AS DESCRIBED IN THE OFFER OF DEDICATION RECORDED FEBRUARY 4, 1980 AS INSTRUMENT NO. 80-125713, OF OFFICIAL RECORDS.

SAID DESCRIPTION DESCRIBED AS "EXHIBIT A LEGAL DESCRIPTION FOR PARCEL 1" OF THE CERTIFICATE OF LOT LINE ADJUSTMENT RECORDED MAY 10, 2019 AS INSTRUMENT NO. 20190429681, OF OFFICIAL RECORDS.

ALSO EXCEPT ALL OIL, GAS AND OTHER HYDROCARBONS, GEOTHERMAL RESOURCES AS DEFINED IN SECTION 6903 OF THE CALIFORNIA PUBLIC RESOURCES CODE AND ALL OTHER MINERALS, WHETHER SIMILAR TO THOSE HEREIN SPECIFIED OR NOT, WITHIN OR THAT MAY BE PRODUCED FROM THE PROPERTY; PROVIDED, HOWEVER THAT ALL RIGHTS AND INTEREST IN THE SURFACE OF THE PROPERTY HAVE BEEN CONVEYED TO GRANTEE, NO RIGHTS OR INTEREST OF ANY KIND THEREIN, EXPRESS OR IMPLIED, BEING EXCEPTED OR RESERVED TO GRANTOR EXCEPT AS THEREINAFTER EXPRESSLY SET FORTH THEREIN.

ALSO EXCEPT THE SOLE AND EXCLUSIVE RIGHT FROM TIME TO TIME TO DRILL AND MAINTAIN WELLS OR OTHER WORKS INTO OR THROUGH PROPERTY BELOW A DEPTH OF 500 FEET AND TO PRODUCE, INJECT, STORE AND REMOVE FROM OR THROUGH SUCH WELLS OR WORKS, OIL, GAS AND OTHER SUBSTANCES OF WHATEVER NATURE, INCLUDING THE RIGHT TO PERFORM ANY AND ALL OPERATIONS DEEMED NECESSARY OR CONVENIENT FOR THE EXERCISE OF SUCH RIGHTS, AS RESERVED BY CHEVRON U.S.A., INC., A CALIFORNIA CORPORATION, RECORDED APRIL 19, 1979 AS INSTRUMENT NO. 79-424732 OF OFFICIAL RECORDS, AS TO THAT PORTION OF SAID LAND, ACQUIRED BY DEED RECORDED APRIL 2, 1923 IN BOOK 1993 PAGE 351, OFFICIAL RECORDS.

PARCEL 7:

THAT PORTION OF PARCELS 14 AND 17, IN THE CITY OF MANHATTAN BEACH, COUNTY OF LOS ANGELES, STATE OF CALIFORNIA, AS SHOWN ON PARCEL MAP NO. 12219, FILED IN BOOK 122 PAGES 33 THROUGH 35 INCLUSIVE OF PARCEL MAPS, IN THE OFFICE OF THE COUNTY RECORDER OF SAID COUNTY AND THAT PORTION OF PARCEL 1, IN THE CITY OF MANHATTAN BEACH, COUNTY OF LOS ANGELES, STATE OF CALIFORNIA, AS SHOWN ON LOT LINE ADJUSTMENT RECORDED SEPTEMBER 13, 2018 AS INSTRUMENT NO. 20180943008, IN THE OFFICE OF THE COUNTY RECORDER OF SAID COUNTY AND THAT PORTION OF PARCEL 1, IN THE CITY OF MANHATTAN BEACH, COUNTY OF LOS ANGELES, STATE OF CALIFORNIA, AS SHOWN ON LOT LINE ADJUSTMENT RECORDED NOVEMBER 29, 2018 AS INSTRUMENT NO. 20181203208, IN THE OFFICE OF THE COUNTY RECORDER OF SAID COUNTY, DESCRIBED AS FOLLOWS:

BEGINNING AT THE NORTHEAST CORNER OF PARCEL 16 OF SAID PARCEL MAP NO. 12219; THENCE SOUTH 0°03'57" EAST 117.50 FEET TO THE SOUTHEAST CORNER OF SAID PARCEL 16; THENCE SOUTH 44°24'03" EAST 67.33 FEET; THENCE EAST 76.17 FEET TO THE TRUE POINT OF BEGINNING; THENCE NORTH 65.93 FEET; THENCE NORTH 89°59'50" WEST 5.46 FEET; THENCE, NORTH 186.50 FEET; THENCE WEST 31.43 FEET; THENCE NORTH 184.33 FEET; THENCE NORTH 89°59'50" WEST 39.28 FEET; THENCE NORTH 150.93 FEET; THENCE EAST 277.64 FEET; THENCE SOUTH 0°00'10" WEST

**City of Manhattan Beach Environmental Information Form
And Master Application Form
Manhattan Village Shopping Center MUP Amendment**

122.34 FEET; THENCE SOUTH 11°33'20" WEST 474.99 FEET; THENCE WEST 106.31 FEET TO THE TRUE POINT OF BEGINNING.

SAID DESCRIPTION DESCRIBED AS "EXHIBIT A LEGAL DESCRIPTION FOR PARCEL 2" OF THE CERTIFICATE OF LOT LINE ADJUSTMENT RECORDED MAY 10, 2019 AS INSTRUMENT NO. 20190429681, OF OFFICIAL RECORDS.

EXCEPT ALL OIL, GAS AND OTHER HYDROCARBONS, GEOTHERMAL RESOURCES AS DEFINED IN SECTION 6903 OF THE CALIFORNIA PUBLIC RESOURCES CODE AND ALL OTHER MINERALS, WHETHER SIMILAR TO THOSE HEREIN SPECIFIED OR NOT, WITHIN OR THAT MAY BE PRODUCED FROM THE PROPERTY; PROVIDED, HOWEVER THAT ALL RIGHTS AND INTEREST IN THE SURFACE OF THE PROPERTY HAVE BEEN CONVEYED TO GRANTEE, NO RIGHTS OR INTEREST OF ANY KIND THEREIN, EXPRESS OR IMPLIED, BEING EXCEPTED OR RESERVED TO GRANTOR EXCEPT AS THEREINAFTER EXPRESSLY SET FORTH THEREIN.

ALSO EXCEPT THE SOLE AND EXCLUSIVE RIGHT FROM TIME TO TIME TO DRILL AND MAINTAIN WELLS OR OTHER WORKS INTO OR THROUGH PROPERTY BELOW A DEPTH OF 500 FEET AND TO PRODUCE, INJECT, STORE AND REMOVE FROM OR THROUGH SUCH WELLS OR WORKS, OIL, GAS AND OTHER SUBSTANCES OF WHATEVER NATURE, INCLUDING THE RIGHT TO PERFORM ANY AND ALL OPERATIONS DEEMED NECESSARY OR CONVENIENT FOR THE EXERCISE OF SUCH RIGHTS, AS RESERVED BY CHEVRON U.S.A., INC., A CALIFORNIA CORPORATION, RECORDED APRIL 19, 1979 AS INSTRUMENT NO. 79-424732 OF OFFICIAL RECORDS, AS TO THAT PORTION OF SAID LAND, ACQUIRED BY DEED RECORDED APRIL 2, 1923 IN BOOK 1993 PAGE 351, OFFICIAL RECORDS.

PARCEL 8:

THAT PORTION OF PARCEL 17, IN THE CITY OF MANHATTAN BEACH, COUNTY OF LOS ANGELES, STATE OF CALIFORNIA, AS SHOWN ON PARCEL MAP NO. 12219, FILED IN BOOK 122 PAGES 33 THROUGH 35 INCLUSIVE OF PARCEL MAPS, IN THE OFFICE OF THE COUNTY RECORDER OF SAID COUNTY AND THAT PORTION OF PARCEL 1, IN THE CITY OF MANHATTAN BEACH, COUNTY OF LOS ANGELES, STATE OF CALIFORNIA, AS SHOWN ON LOT LINE ADJUSTMENT RECORDED SEPTEMBER 13, 2018 AS INSTRUMENT NO. 20180943008, IN THE OFFICE OF THE COUNTY RECORDER OF SAID COUNTY AND THAT PORTION OF PARCEL 1, IN THE CITY OF MANHATTAN BEACH, COUNTY OF LOS ANGELES, STATE OF CALIFORNIA, AS SHOWN ON LOT LINE ADJUSTMENT RECORDED NOVEMBER 29, 2018 AS INSTRUMENT NO. 20181203208, IN THE OFFICE OF THE COUNTY RECORDER OF SAID COUNTY DESCRIBED AS FOLLOWS:

BEGINNING AT THE NORTHEAST CORNER OF PARCEL 16 OF SAID PARCEL MAP NO. 12219; THENCE SOUTH 0°03'57" EAST 117.50 FEET TO THE SOUTHEAST CORNER OF SAID PARCEL 16; THENCE SOUTH 44°24'03" EAST 67.33 FEET TO THE TRUE POINT OF BEGINNING; THENCE EAST 76.17 FEET; THENCE NORTH 65.93 FEET; THENCE NORTH 89°59'50" WEST 76.17 FEET; THENCE SOUTH 65.94 FEET TO THE TRUE POINT OF BEGINNING.

SAID DESCRIPTION DESCRIBED AS "EXHIBIT A LEGAL DESCRIPTION FOR PARCEL 3" OF THE CERTIFICATE OF LOT LINE ADJUSTMENT RECORDED MAY 10, 2019 AS INSTRUMENT NO. 20190429681, OF OFFICIAL RECORDS.

EXCEPT ALL OIL, GAS AND OTHER HYDROCARBONS, GEOTHERMAL RESOURCES AS DEFINED IN SECTION 6903 OF THE CALIFORNIA PUBLIC RESOURCES CODE AND ALL OTHER MINERALS, WHETHER SIMILAR TO THOSE HEREIN SPECIFIED OR NOT, WITHIN OR THAT MAY BE PRODUCED FROM THE PROPERTY; PROVIDED, HOWEVER THAT ALL RIGHTS AND INTEREST IN THE SURFACE OF THE PROPERTY HAVE BEEN CONVEYED TO GRANTEE, NO RIGHTS OR INTEREST OF ANY KIND

City of Manhattan Beach Environmental Information Form
And Master Application Form
Manhattan Village Shopping Center MUP Amendment

THEREIN, EXPRESS OR IMPLIED, BEING EXCEPTED OR RESERVED TO GRANTOR EXCEPT AS THEREINAFTER EXPRESSLY SET FORTH THEREIN.

ALSO EXCEPT THE SOLE AND EXCLUSIVE RIGHT FROM TIME TO TIME TO DRILL AND MAINTAIN WELLS OR OTHER WORKS INTO OR THROUGH PROPERTY BELOW A DEPTH OF 500 FEET AND TO PRODUCE, INJECT, STORE AND REMOVE FROM OR THROUGH SUCH WELLS OR WORKS, OIL, GAS AND OTHER SUBSTANCES OF WHATEVER NATURE, INCLUDING THE RIGHT TO PERFORM ANY AND ALL OPERATIONS DEEMED NECESSARY OR CONVENIENT FOR THE EXERCISE OF SUCH RIGHTS, AS RESERVED BY CHEVRON U.S.A., INC., A CALIFORNIA CORPORATION, RECORDED APRIL 19, 1979 AS INSTRUMENT' NO. 79-424732 OF OFFICIAL RECORDS, AS TO THAT PORTION OF SAID LAND, ACQUIRED BY DEED RECORDED APRIL 2, 1923 IN BOOK 1993 PAGE 351, OFFICIAL RECORDS.

PARCEL 9:

A PORTION OF PARCEL 1, IN THE CITY OF MANHATTAN BEACH, COUNTY OF LOS ANGELES, STATE OF CALIFORNIA, AS SHOWN ON LOT LINE ADJUSTMENT RECORDED NOVEMBER 29, 2018 AS INSTRUMENT NO. 20181203208, IN THE OFFICE OF THE COUNTY RECORDER OF SAID COUNTY, DESCRIBED AS FOLLOWS:

BEGINNING AT THE NORTHWEST CORNER OF PARCEL 19 OF SAID PARCEL MAP NO. 12219; THENCE SOUTH 0°03'57" EAST 100.00 FEET TO THE SOUTHWEST CORNER OF SAID PARCEL 19; THENCE SOUTH 0°03'57" EAST 61.00 FEET TO THE TRUE POINT OF BEGINNING; THENCE SOUTH 0°03'57" EAST 100.00 FEET; THENCE NORTH 89°56'03" EAST 99.00 FEET; THENCE NORTH 0°03'57" WEST 100.00 FEET; THENCE SOUTH 89°56'03" WEST 99.00 FEET TO THE TRUE POINT OF BEGINNING.

SAID DESCRIPTION DESCRIBED AS "EXHIBIT A LEGAL DESCRIPTION FOR PARCEL 4" OF THE CERTIFICATE OF LOT LINE ADJUSTMENT RECORDED MAY 10, 2019 AS INSTRUMENT NO. 20190429681, OF OFFICIAL RECORDS.

EXCEPT ALL OIL, GAS AND OTHER HYDROCARBONS, GEOTHERMAL RESOURCES AS DEFINED IN SECTION 6903 OF THE CALIFORNIA PUBLIC RESOURCES CODE AND ALL OTHER MINERALS, WHETHER SIMILAR TO THOSE HEREIN SPECIFIED OR NOT, WITHIN OR THAT MAY BE PRODUCED FROM THE PROPERTY; PROVIDED, HOWEVER THAT ALL RIGHTS AND INTEREST IN THE SURFACE OF THE PROPERTY HAVE BEEN CONVEYED TO GRANTEE, NO RIGHTS OR INTEREST OF ANY KIND THEREIN, EXPRESS OR IMPLIED, BEING EXCEPTED OR RESERVED TO GRANTOR EXCEPT AS THEREINAFTER EXPRESSLY SET FORTH THEREIN.

ALSO EXCEPT THE SOLE AND EXCLUSIVE RIGHT FROM TIME TO TIME TO DRILL AND MAINTAIN WELLS OR OTHER WORKS INTO OR THROUGH PROPERTY BELOW A DEPTH OF 500 FEET AND TO PRODUCE, INJECT, STORE AND REMOVE FROM OR THROUGH SUCH WELLS OR WORKS, OIL, GAS AND OTHER SUBSTANCES OF WHATEVER NATURE, INCLUDING THE RIGHT TO PERFORM ANY AND ALL OPERATIONS DEEMED NECESSARY OR CONVENIENT FOR THE EXERCISE OF SUCH RIGHTS, AS RESERVED BY CHEVRON U.S.A., INC., A CALIFORNIA CORPORATION, RECORDED APRIL 19, 1979 AS INSTRUMENT' NO. 79-424732 OF OFFICIAL RECORDS, AS TO THAT PORTION OF SAID LAND, ACQUIRED BY DEED RECORDED APRIL 2, 1923 IN BOOK 1993 PAGE 351, OFFICIAL RECORDS.

PARCEL 9A:

THE NON-EXCLUSIVE EASEMENTS FOR, INGRESS, EGRESS, PARKING, UTILITIES AND MAINTENANCE IN, TO, OVER, UNDER AND ACROSS, THE "COMMON AREA" ALL AS DESCRIBED AND SHOWN IN THAT CERTAIN CONSTRUCTION, OPERATION AND RECIPROCAL EASEMENT AGREEMENT DATED NOVEMBER 1, 1980, EXECUTED BY MANHATTAN BEACH COMMERCIAL

**City of Manhattan Beach Environmental Information Form
And Master Application Form
Manhattan Village Shopping Center MUP Amendment**

PROPERTIES, A GENERAL PARTNERSHIP, MANHATTAN HACIENDA PROPERTY CO., A GENERAL PARTNERSHIP AND FEDERATED DEPARTMENT STORES, INC., A DELAWARE CORPORATION, WITH ADDENDUM EXECUTED BY BUFFUMS, INC., A CALIFORNIA CORPORATION, RECORDED ON NOVEMBER 25, 1980 AS INSTRUMENT NO. 80-1188655 OF OFFICIAL RECORDS.

THE INTEREST OF MANHATTAN BEACH COMMERCIAL PROPERTIES, A CALIFORNIA GENERAL PARTNERSHIP UNDER SAID AGREEMENT HAS BEEN ASSIGNED TO BANK OF AMERICA NATIONAL TRUST AND SAVINGS TRUST AND SAVINGS ASSOCIATION, AS TRUSTEE OF THE MASTER PENSION TRUST OF THE PACIFIC TELESIS GROUP, BY ASSIGNMENT DATED DECEMBER 24, 1986 AND RECORDED DECEMBER 24, 1986 AS INSTRUMENT NO. 86-1800316, OFFICIAL RECORDS.

THE INTEREST OF BANK OF AMERICA NATIONAL TRUST AND SAVINGS ASSOCIATION, AS TRUSTEE OF THE MASTER PENSION TRUST OF THE PACIFIC TELESIS GROUP UNDER SAID AGREEMENT HAS BEEN ASSIGNED TO MANHATTAN ORE HOLDING COMPANY, INC. BY UNRECORDED ASSIGNMENTS NOT APPEARING IN THE PUBLIC RECORD.

THE INTEREST OF MANHATTAN QRE HOLDING COMPANY, INC. UNDER SAID AGREEMENT HAS BEEN ASSIGNED TO MANHATTAN VILLAGE, LLC, A CALIFORNIA LIMITED LIABILITY COMPANY BY ASSIGNMENT DATED AUGUST 19, 1997 AND RECORDED AUGUST 20, 1997 AS INSTRUMENT NO. 97-1291551 OF OFFICIAL RECORDS.

THE EASEMENT RIGHTS OF MANHATTAN VILLAGE, LLC, A CALIFORNIA LIMITED LIABILITY COMPANY UNDER SAID AGREEMENT HAVE BEEN CONVEYED TO MADISON MANHATTAN VILLAGE L.P., A DELAWARE LIMITED PARTNERSHIP BY DEED RECORDED OCTOBER 3, 2000 AS INSTRUMENT NO. 00-1548302 OF OFFICIAL RECORDS.

THE INTEREST OF MADISON MANHATTAN VILLAGE L.P., A DELAWARE LIMITED PARTNERSHIP UNDER SAID AGREEMENT HAS BEEN ASSIGNED TO MADISON MANHATTAN VILLAGE, LLC, A DELAWARE LIMITED LIABILITY COMPANY BY ASSIGNMENT DATED JUNE 28, 2002 AND RECORDED JULY 8, 2002 AS INSTRUMENT NO. 02-1536001 OF OFFICIAL RECORDS.

THE INTEREST OF MADISON MANHATTAN VILLAGE, LLC HAS BEEN ASSIGNED OF RECORD TO RREEF AMERICA REIT II CORP. BBB, A MARYLAND CORPORATION, BY AN ASSIGNMENT AND ASSUMPTION OF GROUND LEASE RECORDED MAY 5, 2004 AS INSTRUMENT NO. 04-1123082 OF OFFICIAL RECORDS.

PARCEL 10:

PARCEL 12, IN THE CITY OF MANHATTAN BEACH, COUNTY OF LOS ANGELES, STATE OF CALIFORNIA,

AS SHOWN ON PARCEL MAP NO. 12219, FILED IN BOOK 122, PAGES 33 TO 35 INCLUSIVE OF PARCEL MAPS, IN THE OFFICE OF THE COUNTY RECORDER OF SAID COUNTY.

PARCEL 10A:

AN EASEMENT FOR THE MAINTENANCE AND USE OF A GASOLINE DOCK AND UNDERGROUND

GASOLINE STORAGE TANK OVER THAT PORTION OF PARCEL 23 IN THE CITY OF MANHATTAN BEACH, COUNTY OF LOS ANGELES, STATE OF CALIFORNIA, AS SHOWN ON A MAP FILED IN BOOK 122, PAGES 33 THROUGH 35 INCLUSIVE OF PARCEL MAPS, IN THE OFFICE OF THE COUNTY RECORDER OF LOS ANGELES COUNTY, CALIFORNIA, DESCRIBED AS FOLLOWS:

City of Manhattan Beach Environmental Information Form
And Master Application Form
Manhattan Village Shopping Center MUP Amendment

BEGINNING AT A PORTION ON THE WESTERLY LINE OF PARCEL 12 AS SHOWN ON SAID PARCEL MAP, DISTANT THEREON SOUTH 00°03'57" EAST 90.35 FEET FROM THE NORTHWESTERLY CORNER OF SAID PARCEL; THENCE NORTH 89° 56' 03" EAST 49.29 FEET TO A NON-TANGENT CURVE CONCAVE

SOUTHEASTERLY HAVING A RADIUS OF 240.00 FEET, A RADIAL TO SAID POINT BEARS NORTH 70°09'15" WEST; THENCE NORTHEASTERLY ALONG THE ARC OF SAID CURVE THROUGH A CENTRAL ANGLE OF 18°46'11" AN ARC DISTANCE OF 78.62 FEET; THENCE ALONG THE NORTHWESTERLY PROLONGATION OF A LINE RADIAL TO SAID CURVE NORTH 51° 23'04" WEST 43.89 FEET; THENCE SOUTH 52°39'30" WEST 67.02 FEET TO THE WESTERLY LINE OF SAID PARCEL 12; THENCE ALONG SAID WESTERLY LINE SOUTH 00°03'57" EAST 55.10 FEET, MORE OR LESS, TO THE POINT OF BEGINNING.

EXCEPT ALL OIL, GAS AND OTHER HYDROCARBONS, GEOTHERMAL RESOURCES AS DEFINED IN SECTION 6903 OF THE CALIFORNIA PUBLIC RESOURCES CODE AND ALL OTHER MINERALS, WHETHER SIMILAR TO THOSE HEREIN SPECIFIED OR NOT, WITHIN OR THAT MAY BE PRODUCED FROM THE PROPERTY; PROVIDED, HOWEVER, THAT ALL RIGHTS AND INTEREST IN THE SURFACE OF THE PROPERTY ARE HEREBY CONVEYED TO GRANTEE, NO RIGHTS OR INTEREST OF ANY KIND THEREIN, EXPRESS OR IMPLIED, BEING EXCEPTED OR RESERVED TO GRANTOR EXCEPT AS HEREINAFTER EXPRESSLY SET FORTH THEREIN.

ALSO EXCEPT, THE SOLE AND EXCLUSIVE RIGHT FROM TIME TO TIME TO DRILL AND MAINTAIN WELLS OR OTHER WORKS INTO OR THROUGH THE PROPERTY BELOW A DEPTH OF 500 FEET AND TO PRODUCE, INJECT, STORE AND REMOVE FROM OR THROUGH SUCH WELLS OR WORKS, OIL, GAS, AND OTHER SUBSTANCES OF WHATEVER NATURE, INCLUDING THE RIGHT TO PERFORM ANY AND ALL OPERATIONS DEEMED BY GRANTOR NECESSARY OR CONVENIENT FOR THE EXERCISE OF SUCH RIGHTS, AS RESERVED BY CHEVRON U.S.A. INC., A CALIFORNIA CORPORATION, RECORDED APRIL 19, 1979 AS INSTRUMENT NO. 79-424732, OFFICIAL RECORDS, AS TO THAT PORTION OF SAID LAND ACQUIRED BY DEED RECORDED APRIL 2, 1923 IN BOOK 1993, PAGE 351, OFFICIAL RECORDS.

PARCEL 10B:

THE NON-EXCLUSIVE EASEMENTS FOR INGRESS, EGRESS, PARKING, UTILITIES AND CONSTRUCTION IN, TO, OVER, UNDER AND ACROSS THE "COMMON AREA" ALL AS DESCRIBED AND SHOWN IN THAT CERTAIN CONSTRUCTION, OPERATION AND RECIPROCAL EASEMENT AGREEMENT DATED NOVEMBER 1, 1980, EXECUTED BY MANHATTAN BEACH COMMERCIAL PROPERTIES, A GENERAL PARTNERSHIP, MANHATTAN HACIENDA PROPERTY CO., A GENERAL PARTNERSHIP AND FEDERATED DEPARTMENTS STORES, INC., A DELAWARE CORPORATION, WITH ADDENDUM EXECUTED BY BUFFUMS, INC., A CALIFORNIA CORPORATION, RECORDED ON NOVEMBER 25, 1980 AS INSTRUMENT NO. 80-1188655 OFFICIAL RECORDS.

PARCEL 10C:

THE NON-EXCLUSIVE EASEMENT FOR VEHICULAR AND PEDESTRIAN ACCESS AND PARKING OVER THAT PORTION OF PARCEL 23 IN THE CITY OF MANHATTAN BEACH, COUNTY OF LOS ANGELES, STATE OF CALIFORNIA, AS SHOWN ON A MAP FILED IN BOOK 122, PAGES 33 THROUGH 35 INCLUSIVE OF PARCEL MAPS, IN THE OFFICE OF THE COUNTY RECORDER OF LOS ANGELES COUNTY, CALIFORNIA, AS DESCRIBED IN THAT CERTAIN GRANT DEED AND GRANT OF EASEMENT WITH COVENANTS RUNNING WITH THE LAND RECORDED NOVEMBER 25, 1980 AS INSTRUMENT NO. 80-1188654, OFFICIAL RECORDS.

ASSESSOR'S PARCEL NUMBER: 4138-020-014

City of Manhattan Beach Environmental Information Form
And Master Application Form
Manhattan Village Shopping Center MUP Amendment

PARCEL 11:

PARCEL 10, IN THE CITY OF MANHATTAN BEACH, COUNTY OF LOS ANGELES, STATE OF CALIFORNIA, AS SHOWN ON PARCEL MAP NO. 12219, FILED IN BOOK 122 PAGES 33 THROUGH 35 INCLUSIVE OF PARCEL MAPS, IN THE OFFICE OF THE COUNTY RECORDER OF SAID COUNTY.

TOGETHER WITH THAT PORTION OF PARCEL 23 OF SAID PARCEL MAP NO. 12219, BEING DESCRIBED AS FOLLOWS:

BEGINNING AT THE NORTHEAST CORNER OF SAID PARCEL 10, THENCE, ALONG THE NORTHERLY LINE OF SAID PARCEL 10, S89°56'03"W 49.81 FEET TO THE TRUE POINT OF BEGINNING;

THENCE CONTINUING ALONG SAID NORTHERLY LINE OF SAID PARCEL 10, S89°56'03"W 266.25 FEET;

THENCE N0°05'28"W 108.98 FEET;

THENCE N89°54'32"E 266.25 FEET;

THENCE S0°05'28"E 109.09 FEET TO THE TRUE POINT OF BEGINNING.

EXCEPTING THEREFROM THAT PORTION OF SAID PARCEL 10 LYING WESTERLY OF THE FOLLOWING DESCRIBED LINE:

BEGINNING AT THE NORTHEAST CORNER OF SAID PARCEL 10,

THENCE, ALONG THE NORTHERLY LINE OF SAID PARCEL 10, S89°56'03"W, 316.06 FEET TO THE TRUE POINT OF BEGINNING;

THENCE S0°05'28"E 40.07 FEET;

THENCE N90°00'00"E 6.52 FEET;

THENCE S0°01'30"E 64.00 FEET;

THENCE N90°00'00"W 5.00 FEET;

THENCE S0°01'30"E 35.00 FEET;

THENCE N90°00'00"E 5.00 FEET;

THENCE S0°01'30"E 78.36 FEET;

THENCE S45°27'45"E 24.30 FEET TO THE SOUTHERLY LINE OF SAID PARCEL 10.

SAID LAND IS SHOWN AS PARCEL 2 IN THAT CERTIFICATE OF LOT LINE ADJUSTMENT RECORDED ON JUNE 21, 2018 INSTRUMENT NO. 623215 AND AUGUST 2, 2018 INSTRUMENT NO. 779112.

PARCEL 11A:

A NON-EXCLUSIVE EASEMENT, OVER PORTIONS OF THE COMMON AREA OF THE RESPECTIVE OWNER'S "TRACTS", AS SAID "TRACTS" ARE DEFINED IN THAT CERTAIN INSTRUMENT ENTITLED "CONSTRUCTION, OPERATION AND RECIPROCAL EASEMENT AGREEMENT" RECORDED

**City of Manhattan Beach Environmental Information Form
And Master Application Form
Manhattan Village Shopping Center MUP Amendment**

NOVEMBER 25, 1980 AS INSTRUMENT NO. 80-1188655 OFFICIAL RECORDS, FOR THE PASSAGE AND ACCOMMODATION OF PEDESTRIANS AND VEHICLES, SANITARY SEWERS, STORM DRAINS, UNDERGROUND CONDUITS, AND FOR DEVELOPMENT AND CONSTRUCTION THEREOF.

PARCEL 11B: (EASEMENT ARTICLE I)

A NON-EXCLUSIVE EASEMENT FOR VEHICULAR AND PEDESTRIAN ACCESS, PARKING, DRIVEWAYS, SIGNS, UTILITIES, LOADING AND UNLOADING, AND OTHER USES AS MORE PARTICULARLY DESCRIBED IN DEED FROM MANHATTAN BEACH COMMERCIAL PROPERTIES, A GENERAL PARTNERSHIP, RECORDED NOVEMBER 25, 1980 AS INSTRUMENT NO. 80-1188654, OFFICIAL RECORDS, OVER THE LAND DESCRIBED THEREIN.

PARCEL 11C: (EASEMENT ARTICLE II)

A NON-EXCLUSIVE EASEMENT FOR INGRESS AND EGRESS INCLUDING VEHICULAR AND PEDESTRIAN TRAFFIC, UTILITIES, CONDUITS, CURBS, GUTTERS, DRIVEWAYS, LIGHT STANDARDS, OVER THE LAND DESCRIBED IN A GRANT DEED AND GRANT OF EASEMENTS WITH COVENANTS RUNNING WITH THE LAND RECORDED NOVEMBER 25, 1980 AS INSTRUMENT NO. 80-1188654.

APN: 4138-020-059

PARCEL 12:

PARCEL 4 OF PARCEL MAP NO. 12010, IN THE CITY OF MANHATTAN BEACH, COUNTY OF LOS ANGELES, STATE OF CALIFORNIA, AS PER MAP FILED IN BOOK 116, PAGES 75-76 IN THE OFFICE OF THE COUNTY RECORDER OF SAID COUNTY.

APN: 4138-018-024

CITY COUNCIL RESOLUTION NO. 14-0026

A RESOLUTION OF THE MANHATTAN BEACH CITY COUNCIL APPROVING A MASTER USE PERMIT AMENDMENT, HEIGHT VARIANCE, AND SIGN EXCEPTION/PROGRAM FOR THE REMODELING AND EXPANSION OF A PORTION OF THE MANHATTAN VILLAGE SHOPPING CENTER LOCATED AT 2600 THROUGH 3600 SEPULVEDA BOULEVARD AND 1220 ROSECRANS AVENUE (RREEF AMERICA REIT CORP BBB II)

THE MANHATTAN BEACH CITY COUNCIL HEREBY RESOLVES, DETERMINES AND FINDS AS FOLLOWS:

SECTION 1. On November 7, 2006, RREEF America Reit Corp BBB II ("RREEF" hereinafter) applied for land use entitlements for improvements (the "Project") to an 18.4-acre portion (the "site") of the 44-acre Manhattan Village shopping center ("Shopping Center") located at 3200 – 3600 North Sepulveda Boulevard, Manhattan Beach. RREEF seeks to: construct new retail and restaurant gross leasable area and three parking structures; reconfigure existing surface parking areas; and install signs to identify and advertise the businesses within Shopping Center. The Manhattan Beach Municipal Code ("MBMC" or "Code") requires an amendment to the existing Master Use Permit, a height variance, and an amendment/exception to the existing Master Sign Program to permit the application.

SECTION 2. The site is designated "Manhattan Village and General Commercial" in the Land Use Element of the City's General Plan and, with the exception of the 3.6 acres northwest corner, is zoned Community Commercial (CC). The subject property is surrounded by a mixture of commercial, residential and senior housing uses.

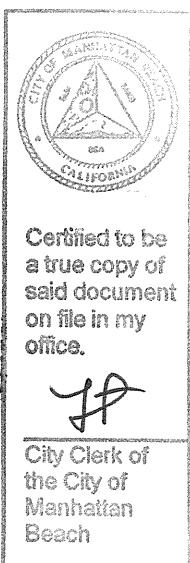
SECTION 3. Three property owners own a portion of the site: (a) 3500 Sepulveda LLC ("3500 Sepulveda" hereinafter) owns 0.7 acres where the Hacienda building is located; (b) Bullocks Properties Corp. ("Macy's" hereinafter) owns 1.9 acres where Macy's main department store is located; and (c) RREEF owns the balance of the site.

SECTION 4. Since 2006, RREEF and its team of consultants have met with neighbors, tenants, adjacent property owners, staff, and community leaders to review the proposed Project and to make revisions to address concerns, as well as the needs of a changing consumer market.

SECTION 5. After conducting duly noticed public hearings on the Project on June 27, 2012, October 3, 2012, March 13, 2013, April 24, 2013, May 22, 2013, June 26, 2013 and July 24, 2013, and requiring changes to the Project, the Planning Commission certified the EIR on June 26, 2013 and approved the Project, as modified by the Commission, on July 24, 2013.

SECTION 6. On August 6, 2013, 3500 Sepulveda appealed the Commission's approval of the Project, asserting that the Commission did not make "all of the required findings, the findings are not supported with sufficient evidence and the conditions of approval are insufficient." In addition, RREEF filed an "appeal in part" "to preserve administrative remedies related to specific "Conditions of Approval."

SECTION 7. On September 3, 10, and 17, October 8 and November 12, 2013, the City Council held duly noticed public hearings *de novo* to consider RREEF's application for an amendment to the existing Master Use Permit, a height variance, and amendment to the Master Sign program/sign exceptions. In addition, the Council held duly noticed public meetings on August 6, 2013 and January 14, 2014 to consider



the application. Evidence, both written and oral, was presented to the Council. All persons wishing to address the City Council regarding the Project were given an opportunity to do so at the public hearings. Representatives of RREEF and Macy's, residents and local business owners spoke in favor of the Project. Representatives of 3500 Sepulveda LLC and other persons spoke in opposition to the Project on various grounds.

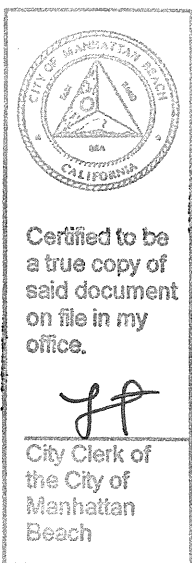
SECTION 8. On January 14, 2014, the City Council provided another opportunity for representatives of RREEF and 3500 Sepulveda LLC, and all other interested persons, to comment on the Project. After providing that opportunity, the Council adopted a motion to direct staff to draft resolutions for the Council to consider certifying the Environmental Impact Report ("EIR") and approving Phases I and II of the proposed Project, subject to requiring:

- A. Coordination of Phases I and II to ensure that Macy's is consolidated.
- B. Elimination of 10,000 square feet from Phase 1.
- C. Redesign of the Phase I "North Parking Structure."
- D. Consolidation of Macy's prior to the issuance of building permits for Phase II.
- E. Submittal by Macy's of a commitment letter.
- F. Installation of the Cedar Way extension to Rosecrans Avenue as part of Phase II.
- G. Negotiations in good faith with Fry's so it remains on the site.
- H. Provision of a bond or other satisfactory security for traffic improvements.
- I. The architectural elements, details, water features, landscaping, hardscaping, and plaza to be similar to the concept renderings.
- J. Commissioning of an Oak Avenue traffic study for a cost not to exceed \$20,000.
- K. Compliance with all of the other conditions that were imposed and previously approved by the Planning Commission.

SECTION 9. In accordance with the Council's motion, RREEF refined and modified the Project and submitted revisions to the Project plans. Such revisions were attached to the May 20, 2014 staff report as Attachment 9. The matrix comparing (a) the Project as analyzed by the EIR to (b) the revisions to the plan reflecting the modifications and refinements requested by the Planning Commission and the City Council was attached to the May 20, 2014 staff report as Attachment 3.

SECTION 10. The City's independent environmental consultant Matrix Environmental ("Matrix") and independent traffic consultant Gibson Transportation Consulting, Inc. ("GTC") have reviewed the revisions to the plans. In close consultation with GTC, Matrix has prepared a comparative environmental analysis, entitled, "Analysis of Proposed Modifications to the Manhattan Village Shopping Center Improvement Project," dated April 2014 ("April 2014 Analysis"). Such analysis is in the Final EIR, Volume II. The analysis concluded that the refined and modified Project would not result in greater impacts than were identified for the Project as originally analyzed in the EIR, and that all of the potential environmental impacts associated with the proposed modifications are within the scope of the potential impacts already evaluated in the EIR. It also recommended that only two Mitigation Measures be modified due to the refinements and modifications. Thus, no new impacts have been identified; two mitigation measures have been slightly revised; and no new mitigation measures are required for implementation of the refined and modified Project.

SECTION 11. The City Council held a public hearing on April 29, 2014 to review the refinements and modifications to the Project, the April 2014 Analysis, the draft resolutions and the proposed conditions of approval. All persons wishing to address the City Council regarding the Project, including representatives of RREEF and 3500 Sepulveda, were given an opportunity to do so at the public hearing. The City Council invited public comment on, *inter alia*, the refined and modified Project, the draft resolutions and the draft conditions of approval. The City invited representatives



of 3500 Sepulveda to provide comments. Principal Mark Neumann and two attorneys spoke for over thirty minutes and presented two letters and a slide show presentation. Mr. Neumann emphasized that he was trying to protect 3500 Sepulveda's property rights. After the conclusion of the public testimony, the City Council closed the public testimony portion of the public hearing, and continued the hearing to May 20, 2014.

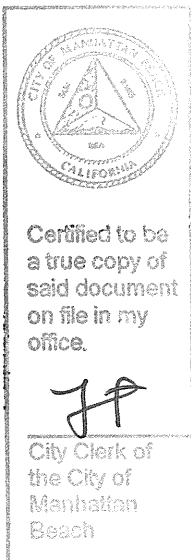
SECTION 12. On May 20, 2014, the City Council provided another opportunity for the public, including representatives of 3500 Sepulveda, to comment on the draft resolutions and the conditions attached to Resolution 14-0026. After the public provided comments, the Council made a motion to return with resolutions to certify the EIR and to approve the project, subject to all the conditions in the draft resolution and additional conditions.

SECTION 13. On December 2, 2014, the City Council provided another opportunity for the public, including representatives of 3500 Sepulveda to comment on the draft resolutions and the conditions attached to Resolution 14-0026. After that opportunity, the City Council adopted Resolution 14-0025, thereby: (1) certifying the Final EIR; (2) making findings in support thereof; and (3) adopting a Mitigation Monitoring and Reporting Program for the Project, as refined and modified. Resolution 14-0025 is hereby incorporated herein as if set forth in full.

SECTION 14. Based upon substantial evidence in the record of the above-mentioned proceedings and pursuant to Manhattan Beach Municipal Code ("MBMC") Section 10.84.060A, the City Council finds:

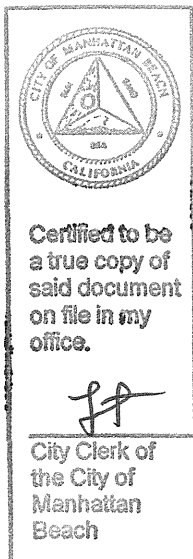
1. The proposed location of the use is in accord with the objectives of this title and the purposes of the district in which the site is located;

- a. The site is located within Area District II and, with the exception of the northwest corner described below, is zoned Community Commercial (CC). The purpose of the CC zoning district is to provide sites for planned commercial centers which contain a wide variety of commercial establishments, including businesses selling home furnishings, apparel, durable goods and specialty items generally having a city-wide market area. Support facilities such as entertainment and eating and drinking establishments are permitted, subject to certain limitations to avoid adverse effects on adjacent uses. The northwest corner of the site (3.6 Acres Fry's site) is zoned General Commercial (CG). The portion of the application relating to that corner is part of the proposed Phase III. The City is not approving Phase III at this time.
- b. As described below, the Project is consistent with the purpose of the CC zone.
 - i. As conditioned to ensure the expansion of the anchor tenant spaces in Phase II and to promote the opportunity for an additional anchor tenant, the project will improve the viability of a wide variety of uses, such as retail, services, restaurants, grocery store, banks and offices will continue to be provided on the site.
 - ii. This wide variety of uses will expand the existing type of services already provided on the site, while providing more diversity and options for the customer.
 - iii. As conditioned to ensure the expansion of the anchor tenant spaces in Phase II and to promote the opportunity for an additional anchor tenant, the Project will aid in attracting and maintaining a diverse mix of high-quality tenants to provide a broad range of shopping and dining



- options with enhanced amenities to serve the needs of the community and ensure the continued success of the shopping center.
- iv. Bars, convenience stores, gyms, liquor stores and similar uses will not be allowed as the traffic and/or parking demands for those uses would exceed the on-site capacity, which could cause adverse impacts on adjacent uses and the surrounding street systems.
- v. Restaurants (eating and drinking establishments) will be limited in square footage. Exceeding 89,000 square feet will increase the parking demand and will exceed the on-site capacity, which could cause adverse impacts on adjacent uses and the surrounding street systems. Thus, the maximum amount of square footage allowed for restaurant uses is 89,000 square feet.
- vi. Medical and Dental offices will be limited in square footage. Exceeding 28,800 square feet (7,000 square feet above the existing square footage) would increase the parking demand and would exceed the on-site capacity, which could cause adverse impacts on the site, adjacent uses and the surrounding street systems. Thus, the maximum amount of square footage allowed for medical and dental offices is 28,800 square feet.
- c. As described below, the proposed location is consistent with the purposes of the Commercial Districts, as stated in MBMC Section 10.16.010.

- i. **One of the purposes of the Commercial Districts is to provide appropriately located areas consistent with the General Plan for a full range of office, retail commercial, and service commercial uses needed by residents of, and visitors to, the City and region.** Given the combination of uses expected to be included in the Project, including expanded commercial center anchor tenants, high-end retail, and restaurant uses, the Project will continue to provide a full range of office, retail, service and other commercial uses on the site, and will expand those commercial opportunities. The proposed Project provides commercial opportunities for residents and visitors to the City, while also enhancing connections to the existing infrastructure such as the extension of Cedar Way.
- ii. **One of the purposes of the Commercial Districts is to strengthen the City's economic base, but also protect small businesses that serve City residents.** As conditioned to ensure the expansion of anchor tenant space and to promote the opportunity for an additional anchor tenant by consolidating the Macy's retail operation, the project will not be limited to the development of a smaller scale outdoor shopping experience that might compete with small businesses in the downtown commercial area. With the conditions to promote development of Phase II of the project, the project will maintain and enhance its character as a planned commercial center that offers a different and complementary shopping experience to downtown and therefore the project, as conditioned, protects small



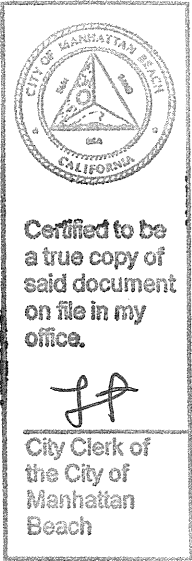
businesses that serve City residents. Without the conditions to ensure Phase II is constructed, the City Council could not make this finding.

iii. Due to the scale of the development, there is also an opportunity for retailers and other commercial users that require larger spaces which cannot be provided in the other smaller scale commercial areas in town. These retail uses will be encouraged by improving the strength of the anchor tenants as proposed in Phase II of the Project. Small businesses will continue to be provided in Downtown, the North End and other commercial areas with smaller sites. By improving the shopping experience, the enhanced shopping center is expected to strengthen the local economy and generate increased sales tax revenue.

iv. ***The purpose of the Commercial Districts also include the creation of suitable environments for various types of commercial and compatible residential uses, the protection of those uses from the adverse effects of inharmonious uses, and the minimization of impacts of commercial development on adjacent residential districts.*** As conditioned to ensure the expansion of the anchor tenant spaces in Phase II and to promote the opportunity for an additional anchor tenant, the project promotes the maintenance of a suitable environment for a planned commercial center that does not exist elsewhere in Manhattan Beach. There are no residential uses on the site. In addition, the residential uses in close proximity are protected with conditions related to traffic and circulation, parking, lighting, landscaping, land uses, and building scale and design. For example, the height of the above-grade parking lots has been scaled back and will be buffered by mature landscaping. In addition, the circulation plan encourages traffic to enter and exit from Rosecrans and Sepulveda. The Project's pedestrian and bicycle improvements will create improved linkages internally and to the surrounding community.

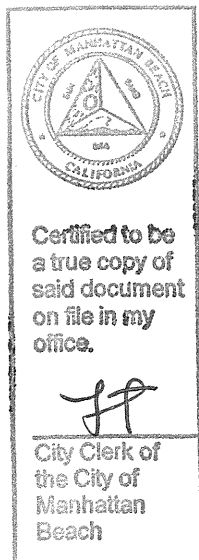
v. ***One of the purposes of the Commercial Districts is to ensure that the appearance and effects of commercial buildings and uses are harmonious with the character of the area in which they are located.*** The architectural style and design features will be compatible with the existing shopping center site, because the proposed additions are intended to mesh seamlessly with existing structures while also updating the aesthetic by providing contemporary architecture. The buildings are consistent in height with the existing buildings, and the parking structures are architecturally designed to reflect the rhythm and design features of the commercial buildings. The design also seeks to minimize the scale of the buildings to fit the scale of the surrounding area.

vi. ***One of the purposes of the Commercial Districts is to ensure the provision of adequate off-street parking and loading facilities.*** The Project will provide parking at a ratio of 4.1 spaces per 1,000 square feet consistent with the parking demand study, based on the mix of uses on the site. Uses with high parking demand will be limited in square footage (restaurants and Medical/Dental offices)



and some uses will be prohibited due to the high parking demand (gyms, trade schools, liquor stores, etc.). Loading facilities shall be located in close proximity to stores, and shall be adequate in size and number.

- d. The proposed Project and future tenant improvements to the remainder of the site will be consistent with each of the eleven development criteria outlined in the Sepulveda Boulevard Development Guide, as conditioned, specifically:
 - i. **Reciprocal Access**—Circulation within and off the shopping center site, including vehicular, bicycle, pedestrian and transit will be integrated and connected.
 - ii. **Right-turn Pockets**—Right-turn pockets shall be provided internally throughout the shopping center site. Dedication on Sepulveda Boulevard near Rosecrans Avenue will bring the area up to current ADA and other standards, improve pedestrian circulation, provide an improved deceleration lane per Caltrans requirements for the possible retention of the Fry's Sepulveda Boulevard driveway (3600 Sepulveda Blvd) as a right-turn entry only, provide for a right-turn/deceleration lane at 33rd Street, and allow the future Sepulveda bridge widening to function effectively.
 - iii. **Driveway Throats**—Driveway throats will minimize traffic and circulation impacts to Sepulveda Boulevard and allow the bridge widening to function effectively, Sepulveda Blvd driveway access will be modified on the Fry's site.
 - iv. **Sidewalk Dedication**—Sidewalk dedication and related improvements on Sepulveda Boulevard will bring the area up to current ADA and other standards and improve pedestrian circulation.
 - v. **Building Orientation**—The Sepulveda Boulevard and Rosecrans Avenue other improvements will be designed as an architectural entry statement to emphasize the importance of this key corner Gateway into the City.
 - vi. **Visual Aesthetics**—Review of architectural plans is required, including material boards, samples, renderings, and assurance that there is a high quality of design and materials as reflected in the concept plans. The site plan and layout of the buildings and parking structures provide landscaping and architectural features along Sepulveda Boulevard.
 - vii. **Residential Nuisances**—Residential nuisances will be minimized through Project design and conditions related to lighting, landscaping, traffic, multi-modal transportation, design, and allowed land uses.
 - viii. **Pedestrian Access**—Pedestrian access will be encouraged with strong on- and off-site linkages, a



network that connects to transit, under the Sepulveda bridge, as well as a village pedestrian-oriented design.

- ix. **Landscaping**—Mature shade trees and other landscaping will soften and complement the buildings, provide shade for parking, and screen, buffer and soften uses.
- x. **Signs**—There shall be no harsh light, blinking, moving, or flashing signs, consistent with the scale of the development, comprehensive site-wide consistent plan, complementary to the site and building architecture, and removal of obsolete and outdated pole signs.
- xi. **Utility Undergrounding**—Utility undergrounding will be required for all new construction.

2. ***The proposed location of the use and the proposed conditions under which it would be operated or maintained will be consistent with the General Plan; will not be detrimental to the public health, safety or welfare of persons residing or working on the proposed Project site or in or adjacent to the neighborhood of such use; and will not be detrimental to properties or improvements in the vicinity or to the general welfare of the city;***

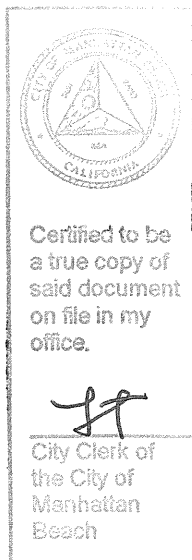
- a. The Project is consistent with the following Goals and Policies of the General Plan: A summary of the reasons for consistency are provided for each of the five categories.

Land Use

The primary purpose of the project is to improve the site to support the remodeling and upgrading needs of businesses within the regional serving commercial center and maintain its viability. As conditioned to ensure the expansion of the anchor tenant spaces in Phase II and to promote the opportunity for an additional anchor tenant, the project ensures that the Shopping Center will maintain its viability as a regional serving shopping district pursuant to General Plan Land Use Goal 8 and, as conditioned to promote the expansion of the anchor tenants, the project will preserve and enhance the features of a planned commercial center, thereby preserving the unique features of this commercial neighborhood and not intruding on the unique features of other commercial neighborhoods.

The MVSC enhancements will also provide visually interesting architecture, constructed with quality materials that facilitate a diverse mix of uses and services that residents and patrons can enjoy year round. The buildings and open spaces are designed to create hubs of activity that are mindful of resource usage such as landscape placement and create community gathering places worthy of Manhattan Beach.

Design and operational project components regarding noise, lighting, signage, odors, parking, architectural articulation, and circulation are consistent with the Sepulveda Development Guide and are either a part of the project description or the subject of conditions of approval to limit any potential impacts.



The design of the shopping center utilizes buffer zones, appropriately located uses, and smart site planning to ensure compatibility with surrounding land uses. Buildings are clustered together to create pedestrian-dominant areas with private landscaped open space and parking decks have been distributed to provide parking adjacent to uses allowing patrons to park once and walk to multiple destinations. The shopping center expansion has been designed to provide a wide range of lease depths, square footages, and locations to encourage both national retailers as well as local business owners to locate within the Project. Enhanced bike and pedestrian paths are proposed to encourage alternative transportation and clearly delineate their respective areas and alert vehicles that they are sharing the roads.

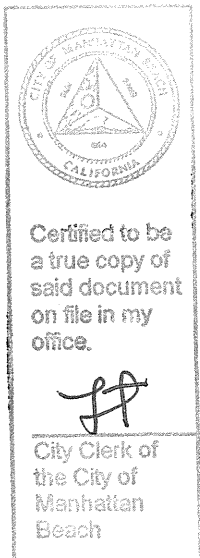
- Policy LU-1.2: Require the design of all new construction to utilize notches, balconies, rooflines, open space, setbacks, landscaping, or other architectural details to reduce the bulk of buildings and to add visual interest to the streetscape.
- Goal LU-2: Encourage the provision and retention of private landscaped open space.
- Goal LU-2.3: Protect existing mature trees throughout the City, and encourage their replacement with specimen trees whenever they are lost or removed.
- Goal LU-3: Achieve a strong, positive community aesthetic.
- Policy LU-3.1: Continue to encourage quality design in all new construction.
- Policy LU-3.2: Promote the use of adopted design guidelines for new construction in Downtown, along Sepulveda Boulevard, and other areas to which guidelines apply.
- Goal LU-4: Preserve the features of each community neighborhood, and develop solutions tailored to each neighborhood's unique characteristics.
- Goal LU-6: Maintain the viability of the commercial areas of Manhattan Beach.
- Policy LU-6.2: Encourage a diverse mix of businesses that support the local tax base, are beneficial to residents, and support the economic needs of the community.
- Policy LU-6.3: Recognize the need for a variety of commercial development types and designate areas appropriate for each. Encourage development proposals that meet the intent of these designations.
- Goal LU-8: Maintain Sepulveda Boulevard, Rosecrans Avenue, and the commercial areas of Manhattan Village as regional-serving commercial districts.
- Policy LU-8.2: Support the remodeling and upgrading needs of businesses as appropriate within these regional serving commercial districts.

Infrastructure

The Project includes significant upgrades to either maintain or improve the supporting infrastructure and utility systems and provides solutions that: facilitate circulation for pedestrians, bicyclists, mass transit riders and cars; treat storm water run-off on-site to the degree feasible; and manage the frequency and location of cars and service trucks during both construction and operation of the shopping center.

A significant number of on- and off-site improvements will result in significantly improved on- and off-site traffic circulation and parking. The project unites the Fry's and other shopping center parcels and improves traffic circulation for cars, bikes and pedestrians. Caltrans has been consulted to coordinate the Sepulveda bridge widening project.

Bio-filtration will be used to avoid potential contamination of run-off due to the

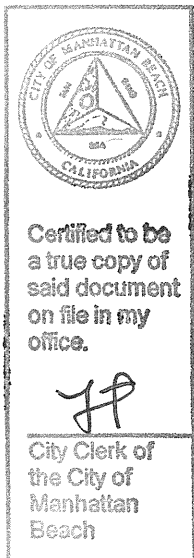


existence of the underlying hydrocarbon contamination and achieve clean storm water run-off prior to reaching the public storm drain system.

The shopping center site currently exceeds the code minimum percentage of landscape and the proposed Project will also provide a higher percentage than required.

Best Management Practices (BMPs) will be used during construction to reduce soil loss, sedimentation and dust/particulate matter air pollution. The Construction Parking Plan will take into account parking for patrons, employees as well as construction vehicles and construction buffer areas. Parking counts will be monitored to ensure appropriate ratios are maintained throughout all phases of construction.

- Goal I-1 Provide a balanced transportation system that allows the safe and efficient movement of people, goods and services throughout the City.
- Policy I-1.9: Require property owners, at the time of new construction or substantial remodeling, dedicate land for roadway or other public improvements, as appropriate and warranted by the Project.
- Policy I-1.12: Monitor and minimize traffic issues associated with construction activities.
- Policy I-2.4: Require additional traffic lanes and/or other traffic improvements for ingress and egress for new development along arterials where necessary for traffic and safety reasons.
- Policy I-2.7: Monitor and minimize traffic issues associated with construction activities.
- Goal I-3: Ensure that adequate parking and loading facilities are available to support both residential and commercial needs.
- Policy I-3.4: Review development proposals to ensure potential adverse parking impacts are minimized or avoided.
- Policy I-3.5: Encourage joint-use and off-site parking where appropriate.
- Policy I-3.8: Monitor and minimize parking issues associated with construction activities.
- Goal I-4: Protect residential neighborhoods from the adverse impacts of traffic and parking of adjacent non-residential uses.
- Policy I-4.2: Carefully review commercial development proposals with regard to planned ingress/egress, and enforce restrictions as approved.
- Policy I-4.3: Encourage provision of on-site parking for employees.
- Policy I-4.4: Ensure that required parking and loading spaces are available and maintained for parking.
- Goal I-6: Create well-marked pedestrian and bicycle networks that facilitate these modes of circulation.
- Policy I-6.6: Incorporate bikeways and pedestrian ways as part of the City's circulation system where safe and appropriate to do so.
- Policy I-6.7: Encourage features that accommodate the use of bicycles in the design of new development, as appropriate.
- Policy I-7.2: Ensure that all new development or expansion of existing facilities bears the cost of providing adequate water service to meet the increased demand which it generates.
- Policy I-8.2: Ensure that all new development or expansion of existing facilities bears the cost of expanding the sewage disposal system to handle the increased load, which they are expected to handle.
- Goal I-9: Maintain a storm drainage system that adequately protects the health and safety and property of Manhattan Beach residents.
- Policy I-9.2: Evaluate the impact of all new development and expansion of existing facilities on storm runoff, and ensure that the cost of upgrading existing drainage facilities to handle the additional runoff is paid for by the development which generates it.
- Policy I-9.3: Support the use of storm water runoff control measures that are



effective and economically feasible.

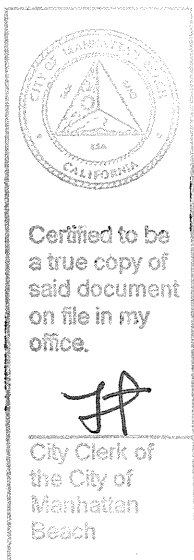
- Policy I-9.4: Encourage the use of site and landscape designs that minimize surface runoff by minimizing the use of concrete and maximizing the use of permeable surface materials.
- Policy I-9.5: Support appropriate storm water pollution mitigation measures.

Community Resources

RREEF has committed to build the project to a U.S. Green Building Council Leadership in Energy and Environmental Design (LEED) Silver standard, or equivalent, as required by the Municipal Code. Protection and enhancement of existing landscape and mature trees is a part of the project description. Extensive outreach has resulted in the proposed enhancement and promotion of alternative transportation to and from the shopping center site.

Additional sustainable and energy-efficient project components include potable water use reduction of at least 20%, Electrical Vehicle (EV) charging stations, reduction in the use of utilities, and minimized generation of non recyclable waste.

- Policy CR-4: Preserve the existing landscape resources in the City, and encourage the provision of additional landscaping.
- Policy CR-4.1: Protect existing mature trees throughout the City and encourage their replacement with specimen trees whenever they are lost or removed.
- Policy CR-4.3: Recognize that landscaping, and particularly trees, provide valuable protection against air pollution, noise, soil erosion, excessive heat, and water runoff, and that they promote a healthy environment.
- Policy CR-4.5: Discourage the reduction of landscaped open space and especially the removal of trees from public and private land.
- Policy CR-5.1: Employ principles of a sustainable environment in the development, operation, and maintenance of the community, emphasizing the importance of respecting and conserving the natural resources.
- Policy CR-5.3: Encourage water conservation, including landscaping with drought-tolerant plants, use of reclaimed water, and recycling of cooling system water, in all development.
- Policy CR-5.7: Encourage the use of energy-saving designs and devices in all new construction and reconstruction.
- Policy CR-5.8: Encourage utilization of “green” approaches to building design and construction, including use of environmentally friendly interior improvements.
- Policy CR-5.10: Encourage and support the use of alternative fuel vehicles, including support of charging or “fueling” facilities.
- Policy CR-5.11: Support sustainable building practices.
- Policy CR-6.1: Encourage alternative modes of transportation, such as walking, biking, and public transportation, to reduce emissions associated with automobile use.
- Policy CR-6.2: Encourage the expansion and retention of local serving retail businesses (e.g., restaurants, family medical offices, drug stores) to reduce the number and length of automobile trips to comparable services located in other jurisdictions.



Community Safety

Providing enhanced safety for shoppers and employees is a high priority for the Project. RREEF will continue to utilize its own private security force that works closely with the City Police Department. Regular patrols will continue, and will be

tailored to the new improvements.

Security cameras shall be installed throughout each of the new parking structures and the surface parking lots for added security and crime prevention. As conditioned, RREEF shall: (1) comply with City Fire Department requirements to insure that bridge heights, building heights and roadway widths allow emergency vehicle access safely throughout the Project site; and (2) provide adequate water distribution and ensure supply facilities have adequate capacity and reliability to supply both everyday and emergency fire-fighting needs. Response times for both Police and Fire will continue to meet or exceed current levels.

- Policy CS-1.3: Ensure that public and private water distribution and supply facilities have adequate capacity and reliability to supply both everyday and emergency fire-fighting needs.
- Policy CS-3: Maintain a high level of City emergency response services.
- Policy CS-3.7: Support the use of the best available equipment and facilities to ensure safety that meets the changing needs of the community.
- Policy CS-3.10: Strive to reduce emergency response time.
- Policy CS-4: Maintain a high level of police protection services.
- Policy CS-4.6: Support proactive measures to enhance public safety, such as use of increased foot or bicycle police patrols.
- Policy CS-4.7: Strive to reduce police response time.

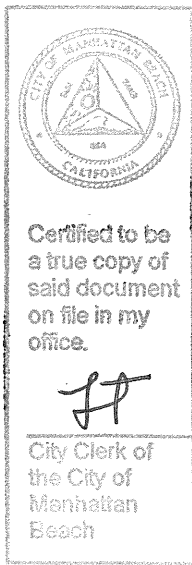
Noise Element

Measures are included to insure no unmitigated construction or operational impacts on surrounding commercial and residential receptors. Construction hours are limited, and construction is phased to minimize synergistic noise that could exceed codified standards. Buildings to be constructed along major arterials will be designed to meet reasonable interior noise levels.

Policy N-2.5: Require that the potential for noise be considered when approving new development to reduce the possibility of adverse effects related to noise generated by new development, as well as impacts from surrounding noise generators on the new development.

Policy N-3.6: Monitor and minimize noise impacts associated with construction activities on residential neighborhoods.

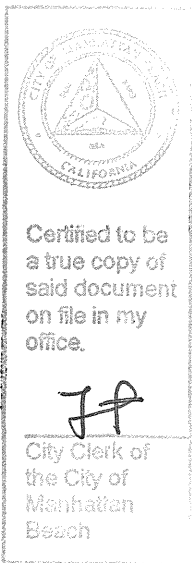
- b. The proposed location of the improvements and the proposed conditions under which it will be operated and maintained will not be detrimental to the public health, safety or welfare of persons residing or working on the proposed Project site or in or adjacent to the neighborhood of such use; and will not be detrimental to properties or improvements in the vicinity or to the general welfare of the City because:
 - i. The Project, as conditioned, including the construction and the on-going physical and operational upgrades associated with tenant improvements and redevelopment across the entire shopping center site, has been designed to minimize impacts to neighboring uses. The conditions of approval for the Project will ensure that the Project is not detrimental to persons or property.
 - ii. The features incorporated into the Project will ensure that there are no detrimental impacts. Such features include appropriate scale, layout, massing, articulation, height,



architectural design and details of the buildings, parking structures, lighting design, signage design, LEED sustainability features, as well as pedestrian, bike, and transit linkages all of which are intended to ensure compatibility with surrounding uses.

- iii. Green-building components addressing water conservation, increased energy efficiency, and pollution reduction are included in the Project description. LEED silver construction will be required.
- iv. The Project conditions will ensure that there are no detrimental impacts as a result of the following: lighting modifications, removal of obsolete pole signs, reduction of visual impact of parking structures, Project phasing, architectural detail review, land use compatibility, alcohol service and square footage limits, fire emergency response upgrades, improved security features, improved on- and off-site pedestrian, bike and transit linkages, parking management programs, traffic, parking and circulation improvements, trash enclosures improvements, and utility upgrades.
- v. The Project conditions will also ensure that there are no detrimental impacts through off-site improvements to the surrounding roadway network as the Project is surrounded on all three sides by arterial streets, including Sepulveda Boulevard and Rosecrans Avenue, the largest arterials in the City. Providing roadway dedication, improvements, and fair-share contributions will improve the regional roadway networks surrounding and servicing the Project site. The improvements will enhance safety, better accommodate emergency vehicles, improve flow of traffic, and improve the regional transportation network on surrounding arterials.
- vi. The conditions will be consistent with General Plan Infrastructure Goals and Policies that require the following:

- Provision of a balanced transportation system that allows the safe and efficient movement of people, goods, and services throughout the City;
- Dedication of land for roadway or other public improvements by property owners at the time of new construction or substantial remodeling, as appropriate and warranted by the Project;
- Upgrade of all major intersections and arterial streets to keep traffic moving efficiently;
- Addition of traffic lanes and/or other traffic improvements for ingress to and egress from new developments along arterials, where necessary, for traffic and safety reasons;
- Coordinate with the neighboring cities and regional and sub-regional agencies to widen and upgrade all major intersections and associated street segments within the City and adjacent jurisdictions to optimize traffic flows.



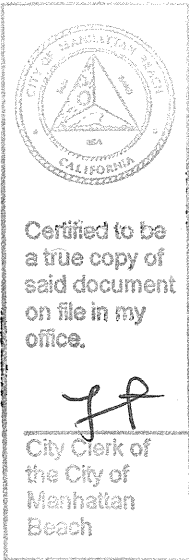
3. The proposed use will comply with the provisions of Manhattan Beach Municipal Code Title 10, including any specific condition

required for the proposed use in the district in which it would be located.

- a. Existing and proposed improvements within the site are, or will be, developed in accordance with the purpose and standards of the CC Zoning District. A variety of retail, restaurant, office, and specialty uses exist and are proposed to continue. Parking and landscaping will be provided at a rate above that required by the Municipal Code.
- b. A variety of commercial uses will be allowed, but limitations and prohibitions will be placed on certain uses to ensure that the Project complies with the intent and purpose of the Code.
- c. The Project and future improvements to the shopping center site will be consistent with each of the eleven Sepulveda Boulevard Development Guide development criteria, as previously outlined in this Resolution.
- d. Conditions of approval, including specifically conditions to ensure the construction of Phase II, which will include the expansion of anchor tenants, will ensure consistency with Municipal Code Section 10.16.010 that provides that the CC zone shall be for planned commercial centers and that entertainment and eating and drinking facilities shall be for support, not primary uses.

4. The proposed use will not adversely impact nor be adversely impacted by nearby properties. Potential impacts are related but not necessarily limited to: traffic, parking, noise, vibration, odors, resident security and personal safety, and aesthetics, or create demands exceeding the capacity of public services and facilities which cannot be mitigated.

- a. The Project will not result in adverse impacts to nearby properties because the Project, as conditioned herein, will be sensitive to nearby properties with respect to aesthetic design, site planning, building layout, and parking structures.
- b. The conditions of approval related to traffic, parking, noise, security, landscaping, lighting, signage, utilities, and other provisions will ensure that the Project will not adversely impact nearby properties.
- c. The Project will not be adversely impacted by nearby properties, as the surrounding land uses are commercial and residential and will not impact the site. The industrial land use – i.e., the Chevron Refinery in the City of El Segundo to the northwest of the site – is separated by two major arterial streets (Sepulveda Boulevard and Rosecrans Avenue) as well as a large landscaped berm. These features address any potential adverse impacts.
- d. Proposed lighting will produce minimal off-site illumination onto nearby residential properties while still accomplishing the goals of enhancing security, pedestrian and vehicular path of travel, and parking space illumination. Residentially-zoned properties are located more than 250 feet to the south and east of the nearest proposed parking deck light source. Residences to the west of Sepulveda Boulevard are approximately 200 feet from existing or proposed lighting in the Project area. Lighting also will be screened by mature vegetation, oblique orientation of buildings, light standards, LED fixtures with shielding and direct (not



dispersed) lighting patterns, as well as screening by existing buildings. Buffering also is achieved by the difference in ground elevation relative to the nearest residential properties. Project lighting is consistent with the Code standards which regulate lighting. Thus, the Project will not adversely impact, nor be adversely impacted by, nearby properties.

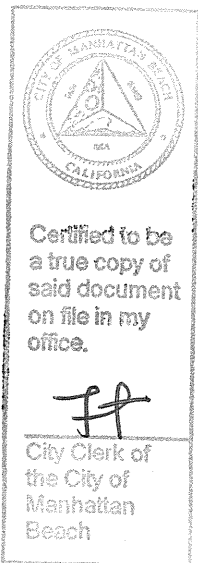
SECTION 14. RREEF has applied for a variance to permit certain structures in the Project to exceed the maximum height of 30 feet by a range of 2 to 26 feet (for required equipment). The Village shops buildings are proposed to be up to 32 feet in height and the Macy's Expansion building is proposed to be 42 feet in height to match and to maintain consistency with the height of the existing buildings that were entitled by a previous height variance. The South Parking Structure is proposed to be 26 feet high, with architectural features up to 32 feet, but it will not exceed the height of the surrounding buildings. The maximum height for the Northeast Parking Structure is 35 feet. The North Parking Structure will not exceed G+2 in height. Mechanical, elevator overruns, architectural features, parapets, and light fixtures on top of the parking structures are proposed to exceed the height limits, including the Building Safety required elevator overruns at up to 56 feet in height and the lights on top of the parking structures at 15 feet over the height of the top level of the parking decks. Based upon substantial evidence in the record and pursuant to MBMC Section 10.84.060B, the City Council finds:

1. ***Because of special circumstances or conditions applicable to the subject property—including topography, soil conditions, size, shape, location or surroundings--the strict application of height standards in the zoning ordinance deprives such property of privileges enjoyed by other property in the vicinity and under the same zoning and would result in undue hardships upon the owner of the property.***

a. The site has numerous special circumstances or conditions that would deprive the site of privileges enjoyed by other properties in the vicinity. The site is the largest commercial site in the City and suffers from severe topographic variation. The site is bisected by a deep culvert which presents design challenges in creating a unified development. The properties immediately to the east contain skyscrapers with heights that eclipse the height of the proposed Project. The existing buildings on the properties owned by 3500 Sepulveda and Macy's enjoy a height equal to or higher than the heights requested by RREEF.

b. The exceptional topographic variation deprives RREEF of the opportunity to integrate the new buildings into the site because the measurement of height is not made from grade adjacent to the building, but instead from a plane defined by the average elevation of the four corners of the site. Thus, due to the large size of the site and unlike any other property in the city, the allowable height of buildings is influenced by the elevation of grade that may be significantly lower and significantly different than the grade adjacent to the building. The existing buildings in the shopping center already exceed the height limit. Additionally, the Macy's expansion adds onto a building that exceeds the height limit and needs to match the height and floor plates of the existing two-story building.

c. The hydrocarbon soil contamination on the site limits the ability to construct subterranean space. Thus, the soil conditions deprive the property owner of the opportunity to develop below grade. Additional height compensates for the soil conditions by allowing

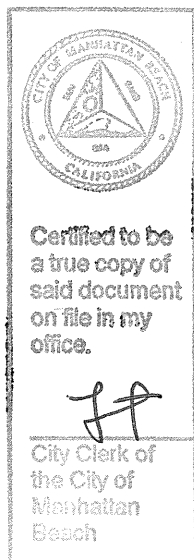


the property owner to develop above grade in order to receive the same privileges as property owners without similar soil conditions.

- d. In light of the topographic fluctuations, and the soil contamination, there are special circumstances and conditions on this property that would result in exceptional difficulties and hardships if the City were to apply the height restriction strictly.

2. The relief may be granted without substantial detriment to the public good; without substantial impairment of affected natural resources; and not be detrimental or injurious to property or improvements in the vicinity of the development site, or to the public health, safety or general welfare; and

- a. The granting of the variance to allow additional building height will not obstruct views from surrounding properties and is generally consistent with the height and massing of the existing shopping center structures.
- b. The site is situated in an area of the City that is fully developed and relatively devoid of natural resources. Project improvements will be conditioned to: meet LEED silver standards; include shade trees and electric vehicle charging facilities to increase energy efficiency; and protect natural resources by including storm water management measures. Most importantly, the height variance will not affect natural resources.
- c. The proposed height variance would not be detrimental or injurious to properties or improvements in the vicinity because the shade/shadow and visual impacts of the Project have been analyzed and will not have aesthetic impacts. The landscaping, screening, and architectural features have been designed to minimize visual impacts. Additionally, the rolling topography of Sepulveda Boulevard, Rosecrans Avenue, and Marine Avenue alleviates adverse impacts generally seen with increased building heights.
- d. The buildings over the height limit have relatively large setbacks from adjacent land uses, are adjacent to major arterial roadways, and will not create adverse light, shadow or massing impacts.
- e. The proposed structures that exceed the Code's height standards are setback more than 180 feet from Sepulveda Boulevard. The row of existing buildings between Sepulveda Boulevard and the proposed structures exceed the height limit. The proposed addition for the purpose of consolidating Macy's is more than 500 feet from Sepulveda Boulevard. All proposed buildings are more than 900 feet from Marine Avenue. The proposed Northeast Parking Structure will be the same height as the existing Medical building at 1220 Rosecrans, immediately adjacent to the east, is setback approximately 20 to 30 feet from Rosecrans Avenue, and the frontage on Rosecrans Avenue is limited and consistent with the surrounding buildings' mass, scale and height.
- f. The proposed heights of the proposed buildings are similar to existing heights of the Macy's and main mall buildings. The only features that exceed existing heights are a few 56-foot elevator overruns which have relatively small mass in comparison to the rest of each structure.



g. The high quality of design will attract new tenants and maintain a diverse and quality mix of tenants. It is not reasonably feasible to accomplish the Project without increasing the height envelopes of new development. Without these increases in the height envelopes, it is difficult to re-orient key parking, maintain or enhance vehicular, pedestrian and bicycle circulation, provide significant new landscaping, plaza areas, open space and upgrade the overall site. The additional height needed for the expansion Project is integral to the continuing improvement of the shopping center. Therefore, allowing the additional height will not result in substantial detriment to the public good, public health, safety or general welfare.

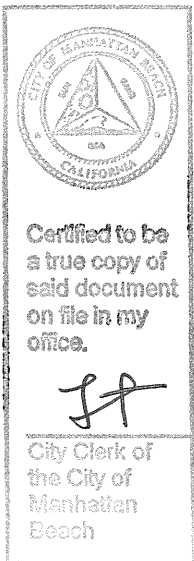
3. Granting the variance is consistent with the purposes of the Zoning Code and will not constitute a grant of special privilege inconsistent with limitations on other properties in the vicinity and in the same zoning district and area district. Further, conditions have been imposed as will assure that the adjustment hereby authorized shall not constitute a grant of special privileges inconsistent with the limitations upon other properties in the vicinity.

a. The additional height needed for the Project is integral to the continuing improvement of the mall in order to fulfill the purposes of the CC zone. The height is necessary to accommodate attractive architecture, fluid circulation, and diverse commercial land uses, with adequate parking. As conditioned to require the construction of Phase II, the proposed Project enhances the ability and willingness for anchor tenants to remain on the site and expand the existing uses, which is consistent with the purpose of providing quality commercial uses in the area. Thus, granting the height variance is consistent with the purposes of the City's zoning code. As conditioned, granting the height variance will not constitute a grant of special privileges because the property is zoned to accommodate a planned commercial center that is regional in nature.

SECTION 15. RREEF applied for a Sign Exception/Sign Program for all phases of the project to amend the 2002 Mall Master Sign Program as well as the separate 1991 Fry's sign approval, to reflect and correspond to expansion of the Shopping Center's street frontage through the addition of the Fry's parcel, the addition of new buildings and parking structures, and installation/updates of existing monument, pole, and wall signs, temporary, directional, and project banner signs, and a City "Gateway" Element sign at Sepulveda and Rosecrans. As noted below, this Resolution does not approve the signs proposed for Phase III, except as specifically mentioned in the conditions of approval hereinafter. Specifically, RREEF requested:

a) Maximum square footage increase- An increase in the maximum square footage of allowed signage. Currently there is 7,600 SF of signage on the site, the Code allows 5,100 square feet of signage (based on the total frontage of 5,100 lineal feet) and RREEF requested an additional 1,900 square feet above the existing for a total of 9,500 square feet of signage;

b) Multiple pole signs- Eight total pole signs proposed while there are seven existing (four to remain and three to be replaced) plus one new pole sign on the 3500 Sepulveda (Hacienda Building) site, for all three Phases. The three new signs would replace the Fry's signs and generally be consistent with the existing 2002 approved site signs, multi-tenant plus project identification. Two proposed with 60 square feet of signage per side, 240 square feet each (per Code calculations) up to 15'-6" tall, and one at the corner of Sepulveda and Rosecrans up to 30 feet tall with 96 square feet of signage per side, 384 square feet each (per Code calculations). The Code allows only



one pole sign, 150 square foot maximum, up to 30 feet tall in lieu of monument/wall/awning signs;

c) Non-Department store anchor wall signs- Up to 200 square feet in size each proposed, with no more than 2 signs per tenant and no more than 2 square feet of signage per linear foot of store frontage. The Code limits the signs to a maximum of 150 square feet in area and no more than 2 square feet of signage per linear foot of store frontage;

d) Signs over 150 square feet to remain- Allow Macy's, CVS and Ralphs to remain over the 150 square foot limit, consistent with prior approvals;

e) Tenant wall signs on parking structures- Allow signs facing Sepulveda, Rosecrans and Marine, to a maximum of 60 square feet each, while the Code does not permit signs on parking structures as they are not located on a business;

f) Monument signs-Allow 13 existing and 5 new monument signs up to 6 feet tall each. No exception needed for the number and height, just the overall site sign square footage;

g) Project identification signs- Allow additional project identification signs on the buildings, while the current approval only allows two at the enclosed Mall entrances and the Code allows none;

h) Directional wall signs on parking structures- Allow wall signs on the parking structures, one at each vehicular entry, without project identification, while the Code does not permit signs on parking structures as they are not located on a business;

i) Directional signs- Allow directional signs up to 6 feet high and 12 square feet while the Code allows 4 feet high and 6 square feet;

j) Project banners on light poles- Allow the continuation of and the addition of project banners at the light poles as allowed under the current approval but not allowed under the Code;

k) Temporary signs- Allow A-frame, portable, sidewalk or other temporary signs on the interior of the project not visible from the public right-of-way up to 365 days a year, while the Code limits the number and size and allows 90 days maximum per year;

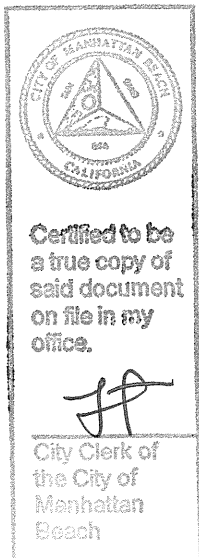
l) Exclude certain square footage-Allow the following sign area to be excluded from counting towards the total allowed square footage: Project graphic banners, Parking Deck Entry signs, Directional Signs, Sidewalk Signs, Temporary A Frame/Sign Holder Signs, and non-tenant oriented portions of Gateway Element Sign; and

m) City Gateway Sign- Allow a City Gateway Sign at the corner of Rosecrans and Sepulveda over 30' in height.

Based upon substantial evidence in the record and pursuant to MBMC Section 10.72.080, the City Council finds:

1. The sign exception, as conditioned, would not be detrimental to, nor adversely impact, the neighborhood or district in which the property is located. Potential impacts may include, but are not limited to, design;

a. The site is surrounded directly by commercial and industrial uses on the north, northeast, west and south, and by residential uses to the east, with residential beyond on the west, south and east sides. Most adjacent residential, commercial, and industrial uses are separated from the subject site by distance, streets, topography, landscaping and/or physical development and would

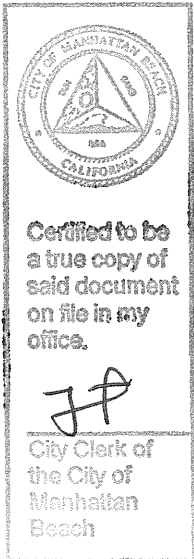


not be impacted by the proposed sign exception, as conditioned. The approved sign exception would be consistent with the Community Commercial and General Commercial zoning districts, since it will provide uniform site signage that is attractive and require the removal of outdated, obsolete signage. Clear consistent signage will direct visitors to the site, instead of having vehicles cut through streets that do not directly access the site. Much of the signage is on the interior of the site and is not even visible from the surrounding public rights-of-way or from surrounding properties.

- b. The scale, size, and function of the Shopping Center is such that the 2002 Master Sign Program needs to be updated and enhanced to promote and advertise key retail tenants without negatively impacting the experiences of pedestrians, drivers and passengers, or residential land uses.
- c. Tenants benefit from signage that attracts visitors but doesn't detract from well-designed exterior building facades. Signage will relate to building wall materials and colors, without creating aesthetic or light/glare impacts.
- d. The approved signs will enhance the shopping center by providing a consistent visual identity and will appear less bulky than the existing signs because they will generally be at a lower height and state-of-the-art.
- e. The rolling topography of Sepulveda Boulevard, Rosecrans Avenue, and Marine Avenue streets also minimizes adverse impacts of increased signage.

2. A sign exception is necessary in order that RREEF may not be deprived unreasonably in the use or enjoyment of the property;

- a. A comprehensive Master Sign Program across the entire shopping center site alleviates confusion to visitors, the need to consult personal digital devices for directions, and provides tenants with assurance that visitors can self-direct towards desired destinations.
- b. The three individual property owners (RREEF, Macy's and Hacienda) have previously agreed to and are developing each of their respective properties to operate as an integrated commercial property. They can now realize a planned development with signage that will be harmonious and consistent throughout the shopping center site.
- c. The enhanced signage increases the potential for visitors to readily grasp the diverse shopping and restaurant opportunities at the shopping center.
- d. The sign exceptions will promote and advertise certain retail tenants without impacting the experiences of pedestrians, drivers and passengers, or adjacent residential land uses.
- e. The approved signage will direct people to the parking structures while being compatible with the architecture and site design.
- f. The Project will be enhanced by one Master Sign Program with consistent signage. The approved square-foot cap will not result in a change to the perceived number or density of signs across



the entire site since the amount of signage will be in proportion to the square footage of new buildings constructed, and many of the new signs will be on the interior of the Project and not visible from the public rights-of-way, or surrounding properties.

- g. The exception is warranted since the shopping center is the largest retail property of its kind in the City, has four major frontage roads, and has multiple internal streets, driveways, and walkways. The signs are necessary to attract and guide visitors from Sepulveda Boulevard, Rosecrans Avenue, Marine Avenue, and Village Drive.

3. The proposed sign exception is consistent with the legislative intent of this title;

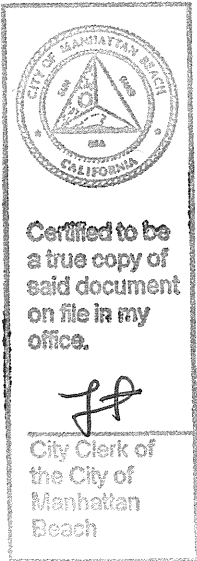
- a. The exceptions, as conditioned, will promote the preservation of the character and quality of the area consistent with the character of Area District II.
- b. The signage will use high quality and attractive materials, blending with the architectural theme of the mall expansion, while enhancing and supporting the retail commercial environment of Sepulveda Boulevard. This will help promote the economic stability of existing land uses and strengthen the City's economic base in a manner that is consistent with other goals in the General Plan, such as creating a harmonious land use scheme.
- c. The approved sign program, including new pole sign design and placement, is consistent with the Sepulveda Development Guide.

SECTION 16. The Project will not individually nor cumulatively have an adverse effect on wildlife resources, as defined in Fish and Game Code Section 711.2.

SECTION 17. This Resolution, upon its effectiveness, constitutes the Master Use Permit and the Sign Exception/Program for the Shopping Center and supersedes all previous site-wide and individual land use approvals, with the exception of: (1) Planning Commission Resolution No. PC 10-03 approving the Vintage Shoppe located on 3500 Sepulveda's property; and (2) Planning Commission Resolution No. PC 12-02 and City Council Resolution No. 6171 as they relate to the Tin Roof Bistro located on 3500 Sepulveda's property. Notwithstanding that this Master Use Permit supersedes previous land use approvals, neither the entitlements conferred herein, nor any condition set forth in Section 18, shall be interpreted to amend, modify, restrict, limit, revise or affect in any way the entitlements and associated conditions applicable to the Vintage Shoppe. Similarly, the conditions set forth in Section 18, shall not be interpreted to restrict, adversely affect or limit in any way the land use entitlements conferred on 3500 Sepulveda by the City prior to the adoption of this Resolution. Nevertheless, this Resolution confers benefits to 3500 Sepulveda, including eliminating established limits on office, medical and dental uses, allowing banking uses up to 2,000 square feet in size on its property (subject to condition 18e) where such banks were not permitted prior to adoption of this Resolution, allowing additional space for restaurants, and increasing the permitted hours of operation and for the sale of alcohol at the Tin Roof Bistro, which is located on the property owned by 3500 Sepulveda.

SECTION 18. The City Council hereby **APPROVES** a Master Use Permit Amendment, Height Variance, and a Sign Exception/Program for Phases I and II of the proposed remodel and expansion of the Manhattan Village shopping center, as refined and modified herein, subject to the following conditions:

GENERAL/PROCEDURAL



1. *Compliance.* Use and development of the site shall be in substantial compliance with the MVSC Enhancement Project Entitlement Request: MUP/MSP/Sign Exception Amendment/Height Variance dated July 24, 2013, as amended April 29, 2014, and November 2014, as amended by the refinements and modifications approved herein subject to any conditions set forth within this Resolution. The Director of Community Development (“Director” hereinafter) shall determine whether any deviation from the Approved Plans requires an amendment to the Master Use Permit or any other discretionary entitlements. RREEF shall fund the cost of the City and its consultants ensuring that the conditions of approval are complied with, as well as monitoring of the Mitigation Measures as required by CEQA in the Mitigation Monitoring and Reporting Program. The Applicant shall submit a final plan incorporating all of the refinements, modifications, and conditions approved in this resolution within 30 days of the date of this resolution (“Approved Plans”).

2. *Lapse of Approval.* The entitlements conferred herein shall lapse four years after the effective date of this Resolution unless implemented or extended in accordance with MBMC Section 10.84.090.

3. *Terms and Conditions are Perpetual; Recordation of Covenant.* The provisions, terms and conditions set forth herein are perpetual, and are binding on RREEF, Macy’s, their respective successors-in-interest, and, where applicable, all tenants and lessees of RREEF or Macy’s. Further, RREEF shall record a covenant indicating its consent to the conditions of approval of this Resolution with the Office of the County Clerk/Recorder of Los Angeles. The covenant is subject to review and approval by the City Attorney. RREEF shall deliver the executed covenant, and all required recording fees, to the Department of Community Development within 30 days of the adoption of this Resolution. If RREEF fails to deliver the executed covenant within 30 days, this Resolution shall be null and void and of no further effect. Notwithstanding the foregoing, the Director may, upon a request by RREEF, grant an extension to the 30-day time limit.

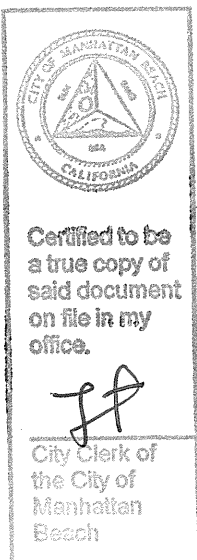
4. *Review.* Provisions of the Master Use Permit Amendment, Variance, and Sign Exception/Program Amendment are subject to review by the Community Development Department within six months after occupancy of the first building constructed in Phase I and yearly thereafter.

5. *Interpretation.* In the event the Director and RREEF disagree regarding the intent or interpretation of any condition, the Planning Commission shall provide a binding and final interpretation of the condition. Such Commission determination cannot be appealed to the City Council.

6. *Fish and Game.* Pursuant to Public Resources Code section 21089(b) and Fish and Game Code section 711.4(c), the entitlements conferred herein are not operative, vested or final until the required filing fees are paid.

7. *Effective Date.* The decision of the City Council is final upon the date this Resolution is adopted.

8. *Tenant Space Chart.* Upon submittal of any request for business license, or application for building permit, which involves the alteration or enlargement of any tenant space, or the introduction of any new business within an existing tenant space, RREEF shall provide to the Community Development Department an up to date site-wide tenant space chart which includes all of the tenants and properties within the Shopping Center including vacant space. The space chart shall include detailed area breakdowns and shall be used to account for decommissioned vacant leasable space which is available for occupancy pursuant to gross leasable area (GLA) square feet maximums addressed in Condition 18 and under the terms of this Master Use Permit. The required space chart shall be consistent in format and information provided with that certain “Manhattan Village Shopping Center Leasable Area Tabulation - November 23, 2014.” The space chart shall also include any outdoor dining areas. The information shall include tenant street addresses and suites, existing and



proposed tenant, and evidence that the proposed alteration/tenant will provide adequate parking and loading as required by applicable parking standard.

9. *Indemnity, Duty to Defend and Obligation to Pay Judgments and Defense Costs, Including Attorneys Fees, Incurred by the City.* RREEF shall defend, indemnify, and hold harmless the City, its elected officials, officers, employees, volunteers, agents, and those City agents serving as independent contractors in the role of City officials (collectively "Indemnitees") from and against any claims, damages, actions, causes of actions, lawsuits, suits, proceedings, losses, judgments, costs, and expenses (including, without limitation, attorneys' fees or court costs) in any manner arising out of or incident to this approval, related entitlements, or the City's environmental review thereof. RREEF shall pay and satisfy any judgment, award or decree that may be rendered against City or the other Indemnitees in any such suit, action, or other legal proceeding. The City shall promptly notify RREEF of any claim, action, or proceeding and the City shall reasonably cooperate in the defense. If the City fails to promptly notify RREEF of any claim, action, or proceeding, or if the City fails to reasonably cooperate in the defense, RREEF shall not thereafter be responsible to defend, indemnify, or hold harmless the City or the Indemnitees. The City shall have the right to select counsel of its choice. RREEF shall reimburse the City, and the other Indemnitees, for any and all legal expenses and costs incurred by each of them in connection therewith or in enforcing the indemnity herein provided. Nothing in this Section shall be construed to require RREEF to indemnify Indemnitees for any Claim arising from the sole negligence or willful misconduct of the Indemnitees. In the event such a legal action is filed challenging the City's determinations herein or the issuance of the approval, the City shall estimate its expenses for the litigation. RREEF shall deposit said amount with the City or enter into an agreement with the City to pay such expenses as they become due.

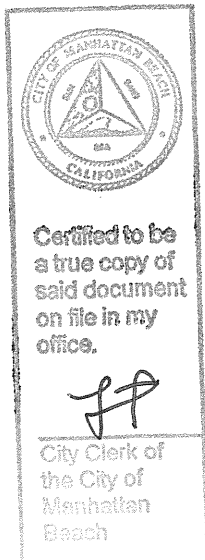
AESTHETICS

10. Landscape/Hardscape/Lighting Sitewide Plan. RREEF shall submit a detailed Landscape/Hardscape/Lighting Plan, including a construction schedule, to the City Police, Fire, Public Works and Community Development Departments and the City Traffic Engineer for review and approval with the submittal of plans for Phase I that provides for the following:

a. RREEF shall provide and maintain consistent drought tolerant landscape, shade trees, hardscape, and lighting improvements throughout the Development Area, as well as certain areas of the entire Shopping Center property as required in these conditions. The improvements shall be consistent with the Approved Plans, renderings, presentations, application material, and project descriptions.

b. RREEF shall provide and maintain mature trees and other landscaping adjacent to the parking structures, particularly in the areas without buildings adjacent to the perimeter of the structures, to screen and soften the parking structures, as shown on the Approved Plans. The trees adjacent to the North Parking structure, as shown on the renderings, shall be a minimum of 5 feet above the top of the parking structure when initially planted. Landscaping and irrigation also shall be provided on the upper levels of the structures in the form of permanent planting receptacles suitable for the planting of vines or similar plants on the parapet walls on the north and west sides of the North Parking Structure and on the south side of the South Parking Structure. Landscaping shall be planted and maintained throughout the surface parking lots. A minimum of 1 tree per 10 parking spaces in a parking structure and 1 tree per 6 surface parking spaces within the Shopping Center property, minimum 24-inch box size, shall be provided at grade. Permanent irrigation shall be provided for all landscaping.

c. RREEF shall provide and maintain consistent drought tolerant landscape, shade trees, hardscape, and lighting improvements throughout the



Shopping Center property as improvements are made in those portions of the Shopping Center property outside of the Development Area, as detailed in the Landscape/Hardscape/Lighting Sitewide Plan.

d. All new light fixtures on the top levels of parking structures shall be no taller than 15 feet, shall utilize LED fixtures, and include shields to reduce glare. All other new exterior lighting, except signage lighting, shall include shields as necessary to reduce glare so that there are no adverse impacts on surrounding properties.

e. As determined in the Police Security Plan, approximately one hour after all businesses on the Shopping Center have closed, the light fixtures on and in the parking lots and structures shall automatically be dimmed or lowered in intensity.

f. RREEF shall evaluate the feasibility of modifying or replacing existing lighting fixtures on the Shopping Center property to reduce off-site illumination and be more energy efficient.

g. Improvements shall be installed per the approved Landscape/Hardscape/Lighting Sitewide Plan, including the approved construction schedule, and improvements associated with the off-site linkages and on-site improvements outside of the Development Area as identified in the Final EIR shall be installed prior to the completion of Phase I, as determined to be feasible by the Community Development Director.

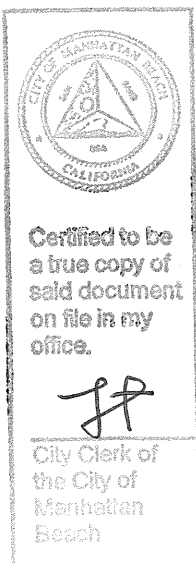
11. Signage Site-wide Plan/Master Sign Program. The Project shall provide consistent signage improvements throughout the Shopping Center property. The total square footage of signage for the Shopping Center property shall not exceed 9,500 square feet as established herein and as defined by the Code. The sign improvements shall generally be consistent with the Master Sign Program as amended herein with the following revisions:

a. Signs shall be compatible with their related buildings and not be crowded within their locations or backgrounds. Harsh plastic or illuminated backgrounds shall be avoided, and low profile monument signs are encouraged.

b. Roof signs are prohibited.

c. All signage on parking structures shall be accessory and compatible to the structure through the design, color, location, size and lighting and not detract from the parking structure's architectural character. Any tenant signage on a parking structure shall have a locational relationship and proximity between the parking structure and the tenant. Signage near the top of parking structures is discouraged, but can be approved by the Director of Community Development through the Master Sign Program if it is compatible with the architectural design of the subject structure on which the signage is proposed, as well as consistent with the intent and criteria of the Sign Code, Master Sign Program and Approved Plans.

d. Plans for interim City Gateway identification signage, and landscaping, at the corner of Rosecrans Avenue and Sepulveda Boulevard, welcoming people to the City of Manhattan Beach, shall be submitted with the submittal of building plans for Phase 1. The Gateway signage shall not count as part of RREEF's square feet of signage approved authorized herein. RREEF shall submit plans for the improvements to the Community Development Department, for review and approval and construct the improvements per plans approved by the City in connection with the construction of Phase I. In the event RREEF seeks approval of Phase III, RREEF shall submit plans for permanent City Gateway identification signage at the corner of Rosecrans Avenue and Sepulveda



Boulevard. RREEF shall install the permanent City Gateway signage before the first building permit for Phase III is issued.

e. The number and size of any new Department store and non-Department store anchor wall signs shall be governed by the Master Sign Program.

f. No interior and exterior signs authorized by this approval may be installed unless: (1) the respective property owner or designated representative has approved the sign in writing; (2) the owner has submitted a sign approval application to the City; and (3) the City determines that the sign is consistent with the Master Sign Program approved herein.

g. At the sole cost of RREEF, Fry's pole sign adjacent to the Sepulveda Boulevard bridge shall be removed, or relocated if Fry's is still occupying the Northwest Corner, by RREEF upon 90 days' notice from the City when the City determines that removal or relocation is necessary as part of the Sepulveda Bridge Widening. The relocation location shall be within the Shopping Center property along the Northwest Corner fronting Sepulveda Boulevard. This Sepulveda Boulevard Fry's pole sign, as well as the two existing Fry's pole signs along Rosecrans Avenue, shall be removed when Fry's vacates the Northwest Corner. The Master Sign Program provides for future new pole signs in the Northwest Corner, in connection with the future development of Phase III.

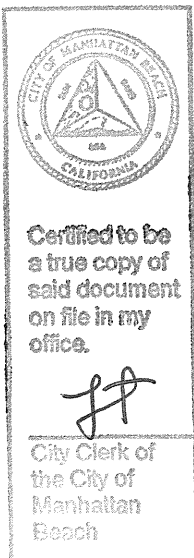
h. The signage for Phase III shall not be installed until Phase III is approved and developed. The signage allocated for and located within the Northwest corner, Phase III, including the square footage and number of signs, shall not be reallocated or used for Phase I or Phase II development.

12. Construction Screening. RREEF shall provide construction screening of 6 feet or greater in height as reasonably determined necessary by the Director to screen the construction site from view. Graphics shall be provided on the screening to enhance the aesthetics of the Shopping Center property and provide Project information. The screening may potentially include announcements for new Shopping Center tenants if approved by the Director through a Temporary Sign Permit application. The screening shall be maintained in good condition at all times. RREEF shall submit plans for the screening to the Community Development Department, for review and approval, with the submittal of plans for each Phase. The City will review and consider approving the plan, and RREEF shall install the screening, per the approved plan, prior to the initiation of construction for each applicable Phase.

LAND USE

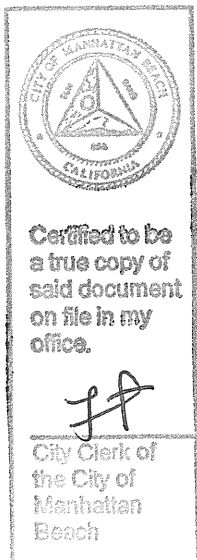
13. In connection with **Phase I (Village Shops)**, RREEF must comply with the following conditions:

a. **Size Reduction and Redesign.** RREEF shall construct the Village Shops building and the North and South parking structures in substantial compliance with the Approved Plans, which requires a 10,000 SF reduction in the Village Shops buildings and a redesign of the North parking structure, as shown on the Approved Plans. The EIR analyzed 60,000 square feet of net new GLA as the maximum buildable area in the Village Shops Component. To achieve the 10,000 square foot reduction in the Village Shops, the maximum net new GLA is set at 50,000 net new square feet. RREEF shall construct a minimum 8-foot wide combined pedestrian/bike path and a minimum 5-foot wide landscaped buffer adjoining the north wall of the North Parking Structure to create a pedestrian/bike linkage between Cedar Way and Carlotta Way as depicted on the Approved Plans. The North Parking Structure shall not exceed a height of G+2 as depicted on the Approved Plans. Approximately the north 60 percent portion of the South Parking Structure shall not exceed a height of G+2 and the approximately 40



percent south portion of the South Parking Structure shall not exceed a height of G+1 as depicted on the Approved Plans.

- b. RREEF shall submit all submittals required in connection with Phase I in accordance with the requirements set forth in the applicable condition.
 - c. **Macy's Consolidation with Phase I.** Prior to the issuance of the first building permit for Phase I, RREEF shall provide written evidence of a commitment binding on RREEF and Macy's to consolidate its Macy's Men's operation at the south end of the Main Mall to an expanded Macy's Fashion Store on the north end as depicted on the Approved Plans and release the Men's Store to RREEF for redevelopment.
 - d. Prior to the issuance of permits for Buildings B, C, D and E in Phase I, RREEF shall submit to the City a non-refundable \$400,000 security deposit. Such deposit may not be drawn upon for any other purpose other than paying City fees associated with the Macy's Fashion Store expansion and the construction of the Northeast parking structure, in compliance with the Approved Plans. In the event the Macy's Fashion Store is not expanded, RREEF shall forfeit the deposit to the City. If, any portion of the deposit remains after occupancy permits are issued to Macy's for the expanded area and all fees have been paid, the balance of the deposit shall be refunded to RREEF.
 - e. Prior to the issuance of Certificates of Occupancy for Buildings B, C, D and E, RREEF shall submit or cause to be submitted, and the City shall accept, a complete building plan check submittal to plan check for the Macy's Fashion Store expansion. RREEF shall also submit a document, acceptable to the City Attorney, waiving any claims against the City if the Certificates of Occupancy are not issued due to the failure to timely submit building plan check submittals for the Macy's Fashion Store expansion.
 - f. RREEF shall provide a U-turn, traffic circle, or other connection at the Rosecrans Avenue entrance in the lower level parking lot with a minimum outside turning radius of 30 feet, to internally connect both drive aisles.
 - g. The driveway access between the lower level parking and Carlotta Way shall be revised to minimize the sharp angle.
 - h. RREEF shall comply with the City Traffic Engineer's recommendations designed to minimize conflicts and improve visibility and safety with the location of parking spaces with direct access onto internal private streets (Cedar, Fashion and Carlotta) and onto accessways leading to parking structures.
 - i. RREEF shall submit Planning Preliminary Plan Check Review, as defined in Condition No. 17, prior to the issuance of building permits.
14. In connection with **Phase II (Northeast corner)**, RREEF and, where applicable, Macy's must comply with the following conditions:
- a. RREEF shall submit all submittals required in connection with Phase II in accordance with the requirements set forth in the applicable condition.
 - b. **Macy's Consolidation with Phase I.** Prior to the issuance of the first building permit for Phase I, RREEF shall provide written evidence of a commitment binding on RREEF and Macy's to: relocate the Macy's Men's operation at the south end of the Main Mall to an expanded Macy's Fashion Store on the north end as depicted in the Approved



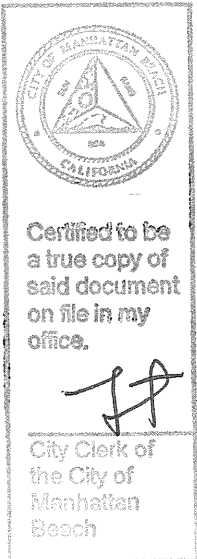
Plans; and the release of the vacated space formerly occupying the Men's Store to RREEF for redevelopment.

- c. Macy's shall expand its Macy's Fashion store by as much as 60,000 square feet, and, RREEF shall lease the space currently occupied by Macy's Men's at the south end of the Main Mall.
- d. Prior to the issuance of Certificates of Occupancy for Buildings B, C, D and E, RREEF shall submit or cause to be submitted, and the City shall accept, a complete building plan check submittal to plan check for the Macy's Fashion Store expansion. RREEF shall also submit a document, acceptable to the City Attorney, waiving any claims against the City if the Certificates of Occupancy are not issued due to the failure to timely submit building plan check submittals for the Macy's Fashion Store expansion.
- e. Existing utilities that are impacted by the construction shall be rerouted to be within the private streets on site or other locations approved by the Public Works Department and any other responsible agencies.
- f. RREEF shall submit to the City a Master Use Permit Amendment and all necessary applications for Phase III-Northwest corner, including a construction schedule, within 3 months of Fry's vacating their current Northwest corner location, and the City shall take action on the applications in a timely manner.
- g. Prior to issuance of building permits for Phase II, plans shall be submitted to plan check for the vehicular access ramp between the Medical Building at 1200 Rosecrans Avenue and new Northeast parking structure to be redesigned to accommodate two-way traffic to connect the lower level parking lot to the main Shopping Center level surface parking. The new ramp shall be completed prior to the issuance of a Certificate of Occupancy for the Macy's Fashion Store expansion.
- h. Cedar Way connection to Rosecrans with Phase II. Prior to issuance of building permits for Phase II, plans for the extension of Cedar Way to be connected through to Rosecrans Avenue shall be submitted to the City for plan check. The extension shall be completed prior to the issuance of a building permit final for the Macy's Fashion Store Expansion.
- i. Existing unscreened rooftop equipment that is visible from ground view (i.e., Islands restaurant) shall be screened prior to issuance of a building permit final for the Macy's Men's Store redevelopment.
- j. RREEF shall submit planning staff Preliminary Plan Check Review as defined in Condition No. 17 prior to the issuance of building permits.

15. **Phase III (Northwest corner).** Phase III is not a part of this approval and cannot be implemented until a Master Use Permit Amendment and other related applications for that phase are approved by the City.

16. **Development Area Envelopes and Maximum Heights.** The Development Area Envelopes and maximum heights as analyzed in the Final EIR and as shown in the Approved Plans, for Phases I and II, are approved in concept, subject to the project conditions. Planning Staff review is required for the site improvement details through the Preliminary Plan Check Review process.

17. **Architectural Elements Required Through Preliminary Plan Check Review.** Except as provided in Condition 15, RREEF shall submit to the City Planning staff for Preliminary Plan Check Review all architectural plans, to show that the Project is consistent with the architecture, quality and concept plans as shown in the Approved



Plans. The architectural plans shall include, but not be limited to, plans, material boards, color samples, renderings, and other visual displays to provide the following:

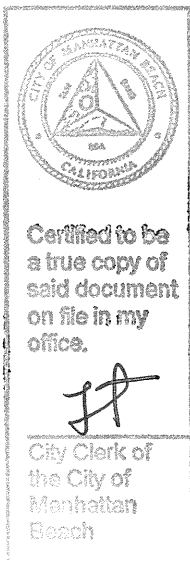
- a. Building and parking site plan-layout within the Development Area Envelopes.
- b. Facades/elevations design motifs.
- c. Colors, textures, and materials as concept design.
- d. Landscaping, lighting, signage, and common area treatments as concept design.
- e. Streetscape and common-outdoor plaza areas design - pavement treatment, sidewalks, pedestrian crosswalks, street/courtyard furniture, the clock tower, as concept design.

18. **Land Uses and Square Footages.** The existing Shopping Center contains approximately 572,837 square feet gross leasable area (GLA). The Project may add a maximum of 79,872 net new square feet GLA (89,589 square feet with the Equivalency Program) within Phases I and II in the Development Area. The Shopping Center property may not exceed 686,509 square feet GLA (696,226 square feet with the Equivalency Program).

For any proposed square footage that exceeds 686,509 square feet, up to the 696,226 square foot cap, RREEF shall submit traffic and parking data for review by the Community Development Department and the City Traffic Engineer to determine if the proposal is consistent with the trip generation and parking thresholds established in the Certified Final EIR and the Equivalency Program. The study shall include an update of the sitewide list of tenants in Exhibit "A", uses and GLA, and RREEF shall pay the cost of the City Traffic Engineer's review.

The following land uses are allowed in the Shopping Center, provided that no land use type exceeds the applicable maximum square footage for each type:

- a. Retail Sales (including drug stores)
- b. Personal Services (e.g., Beauty salons, Dry-Cleaners, Shoe repair)
- c. Food and Beverage Sales (including Grocery Stores, but excluding high traffic generating or high parking demand land uses such as liquor or convenience stores as determined by the Director)
- d. Offices, Business and Professional - 69,300 square feet maximum for Business and Professional offices. Additionally, 28,800 square feet maximum for Medical and Dental offices (existing square footage rounded, plus an additional 7,000 square feet allowed). The 3500 Sepulveda Boulevard building may be occupied with 100% Business and Professional and/or Medical and Dental offices, as long as the total combined office square footage on the entire Mall site does not exceed 98,100 square feet, and the parking requirements are met.
- e. Banks and Savings and Loans - 36,200 square feet maximum (existing square footage, no additional square footage allowed). If any of the existing bank operators in stand-alone buildings adjacent to Sepulveda Boulevard terminate their bank operation for a period longer than 6 months (except for suspended operation in the event of fire, casualty or major renovation), they may not be replaced with another bank or savings and loan use. This clause is not intended to govern business name changes or mergers or acquisitions among bank operators, commercial banks or savings and loans. No new bank or savings and loan uses are permitted in existing or new stand-alone buildings. New



banks or savings and loan uses are limited to a maximum of 2,000 square feet in area.

- f. Eating and Drinking Establishments (restaurants) - 89,000 square feet maximum, which includes outdoor dining areas for restaurants that provide full table service.
- g. Uses identified as permitted (by right) in the underlying zoning district (CC) which are not included in this Master Use Permit shall be left to the discretion of the Director to determine if Planning Commission review is required.

The following uses are not permitted by this Master Use Permit:

- a. Personal Improvement Services (Gyms, Dance studios, Trade schools, etc).
- b. High traffic generating or parking demand land uses, including but not limited to, liquor stores and convenience stores as determined by the Director of Community Development.
- c. Bars.

19. **Fry's continued operation and future tenant.**

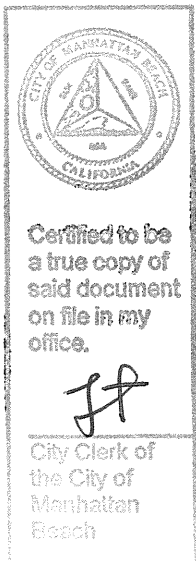
- a. **Good Faith Negotiations with Fry's.** If Fry's indicates in writing to RREEF that it desires to continue to operate the Fry's retail store at its current location after the termination date of its current lease which expires in December 2016, RREEF will negotiate in good faith with Fry's on an annualized lease extension option or options on terms mutually acceptable to both parties and subject to RREEF's need to provide for a Fry's termination to accommodate the future redevelopment of the Northwest Corner.
- b. Any new tenant proposed to occupy the existing building on the Fry's 3600 Sepulveda Boulevard site shall require Planning Commission review at a noticed public hearing. Criteria and potential impacts to consider include but are not limited to, traffic, parking, access, land use compatibility including architectural entryway enhancement, length of tenancy security/crime, noise, light, hazards, vibrations, odors, aesthetics, and demand on public services.

20. **Alcohol Off-site Sales.** An amendment to the Master Use Permit must be approved by the City prior to the sale of alcohol other than for on-site consumption at an eating and drinking establishment, unless specifically permitted by this Resolution. Tenants with existing ABC licenses and City approval for off-site alcohol sales and/or on-site tasting – i.e., Ralphs, CVS, and the Vintage Shoppe – may continue to sell alcohol for off-site consumption and/or on-site tasting in accordance with their approvals.

21. **Restaurant Drive-Through.** There shall be no Restaurant drive-through service allowed in conjunction with any existing or proposed Eating and Drinking Establishment.

22. **Restaurant Hours.** No restaurant use shall be open between 2:00 a.m. and 6:00 a.m. on any day.

23. **Restaurant Alcohol.** Any restaurant may provide full alcohol service, which is incidental to, and in conjunction with, the service of food provided that such use does not include a retail bar, to a maximum area of 89,000 square feet site-wide as set forth in Condition No. 18. This approval shall operate within all applicable State, County and City regulations governing the sale of alcohol. Any violation of the regulations of



the Department of Alcohol and Beverage Control as they pertain to the subject location, or of the City of Manhattan Beach, as they relate to the sale of alcohol, may result in the revocation and/or modification of the subject Master Use Permit.

24. **Entertainment.** Any entertainment proposed (with the exception of background music, television and no more than 3 games or amusements) shall be required to obtain a Class I Entertainment Permit consistent with the provision of Section 4.20.050 of the Manhattan Beach Municipal Code.

25. **Landscape Maintenance.** Landscaping and maintenance activities (including, but not limited to, parking lot cleaning, grounds-keeping and outdoor equipment and shopping cart cleaning) shall occur in accordance with a Landscape Maintenance Plan ("The Maintenance Plan") approved by the Director of Community Development. The Maintenance Plan shall establish permitted hours of operation for specific maintenance activities and areas of the shopping center, based on compatibility with nearby land uses, both on and adjacent to the center. All landscaping materials shall be maintained to the satisfaction of the Director of Community Development.

NOISE MITIGATION

26. **Deliveries.** Delivery activities that are adjacent to residentially zoned and improved properties shall be limited to between 7:00 a.m. and 10:00 p.m. Monday through Friday and between 8:00 a.m. and 10:00 p.m. on Saturdays, Sundays and major holidays, including New Year's Day, Memorial Day, Independence Day, Labor Day, Thanksgiving Day, and Christmas Day. Delivery operations shall be conducted in such a manner so as not to exceed applicable residential noise standards. The term "delivery activities" shall include, but not be limited to the presence of workers or delivery trucks at the business site even if not actual delivery work or unloading is being done. It shall also include vehicles or delivery equipment being started or idled, playing of radios or other devices, loud talking, and unloading of materials. Business delivery doors shall not be opened before hours of permitted deliveries as specified herein. Delivery vehicles shall park in designated commercial loading areas only and shall not obstruct designated fire lanes.

27. **Trash Collection.** Routine trash collection on the entire site shall occur after 9:00 a.m. and before 10:00 p.m. Construction material trash collection activities (drop off and pick-up) shall be limited to hours of permitted construction as specified in the City's Noise Ordinance, or between 7:30 a.m. and 6:00 p.m. Mondays through Fridays, and between 9:00 a.m. and 6:00 p.m. on Saturdays.

FIRE PROTECTION

28. **Fire Emergency Response Plan.** A Fire Emergency Response Plan for fire lanes, fire sprinklers, fire hydrants, and other Fire emergency response requirements shall be provided and maintained for the Shopping Center property. The Fire Emergency Response Plan shall include, but not be limited to, the following:

- a. Provide a minimum vertical clearance of 15 feet and horizontal clearance of 20 feet for Fire vehicle access under all bridges and other overhead structures on Village Drive, Cedar Way, Carlotta Way, Fashion Boulevard, and within the lower level parking lot. In the lower level parking lot, the horizontal clearance of 20 feet for Fire vehicle access is required in only one of the two drive aisles. This is intended to allow ambulance-paramedic vehicle access throughout the Shopping Center property, but not within the parking structures. Village Drive, Cedar Way, Carlotta Way, Fashion Boulevard, and within the lower level parking area, and any other required roadways, shall be designated as Fire lanes as determined by the Fire Department, shall allow "no stopping" on both sides of roadways, and be clearly marked. Additional lane width will be



required in certain areas to accommodate vehicle turning movements and bicycles.

b. All parking structures shall provide a minimum vertical clearance as required by the current Code at the time of Building Permit approval for disabled/ADA access at grade level. All parking structures shall also have the required stand pipes, sprinklers, hydrants, perimeter and internal access, gurney size elevators, and exterior stairs for Fire suppression.

c. RREEF shall provide a "gator" or similar gurney transport vehicle on the site to provide Fire Department access within the parking structures and other remote areas.

d. Fire hydrants shall be located within 15 feet of the Fire Department Connections (FDC), and the FDC and related double check valve assembly shall be integrated into the design of the buildings to screen the valves but allow clear visibility and access to the FDC, subject to Fire and Community Development Department approval.

e. Upgrade to current standards the Opticom emergency vehicle preemption devices at all signalized intersections adjacent to the project site.

f. An Emergency Response Plan that includes 24/7 on-site personnel to direct emergency response teams to the exact location of incidents shall be provided.

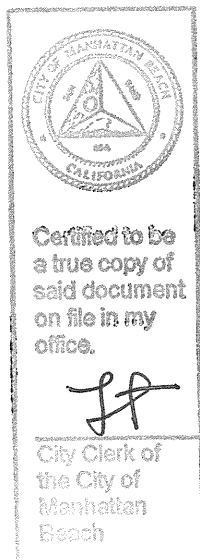
g. RREEF shall work cooperatively with the Fire Department to provide, if feasible, a pedestrian ramp or at-grade access at the rear of the existing enclosed main Shopping Center to facilitate the safe removal of patients from that location.

RREEF shall submit the Fire Emergency Response Plan to the City Fire and Community Development Departments with the submittal of plans for each Phase, including an implementation and maintenance schedule. The City will review and approve the Plan, and RREEF shall install, implement and maintain the improvements and requirements per the approved Plan.

SAFETY AND SECURITY MEASURES

29. **Police Holding Office.** The Project shall lease at no rent to the City a separate and secure Police "holding" office within the main, enclosed Mall approximately 100-150 square feet in area. The location of the office is subject to Police Department review and approval but it must have access from the interior of the Mall during Mall operating hours, such as from a corridor, and exterior access is not required. This will be separate from the Mall Security staff office. The intent and use of this area will be for the exclusive use of the Police Department to have a safe, secure, convenient, comfortable and private area for interviewing and consulting with victims, witnesses, and others with security issues and concerns. The area will provide for storage of Security and Safety Educational material for Police use. RREEF shall submit a Police Holding Office Plan to the City Police and Community Development Departments with the submittal of plans for Phase I. The City will review and approve the Police Holding Office Plan, and RREEF shall install the improvements, which shall include drywall, paint, and electrical utilities, but shall not include plumbing, per the approved plan prior to the issuance of the first building final for Phase I. If the City Police Department determines it no longer needs the "holding" office, or its use ceases, the lease shall terminate.

30. **Security Cameras.** RREEF shall provide security cameras throughout the parking structures and surface parking lots within the entire Shopping Center property to the reasonable satisfaction of the Police Department. RREEF shall provide a Security Camera Plan for the installation of the cameras during construction on the



Shopping Center property. Cameras shall be placed at parking structure entrances, exits, stairwells, elevators, and distributed throughout the parking areas pursuant to a plan to be provided by RREEF's security consultant. Cameras shall be located so that license plate numbers are readable. Some cameras shall be capable of being relocated as needed to monitor Special Events. Cameras are not required to be manned, and a holding period for archival of recordings shall be agreed upon. RREEF shall submit the Security Camera Plan as part of the Security Plan to the City Police and Community Development Departments with the submittal of plans for Phase I. The City will review and approve the Plan, and RREEF shall install the improvements per the approved Plans. The approved Security Camera Plan shall be reviewed annually by the City.

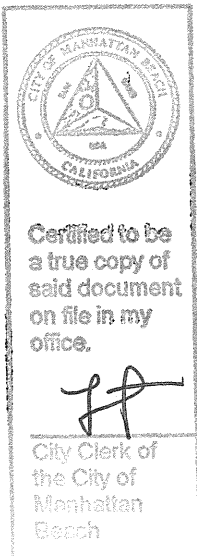
31. **Police Special Event/Security and Cedar Way Plan.** RREEF shall provide a Holiday/Sales-Special Events/Peak Customer Security, Traffic and Parking Control Plan as part of the overall Security Plan. The Plan shall include a provision for reimbursement of Police services when additional services are requested by RREEF. The Plan shall include an update and amendment to the existing Vehicle Code and Parking Enforcement Agreement (June 1, 1987) between the City and the Mall to ensure adequate enforcement mechanisms are in place. The Plan shall provide for RREEF to install repeaters or other devices in the parking structure if it is determined that they are necessary for cell phone and emergency communication needs. The Plan shall also provide for the possibility of closing Cedar Way during Special Events. RREEF shall submit the Plan to the City Police, Fire and Community Development Departments with the submittal of plans for Phase I. The City will review and approve the Plan, and RREEF shall implement the provisions as detailed in the approved Plan. The City may request a periodic review of the operations of Cedar Way to determine if the core area should be closed to vehicular traffic and limited to pedestrians, bikes and emergency vehicle access only.

32. **Package Check.** RREEF shall provide a central package check service for customer use for purchases within the Mall. The Plan for the secure location and operation of the service shall be subject to the City Police Department review and comments and the Community Development Department review and approval. The intent of this condition is for security and convenience in a central location near the valet and loading/unloading area, or other central location, so packages can be held and then loaded directly into the customers' vehicle. RREEF shall submit Plans to the City Police and Community Development Departments with the submittal of plans for Phase I. The City will review and comment/approve the Plan, and RREEF shall install the improvements per the approved Plan prior to the issuance of the first building final for Phase I.

TRANSPORTATION, CIRCULATION AND PARKING

33. **Veterans Parkway Linkage Plan.** RREEF shall submit a Veterans Parkway Linkage Plan as depicted in the Approved Plans to provide bicycle and pedestrian paths under the Sepulveda Bridge and onto the Shopping Center property that link the Shopping Center property and Veterans Parkway. The Veterans Parkway Linkage Plan shall include lighting, signage, and other improvements to enhance the aesthetics, usability and security of the area, to create an inviting entry and secure environment, and to connect the site. The Veterans Parkway Linkage Plan shall coordinate with the construction of the improvements on the Shopping Center property and the Sepulveda Bridge widening project. RREEF shall submit the Plan to the City Police, Fire, Public Works and Community Development Departments, the City Traffic Engineer, and if necessary Caltrans, with the submittal of plans for Phase I. The City, and any other agency with jurisdiction, will review and approve the Plan, and RREEF shall install the improvements per the approved Plan. The City shall maintain the public portions, and the Mall shall maintain the private portions.

34. **Bicycle and Pedestrian Plan.** RREEF shall submit a Bicycle and Pedestrian Plan (the "Plan" in this condition) to provide bicycle and pedestrian improvements throughout the Shopping Center property as depicted in the Approved Plans, including



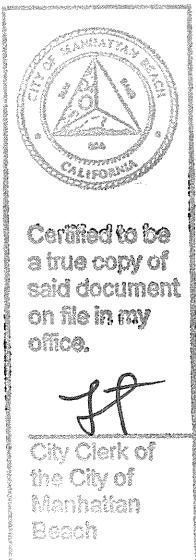
the perimeter of the property, with interconnected walkway and bicycle networks and linkages to off-site improvements and transit (including pavement treatment, raised intersections, improved pedestrian crossings, bike parking, and arrows). Crosswalks with activated flashing beacons on key uncontrolled crossings on Carlotta Way, such as at Carlotta Way in the vicinity of the 3500 Sepulveda Boulevard building, shall be provided. A dedicated separate bikeway under the Sepulveda Bridge, through the Shopping Center Property, and connecting to Village Drive shall be provided. The bikeway in the lower level parking lot shall connect from under the Sepulveda Bridge and up to the Fry's site, but it does not need to continue and connect to Rosecrans Avenue. A separate pedestrian pathway (maximum width of six feet clear) shall link the entire length of the lower level parking lot (Sepulveda Bridge to Rosecrans Avenue). The bike path on Cedar Way shall extend south from Fashion Avenue to Village Circle; a sharrow shall be provided from Rosecrans Avenue to Marine Avenue, as well as a sharrow on Fashion Avenue. The bike network shall connect on and off site and to the bike racks/lockers/facilities, with racks distributed in key locations. The Plan shall include an active "Walk to the Mall" program to encourage non-motorized access to the Shopping Center. The Plan shall include a component of working and partnering with groups that promote walking and alternative forms of transportation. The improvements shall generally be consistent with the Approved Plans, although the pavement treatments shall be provided throughout Cedar Way from Macy's Fashion store to Ralph's. Additional improvements shall be provided at the Ralph's/CVS building at the south end of the Shopping Center to enhance pedestrian accessibility and safety from the parking lot to the buildings as depicted in the Approved Plans. All access shall meet ADA requirements.

Improvements shall be installed per the approved plans with each Phase, except that the off-site linkages and on-site improvements outside of the Development Area as identified in the Approved Plans shall be installed prior to the completion of Phase I, as determined to be feasible by the Community Development Director.

RREEF shall submit the Plan to the City Police, Fire, Public Works and Community Development Departments and the City Traffic Engineer with the submittal of plans for Phase I. The Plan shall include a phasing plan for construction of the improvements that considers construction Phasing on the property, as well as the Sepulveda Bridge widening project. The City will review and approve the Plan, and RREEF shall install the improvements, and RREEF shall maintain the improvements, except for those located on public land such as the extension of Veteran's Parkway under the Sepulveda Bridge as set forth in Condition 33, which shall be maintained by the City, per the approved Plan.

35. Pedestrian Off-site Linkage Plan. RREEF shall provide improvements to the City leased parking lot to encourage and enhance use of the parking lot for employees and customers. Such improvements shall include and be limited to: wayfinding signage and lighting on the staircase serving the City leased parking lot; wayfinding signage and lighting on the staircase between the Village homes and the Shopping Center site; wayfinding signage from the Senior Housing; and maintenance of landscaping on the slope. RREEF shall submit a Pedestrian Off-site Linkage Plan to the City Police, Fire, Public Works and Community Development Departments and the City Traffic Engineer with the submittal of plans for Phase I. The City will review and approve the Plan, and RREEF shall install the improvements per the approved plan prior to the issuance of the first building final for Phase I. Upon the City's acceptance of RREEF's improvements to the City's parking lot, the City will release and indemnify RREEF from any liability related to the improvements.

36. Employee Parking Management Program. The Project shall provide an Employee Parking Management Program to encourage remote parking, parking in the lower level parking lot, off-site parking, walking, biking, transit use, carpooling and other forms of alternative and non-motorized transportation, and incentives to reduce employee parking. Street or other public parking, other than the leased City parking lot off of Village Drive, shall not be used for employee parking. The Program shall actively promote reducing employee parking, shall prohibit parking in structures and



certain surface lots during the peak parking season, and shall include active enforcement by Shopping Center personnel. The Program shall be submitted to the Community Development Department and the City Traffic Engineer for review and approval with the submittal of plans for Phase I and annual reporting shall be provided. The City will review and approve the Program, and RREEF shall implement the Program and install any required improvements per the approved Program prior to the issuance of the first building final for Phase I. The City may request periodic review and adjustment of the Employment Parking Management Program, in cooperation with RREEF, if needed to ensure the goals of this condition and the Program are being met.

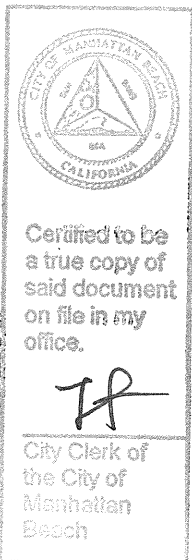
37. **Valet Parking Management Plan.** RREEF shall provide a Valet Parking Management Plan to designate valet parking areas, circulation, hours, days, rates, validations, operations, terms, remote drop-off/pick-up location, signage, passenger drop-off and pick-up, implementation schedule, etc. The Plan shall be submitted to the Community Development Department and the City Traffic Engineer for review and approval with the submittal of plans for Phase I. The City will review and approve the Plan and RREEF shall implement the Plan during Phase I, in accordance with the approved implementation schedule in the Plan. If it is determined that the valet parking is not being fully utilized, RREEF may modify or cease providing valet parking with the approval of the Director of Community Development.

38. **Electric Vehicle (EV) Charging.** RREEF shall install and maintain for public use EV parking/charging stations within the parking structures and/or parking lots at a ratio of a minimum of 1 percent of the total on-site parking spaces, and phased up to 3 percent as usage demands. The installation of stations up to 1 percent may also be phased. RREEF shall provide a minimum of 8 EV parking/charging stations in Phase I. The number of EV parking/charging stations shall be increased in minimum groups of 8 up to 1 percent based on usage. Electrical conduit to support additional charging stations (resulting in a supply of charging stations of up to 3 percent of the total on-site parking spaces) will be installed throughout the Shopping Center site, as is deemed appropriate during initial construction, for future conversion based on usage. The EV parking/charging stations shall be reviewed by the City and RREEF on an annual basis and will evaluate usage, and phasing of future installation of additional EV parking/charging stations. An annual report on charging station use shall be submitted to the Director of Public Works for review and approval, to determine whether evidence supports demand for the phasing and future installation of EV parking/charging stations. The stations shall provide a Level 2 charging capacity (120-240 volts, or as required by Southern California Edison), may charge prevailing rates for the purchase of the energy, and the parking spaces will be designated for the exclusive use of EV charging. RREEF shall submit plans to the Community Development Department with the submittal of plans for each parking structure. The City will review and approve the Plan, and RREEF shall install the improvements per the approved Plan with each parking structure.

39. **Sepulveda Boulevard.** The retention, modification, relocation and/or removal of the existing Fry's driveway off Sepulveda Boulevard that accesses the Northwest Corner parcel is subject to review and approval of Caltrans and the City Public Works, Fire, Police and Community Development Departments.

RREEF shall reimburse the City the \$12,455 cost of the Caltrans required Traffic Stimulation Study that evaluated the impact of the Fry's driveway to the traffic flow on Sepulveda Boulevard.

The retention, modification, relocation, and/or removal of the existing Fry's driveway off Sepulveda Boulevard that accesses the Northwest Corner may be phased as follows: (a) Through the end of 2016, or when Fry's vacates the site, whichever comes first, the existing driveway condition (entry and exit, right in and out) may remain; (b) At the end of 2016, or when Fry's vacates the site, whichever comes first, the driveway must be reconfigured/relocated to be entry, right-in only; (c) At the end of 2016, if Fry's continues to occupy the site or if at any time another tenant occupies the



existing site, the Sepulveda driveway must be reconfigured/relocated to be entry, right-in only; (d) If at any time the site is vacant the driveway shall be barricaded from use or removed; (e) If at any time the site is vacant for 12 months the driveway shall be removed. If the driveway is removed then the curb, gutter, sidewalk and any other required improvements shall be installed by RREEF as soon as possible, as determined by the City, unless building plans for Phase III have been approved; and (f) If the driveway is removed any future driveway for Phase III - Northwest Corner development shall be entry right-in only. Prior to December 31, 2016, plans for the driveway modifications or removal/relocation and related improvements shall be submitted to the City and Caltrans and shall include a schedule for completion of the improvement. The City will cooperate with RREEF to secure approvals affecting this Fry's Sepulveda driveway. The driveway modifications or removal/relocation and related improvements shall be completed by RREEF per the approved Plan. RREEF shall coordinate driveway modifications or removal/relocation with the Sepulveda Bridge widening project.

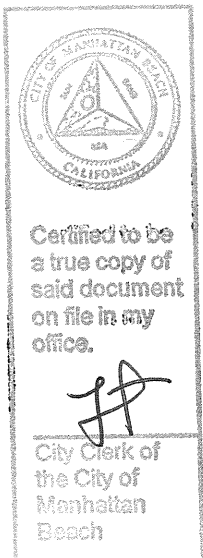
RREEF shall also be required to dedicate land or submit and record an irrevocable offer to dedicate (IOD) land, and construct, or fund the construction of, any required improvements related solely to the driveway on Sepulveda Boulevard, subject to the City of Manhattan Beach Public Works and Caltrans approval. The required lane width, sidewalk, driveway access design, disabled accessibility, and other improvement details shall be subject to City of Manhattan Beach Public Works and Community Development Departments and Caltrans approval. RREEF, City, and Caltrans shall coordinate improvements related to the Sepulveda Boulevard driveway with the Sepulveda Bridge widening project. The schedule for the dedication or IOD and related improvements shall be included with the Plans for the driveway modifications or removal/relocation. The City shall submit a Right-of-Way Map to RREEF, to indicate all of the required right-of-way, easements, and other information required by the dedication for the Sepulveda Boulevard bridge widening project RREEF by June 30, 2014.

RREEF shall also submit dedications, required for the Sepulveda bridge widening project, subject to the City Public Works and Community Development Departments and Caltrans review and approval. The final dedications shall be based on the final design of the Sepulveda Bridge. Dedications shall also include permanent dedications, permanent easement(s) for drainage and any other required utilities, and maintenance easements necessitated by the bridge widening.

RREEF shall also provide temporary construction easement(s) for the temporary construction staging area associated with the Sepulveda bridge widening project, subject to the City Public Works and Community Development Departments and Caltrans' review and approval. The temporary construction staging area shall be located in the lower level parking lot immediately adjacent to the northeast of the bridge for bridge construction, and access from the staging area shall be provided through the lower level parking lot to Rosecrans Avenue. Access to the bridge and roadway for construction shall also be required from RREEF's property.

The City shall submit a Right-of-Way Map to RREEF, to indicate all of the required right-of-way, easements, and other information required by the dedication for the Sepulveda Boulevard bridge widening project by June 30, 2014. The dedications and easements shall be submitted prior to the submittal of plans for Phase I to plan check, or October 31, 2014, whichever comes first. The City and Caltrans, if Caltrans requires, will review and approve the dedication and easements, and RREEF shall implement the provisions as detailed in the approval.

RREEF shall provide an irrevocable offer to dedicate (IOD) at the southeast corner of Sepulveda Boulevard and 33rd Street to accommodate improvements for a future right-turn pocket/deceleration lane from northbound Sepulveda Boulevard to eastbound 33rd Street prior to issuance of permits for Phase I. RREEF shall submit plans for the improvements to the Public Works, Fire, Police, and Community Development Departments and the City Traffic Engineer, for review and approval with the submittal



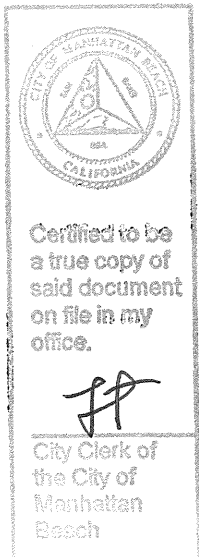
of plans for Phase I. RREEF shall dedicate the property and construct the improvements per plans approved by the City in connection with the construction of Phase I.

40. **Rosecrans Avenue.** RREEF shall provide an irrevocable offer to dedicate (IOD), for a new acceleration/deceleration lane and improved sidewalk on the south side of Rosecrans Avenue, beginning a minimum of 160 feet west of the future Cedar Way extension to the easternmost driveway serving the lower level parking lot off of Rosecrans Avenue prior to issuance of permits for Phase I. The IOD shall provide for a 12 foot curb lane width and 8 foot sidewalk; however, the sidewalk shall be continuous from Sepulveda Boulevard to Village Drive. RREEF shall submit plans for the improvements to the Public Works, Fire, Police and Community Development Departments and the City Traffic Engineer, for review and approval, for the eastern portion serving as a turn lane into the lower level parking driveway with the submittal of plans for Phase I. RREEF shall submit plans for the improvements to the Public Works, Fire, Police and Community Development Departments and the City Traffic Engineer, for review and approval, for the portion adjacent to the Cedar Way extension with Phase II and for the easternmost driveway with the submittal of plans for Phase II, or six months following the vacation of Fry's from the site, whichever comes first. RREEF shall dedicate the property and construct the eastern portion serving as a turn lane into the lower level parking driveway per plans approved by the City in connection with the construction of Phase I. In connection with the construction of Phase II, RREEF shall construct the portion adjacent to the Cedar Way extension.

41. **Rosecrans Avenue Median.** The existing median break and left-turn pocket from westbound Rosecrans Avenue, to the existing Fry's driveway on the south side of Rosecrans Avenue that accesses the Northwest Corner parcel, shall be closed and restored/reconstructed as a median when Fry's vacates the site, or when Cedar Way is extended through to Rosecrans Avenue, whichever comes first. The existing median break and left-turn pocket from eastbound Rosecrans Avenue, into an existing curb-cut and driveway apron on the north side of Rosecrans Avenue shall also be closed and restored/reconstructed when Fry's vacates the site or when Cedar Way is extended through to Rosecrans Avenue, whichever comes first.

If the developer of The Point in El Segundo submits plans for the Rosecrans Avenue median prior to Fry's vacating the site or prior to the Cedar Way extension, the City will work cooperatively with RREEF, the City of El Segundo, and The Point developer to address the median break into Fry's driveway (westbound Rosecrans Avenue, southbound into the Fry's driveway) while Fry's occupies the site, to the satisfaction of the City Traffic Engineer. If the developer of The Point in El Segundo has not submitted plans for the Rosecrans Avenue median work when Fry's vacates the site, or prior to the Cedar Way extension, RREEF shall submit plans for the improvements to the Public Works, Fire, Police and Community Development Departments and the City Traffic Engineer, as well as the City of El Segundo if any of the improvements are located within that City, for review and approval. The improvement plans shall be submitted prior to Fry's vacating the site, unless Fry's vacates the site prior to December 2016, or prior to the Cedar Way extension, whichever first occurs, and the improvement plans shall include a schedule for the completion of the improvements. RREEF shall construct the improvements, or cause the improvements to be constructed, per Plans by the City.

42. **Rosecrans Avenue Left-turn Prohibitions.** On Rosecrans Avenue, no left turns are allowed out of any driveways or Cedar Way from the project site to westbound Rosecrans Avenue. RREEF shall submit plans for signage and other improvements required by the City and a schedule for completion to the Public Works, Police, Fire and Community Development Departments and the City Traffic Engineer, for review and approval, with the submittal of plans for Phase I. Any portions of the improvements within another jurisdiction shall also require a permit from that jurisdiction. RREEF shall install the improvements per the approved plans, in accordance with the City Traffic Engineers requirements.

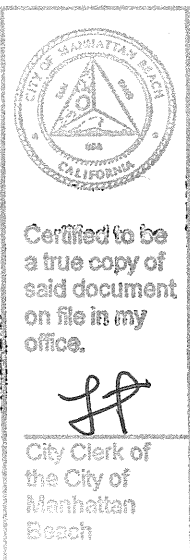


43. **Sepulveda Boulevard and Rosecrans Avenue Corner.** RREEF shall provide an irrevocable offer to dedicate (IOD) at the southeast corner of Sepulveda Boulevard and Rosecrans Avenue for future road and sidewalk widening with an 8 foot sidewalk width, corner improvements, including a 40 foot diagonal corner cut off measured from the back of the new sidewalks, ADA access, traffic signal and utility modifications and other improvements as needed to transition and tie together the Sepulveda Boulevard and Rosecrans Avenue improvements, and upgrade the area to current standards for pedestrian access, upon completion of the Sepulveda Bridge Widening, or the submittal of plans for Phase III, whichever comes first. RREEF shall submit concept plans for the improvements to the Public Works, Fire, Police and Community Development Departments, the City Traffic Engineer, and Caltrans for review and approval, with the submittal of the IOD, and shall include a schedule for the completion of the improvements. The schedule for completion of the improvements shall be coordinated with RREEF's construction associated with Sepulveda Boulevard (Fry's) driveway, the Rosecrans Avenue improvements, and other applicable improvements in the area including but not limited to construction of future Phase III. RREEF shall dedicate the property and construct the improvements per the plans approved by the City. While designing any improvements along Sepulveda Boulevard or at the corner of Sepulveda Boulevard and Rosecrans Avenue, the City shall take into consideration RREEF's desire to provide a right-in only turn from Sepulveda Boulevard into the Northwest Corner of the Shopping Center Property.

44. **Village Drive at Rosecrans Avenue Part I.** RREEF shall provide an irrevocable offer to dedicate (IOD) at the southwest corner of Rosecrans Avenue and Village Drive to accommodate improvements for future dual-left turn lanes and improved truck-turning radii from westbound Rosecrans Avenue to southbound Village Drive provided that the dedication and improvements will not impact the structural integrity or conformance with applicable Codes of the Medical Building at 1200 Rosecrans Avenue. The IOD and a concept plan for the improvements shall be submitted to the Public Works and Community Development Departments, and the City Traffic Engineer, prior to the first building permit being completed (building permit final) for Phase I, and shall include a schedule for the completion of the improvements. The schedule for completion of the improvements shall be coordinated with other planned improvements for the area, including additional improvements at the intersection of Rosecrans Avenue and Village Drive anticipated to be completed by the developer of The Point at El Segundo. RREEF shall dedicate the property and construct, or cause to be constructed, the improvements during construction of Phase I and/or as part of the westbound dual left turn lane improvements on Rosecrans, whichever first occurs, pursuant to plans approved by the City.

45. **Village Drive at Rosecrans Avenue Part II.** RREEF shall provide an irrevocable offer to dedicate (IOD) to provide for future road and sidewalk widening including a minimum of a six foot dedication on Village Drive, a 40 foot diagonal corner cut off, and a 12 foot dedication on Rosecrans Avenue, to accommodate a wider (6 foot to 8 foot) sidewalk, landscaping, disabled access ramps, traffic signal and utility modifications and other improvements on Village Drive and Rosecrans Avenue, as determined feasible from Traffic Engineering standards prior to the first building permit being completed (building permit final) for Phase I. This dedication would accommodate a total of two lanes Northbound and two lanes Southbound on Village Drive and the required corner transition improvements at Rosecrans Avenue and Village Drive if the Medical Building at 1200 Rosecrans Avenue is no longer at the Shopping Center property. If the Medical Building at 1200 Rosecrans Avenue is no longer at the Shopping Center property and the City determines that right-of-way improvements are needed, RREEF shall dedicate the property and shall provide a fair-share contribution to fund the construction of the improvements.

46. **Irrevocable Offer to Dedicate (IOD).** All IODs shall be recorded with the Los Angeles County Recorder's office. All IODs shall have a project description and include a general legal description, prepared by RREEF. All IODs shall be submitted to the City for review and approval and shall be recorded when required by the City as



set forth in the applicable Condition. The dedication of property included in an IOD shall include any temporary right of entry/access, temporary construction easements, utility easements, permanent dedications for roadway and bridge widening improvements, and permanent maintenance easements, in connection with the improvements required by the City per this Master Use Permit and the applicable Plan.

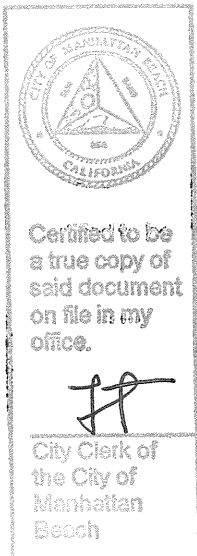
47. **Rosecrans Avenue U-turn at Village Drive.** The City and RREEF will work cooperatively to secure a "U-Turn" movement from eastbound Rosecrans Avenue at Village Drive if the U-turn can be designed to Traffic Engineering standards, all safety criteria is met, and traffic flow is not significantly impacted. RREEF is not required to install these improvements; however, if RREEF seeks to install these improvements, RREEF shall submit plans for the improvements to the Public Works, Police, Fire and Community Development Departments and the City Traffic Engineer, for review and approval. Any portions of the improvements within another jurisdiction shall also require a permit from that jurisdiction. RREEF shall install the improvements per plans approved by the City.

48. **Marine Avenue-Cedar Way.** The existing driveway access at Marine Avenue and Cedar Way shall be improved to provide one or two inbound lane and three outbound lanes, and shall be designed to accommodate emergency vehicle access. The widening shall include all related public and private improvements, and dedication of land if necessary, to accommodate the improvements. RREEF shall submit plans for the improvements to the Public Works, Fire, Police, and Community Development Departments and the City Traffic Engineer, for review and approval, with the submittal of plans for Phase I. RREEF shall construct the improvements per the plans approved by the City prior to the issuance of a certificate of occupancy for Phase I.

49. **Construction Traffic and Parking Management Plans.** The required Construction Parking Management Plan shall be implemented during all construction activity. The required Construction Traffic Management Plan shall address, but not be limited to the following; the management of all construction traffic during all phases of construction, including delivery of materials and parking of construction related vehicles; driver-less vehicles blocking neighbors' driveways without written authorization; the overnight storage of materials in the roadway; and limiting the hours of construction deliveries on weekend mornings where such activities including driving, parking and loading/unloading in areas adjacent to residential uses. The Construction Traffic Management Plan shall be coordinated with the traffic management plan for the Sepulveda Bridge widening project. RREEF shall submit the Plan, and an implementation schedule to the Public Works, Fire, Police, and Community Development Departments and the City Traffic Engineer, for review and approval, with the submittal of plans for Phase I. RREEF shall implement the Plan in accordance with a schedule approved by the City.

50. **Traffic, Circulation, and Parking Plan.** A Traffic, Circulation, and Parking Plan for all parking and roadway striping, signage, pavement treatment (including sharrow markings), pedestrian and bike access shall be provided throughout the Shopping Center property as depicted on the Approved Plans. The Traffic, Circulation, and Parking Plan shall include but not be limited to the following features:

- a. Compact parking spaces shall not be allowed unless approved by the Director of Community Development in limited situations when there are no other design options and the compact spaces will maximize use of the parking structure or lot.
- b. Installation of disabled access parking spaces that exceed the minimum number of required spaces, evenly distributed throughout the site at convenient locations.
- c. Parking structures shall have a minimum of two vehicle entry-exit points and three if over 600 spaces, and shall provide parking occupancy systems with



permanent electronic displays in proximity to parking structure entrances showing unoccupied spaces on each level.

d. Parking shall be provided at a minimum ratio of 4.1 spaces per 1,000 square feet of gross leasable floor area (GLA).

e. Parking shall not be reserved for any particular user, except for disabled parking spaces, EV charging stations, van/car pool spaces, or low emitting vehicles as designated in the approved Employee Parking Management Plan, including in instances where designated parking is required in a tenant's lease, and any Valet Parking Plans.

f. Passenger loading zones shall be provided near the Village Shops.

g. At a minimum, the central core portion of Cedar Way (between buildings "E" and "F" and the main Mall building) shall be constructed with decorative pavement. Curbs, landscaping, bollards or other architectural or hardscaping improvements shall be used to prevent vehicles from driving onto pedestrian only walkways. Stopping, parking and loading shall be prohibited in the decorative pavement area, but accessed by vehicles through the decorative pavement area shall be permitted.

h. Separate pedestrian walkways shall be provided to all parking structures.

i. Truck loading spaces shall be provided close to all buildings.

j. RREEF shall provide a U-turn, traffic circle or other connection at the Rosecrans Avenue entrance in the lower level parking lot with a minimum outside turning radius of 30 feet to internally connect both drive aisles.

k. Northbound left-turn pockets shall be provided on Carlotta Way at 27th and 30th Street entry points. An east-west two-way internal drive aisle will be provided as far south as feasible between Carlotta Way and Cedar Way. No dead-end aisles may be permitted.

l. Cedar Way, Carlotta Way and Fashion Boulevard shall have a minimum 25 foot width for adequate vehicle circulation and turning movements. Roadways with separate bike lanes (not sharrows) shall provide a minimum 30 foot roadway width.

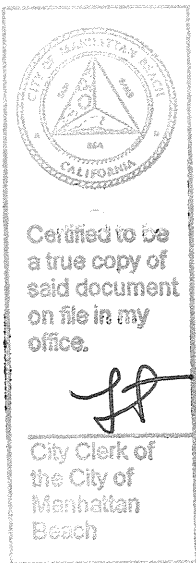
m. Fashion Boulevard at Carlotta Way, shall be designed to line up east to west and not be off-set to the satisfaction of the City Traffic Engineer.

n. The driveway access between the lower level parking and Carlotta Way, north of the 3500 Sepulveda Boulevard building, shall be revised to minimize the sharp angle.

o. RREEF shall work cooperatively with the City Traffic Engineer to minimize conflicts and improve visibility and safety with the location of parking spaces with direct access onto internal private streets (Cedar, Fashion and Carlotta) and onto accessways leading into parking structures.

p. With the extension of Cedar Way to Rosecrans Avenue, the existing Fry's driveway, access on Rosecrans Avenue, and parking lot shall be designed and reconfigured as needed to meet the requirements of the City Traffic Engineer.

q. The North Parking Structure shall include a stairway and elevator on the west side of the parking deck to provide external access.



r. The North Parking Structure shall be limited to G+2, with level 2 set-back 90 feet from the western edge of the parking structure's footprint (so that the western-most 90 feet of the parking structure essentially is capped at G+1).

s. Thirty additional parking spaces shall be provided on the west side of the lower level parking lot with pedestrian access to the 3500 Sepulveda building.

t. A right turn/deceleration lane from northbound Sepulveda Boulevard at 33rd Street shall be provided into the Project site.

RREEF shall submit plans for the improvements, and an implementation schedule to the Public Works, Fire, Police, and Community Development Departments and the City Traffic Engineer, for review and approval, with the submittal of plans for the applicable Phase. RREEF shall construct the improvements per the Plan approved by the City, prior to the issuance of a building permit final for the applicable Phase.

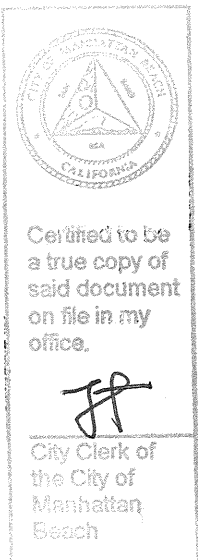
51. **Transit Plan.** RREEF shall submit a Transit Plan to provide a transit route through the Shopping Center property between Rosecrans Avenue and Village Drive via Fashion Boulevard with the plans for Phase II. The plans for Phases II and III shall be consistent with the Transit Plan. RREEF shall coordinate with transit providers and the City to provide a transit route through the Shopping Center including cooperating on grant applications and the design and implementation of improvements within the Shopping Center property to accommodate the transit route. If a transit provider agrees to route through the Shopping Center, RREEF shall make the necessary improvements within the Shopping Center site to accommodate transit through turning radius, clearance, transit stops, shelters, linkages, signage, and similar improvements. Public transit improvements, as detailed above, shall be installed on the property, and on adjacent public property if feasible, providing connectivity on and off-site with transit, pedestrians and bikes. If a transit provider agrees to route through the Shopping Center, RREEF shall construct the improvements, or cause the improvements to be constructed, per the Plan approved by the City.

52. **Oak and Cedar Avenues Traffic Study.** RREEF has offered to voluntarily fund the cost, up to \$20,000 for the City to evaluate non-residential traffic issues on Oak Avenue and Cedar Avenue. The study area shall be determined by the City, but shall focus on the corridor along Oak Avenue between Manhattan Beach Boulevard and 33rd Street and Cedar Avenue between 18th Street and Marine Avenue, and other streets as deemed necessary by the City. The study scope shall include, but not be limited to, cut-through traffic, commercial parking, and speeding. The study will evaluate traffic issues, recommend options to address the issues and include temporary measures, monitoring, follow-up studies, and permanent improvements as needed. The funds for the study shall be submitted by RREEF with the submittal of the first set of plans to plan check for Phase I or initiation of the study, whichever comes first, and returned to RREEF at the end of 12 months if the study is not initiated by the City.

53. **Financial Security for Off-site Improvements.** RREEF shall submit to the City a cost estimate for completion of all of the required off-site improvements, including but not limited to the traffic and public improvements and the Veterans Parkway connection and improvements, with the submittal of the first set of plans to plan check for Phase I. If the City accepts the final cost estimate, RREEF shall provide a bond or other financial security, equal to 1.25 times the estimated cost of the improvements, acceptable to the satisfaction of the Finance Director, Director of Public Works and the City Attorney, prior to the issuance of building permits for Phase I.

WASTEWATER /UTILITIES

54. **Cleaning Outside.** No outside cleaning of kitchen floor mats or shopping carts will be permitted on the site. All kitchen floor mats shall be cleaned in such a manner that the run-off wastewater drains only to a private sewer drain on the premises.



55. **Grease Inceptors and Trash Enclosure Plan.** RREEF shall upgrade any existing grease inceptors to current standards, as feasible, in areas of new construction. RREEF shall also upgrade any existing trash enclosures to provide covers, and adequate room for solid waste, recyclables and food waste recycling. Existing trash enclosures shall also be tied into sanitary sewers, if feasible. RREEF shall work with Waste Management, or the current waste provider, and Public Works to develop a Plan for the improvements to the existing facilities. RREEF shall then submit plans for the improvements to the Public Works, Fire and Community Development Departments, for review and approval, with the submittal of plans for Phase I and shall include a schedule for the completion of the improvements. RREEF shall construct the improvements, or cause the improvements to be constructed, per the Plan as approved by the City, in connection with each phase of construction.

56. **Utilities.** All private utilities on the site shall be maintained by the property owner not the City.

SECTION 19. The time within which judicial review, if available, of this decision must be sought is governed by California Code of Civil Procedure Section 1094.6, unless a shorter time is provided by other applicable law. The City Clerk shall mail by first class mail, postage prepaid, a certified copy of this Resolution and a copy of the affidavit or certificate of mailing to RREEF, 3500 Sepulveda and any other persons or entities requesting notice of the decision.

SECTION 20. The City Clerk shall certify to the adoption of this Resolution.

PASSED, APPROVED AND ADOPTED this 2nd day of December, 2014.

Ayes: Howorth, Lesser and Mayor Powell
Noes: D'Errico and Burton
Absent: None
Abstain: None



Wayne Powell, Mayor
City of Manhattan Beach

Attest:



Liza Tamura, City Clerk

(SEAL)



Certified to be a true copy
of the original of said
document on file in my
office.



City Clerk of the City of
Manhattan Beach, California

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Attachment C. Vicinity Map

3200 N. Sepulveda Boulevard



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Manhattan Village Shopping Center MUP Amendment

LEGAL DESCRIPTION

(CHICAGO TITLE COMPANY ORDER NO. 00197412-987-OC1-K27 DATED AUGUST 24, 2023)

THE LAND REFERRED TO HEREIN BELOW IS SITUATED IN THE CITY OF MANHATTAN BEACH, IN THE COUNTY OF LOS ANGELES, STATE OF CALIFORNIA, AND IS DESCRIBED AS FOLLOWS:

PARCEL 1:

THAT PORTION OF LOT 4 IN SECTION 19, TOWNSHIP 3 SOUTH, RANGE 14 WEST, SAN BERNARDINO BASE AND MERIDIAN, IN THE CITY OF MANHATTAN BEACH, COUNTY OF LOS ANGELES, STATE OF CALIFORNIA, AS SHOWN ON PARTITION MAP SHOWING PROPERTY FORMERLY OF REDONDO LAND COMPANY, SUBDIVIDED BY JAMES F. TOWELL, C.A. EDWARDS AND P.P. WILCOX, COMMISSIONER, SURVEYED AUGUST, 1897, BY L. FRIEL AND FILED IN THE OFFICE OF THE COUNTY RECORDER OF SAID COUNTY ON SEPTEMBER 3, 1897 DESCRIBED AS FOLLOWS:

BEGINNING AT THE NORTHWEST CORNER OF SAID SECTION 19; THENCE SOUTH 0' 04' 16" EAST ALONG THE WEST LINE THEREOF, 77.04 FEET; THENCE NORTH 89' 55' 44" EAST PERPENDICULAR TO SAID WEST LINE 20.00 FEET TO THE TRUE POINT OF BEGINNING; THENCE SOUTH 0' 04' 16" EAST PARALLEL TO SAID WEST LINE 415.97 FEET TO A POINT IN THE SOUTH LINE OF SAID LOT 4; THENCE NORTH 89' 58' 45" EAST ALONG SAID SOUTH LINE 48.35 FEET, TO A POINT IN THE NORTHWESTERLY LINE OF THE 100 FOOT WIDE RIGHT OF WAY OF THE SOUTHERN CALIFORNIA RAILWAY COMPANY PER BOOK D-508 PAGE 76, OFFICIAL RECORDS OF SAID COUNTY, SAID POINT BEING A POINT IN A CURVE CONCAVE SOUTHEASTERLY AND HAVING A RADIUS OF 5779.65 FEET, A RADIAL LINE PASSING THROUGH SAID POINT BEARS NORTH 38' 19' 56" WEST; THENCE NORTHEASTERLY ALONG SAID CURVE AN ARC LENGTH OF 626.67 FEET THROUGH A CENTRAL ANGLE OF 6' 12' 45"; THENCE TANGENT TO SAID CURVE AND CONTINUING ALONG SAID NORTHWESTERLY LINE OF SAID RIGHT OF WAY NORTH 57' 53' 02" EAST 154.19 FEET TO A POINT IN THE SOUTHERLY LINE OF THE NORTHERLY 50.00 FEET OF SAID SECTION 19, SAID POINT ALSO BEING A POINT IN THE SOUTHERLY LINE OF ROSECRANS AVENUE, 100 FEET WIDE AS SAID AVENUE EXISTING ON NOVEMBER 29, 1979; THENCE SOUTH 89' 58' 45" WEST ALONG SAID SOUTHERLY LINE 664.17 FEET TO THE BEGINNING OF A TANGENT CURVE CONCAVE SOUTHEASTERLY AND HAVING A RADIUS OF 27.00 FEET; THENCE WESTERLY, SOUTHWESTERLY AND SOUTHERLY ALONG SAID CURVE AN ARC LENGTH OF 42.43 FEET THROUGH A CENTRAL ANGLE OF 90' 03' 01" TO THE TRUE POINT OF BEGINNING.

EXCEPTING THEREFROM, THAT PORTION OF SAID LAND CONVEYED TO THE CITY OF MANHATTAN BEACH, A MUNICIPAL CORPORATION, BY DEEDS RECORDED OCTOBER 2, 1997 AS INSTRUMENT NO. 97-1521451 AND AS INSTRUMENT NO. 97-1521452, BOTH OF OFFICIAL RECORDS ALSO EXCEPTING THEREFROM THAT PORTION OF SAID LOT 4 DESCRIBED AS FOLLOWS:

BEGINNING AT THE NORTHWEST CORNER OF SAID SECTION 19; THENCE SOUTH 0' 04' 16" EAST ALONG THE WEST LINE THEREOF, 77.04 FEET; THENCE NORTH 89' 55' 44" EAST PERPENDICULAR TO SAID WEST LINE 20.00 FEET; THENCE SOUTH 0' 04' 16" EAST PARALLEL TO SAID WEST LINE 415.97 FEET TO A POINT IN THE SOUTH LINE OF SAID LOT 4; THENCE NORTH 89' 58' 45" EAST ALONG SAID SOUTH LINE 48.35 FEET TO A POINT IN THE NORTHWESTERLY LINE OF THE 100 FOOT WIDE RIGHT OF WAY OF THE SOUTHERN CALIFORNIA RAILWAY COMPANY PER BOOK D-508 PAGE 76, OFFICIAL RECORDS OF SAID COUNTY, SAID POINT BEING A POINT IN A CURVE CONCAVE SOUTHEASTERLY AND HAVING A RADIUS OF 5779.65 FEET, A RADIAL LINE PASSING THROUGH SAID POINT BEARS NORTH 38' 19' 43" WEST; THENCE NORTHEASTERLY ALONG SAID CURVE AN ARC LENGTH OF 532.95 FEET THROUGH A CENTRAL ANGLE OF 5' 17' 00" TO THE TRUE POINT OF BEGINNING; THENCE CONTINUING ALONG SAID NORTHWESTERLY LINE AND SAID CURVE AN ARC LENGTH OF 93.72 FEET THROUGH A CENTRAL ANGLE OF 0' 55' 45"; THENCE TANGENT TO SAID CURVE AND CONTINUING ALONG SAID NORTHWESTERLY LINE OF SAID RIGHT OF WAY

City of Manhattan Beach Environmental Information Form
And Master Application Form
Manhattan Village Shopping Center MUP Amendment

NORTH 57° 53' 02" EAST 154.19 FEET TO A POINT IN THE SOUTHERLY LINE OF THE NORTHERLY 50.00 FEET OF SAID SECTION 19, SAID POINT ALSO BEING A POINT IN THE SOUTHERLY LINE OF ROSECRANS AVENUE, 100 FEET WIDE AS SAID AVENUE EXISTING ON NOVEMBER 29, 1979; THENCE SOUTH 89° 58' 45" WEST ALONG SAID SOUTHERLY LINE 209.57 FEET; THENCE LEAVING SAID SOUTHERLY LINE OF ROSECRANS AVENUE SOUTH 132.37 TO THE TRUE POINT OF BEGINNING.

SAID DESCRIPTION DESCRIBED AS "EXHIBIT A LEGAL DESCRIPTION FOR PARCEL 2" OF THE CERTIFICATE OF LOT LINE ADJUSTMENT RECORDED SEPTEMBER 13, 2018 AS INSTRUMENT NO. 20180943008, OF OFFICIAL RECORDS.

PARCEL 2:

PARCELS 1, 3 THROUGH 9 INCLUSIVE, AND PARCELS 13, 15, 16, 18 THROUGH 21 INCLUSIVE, IN THE CITY OF MANHATTAN BEACH, COUNTY OF LOS ANGELES, STATE OF CALIFORNIA, AS SHOWN ON PARCEL MAP NO. 12219, FILED IN BOOK 122 PAGES 33 THROUGH 35 INCLUSIVE OF PARCEL MAPS, IN THE OFFICE OF THE COUNTY RECORDER OF SAID COUNTY.

EXCEPT ALL OIL, GAS AND OTHER HYDROCARBONS, GEOTHERMAL RESOURCES AS DEFINED IN SECTION 6903 OF THE CALIFORNIA PUBLIC RESOURCES CODE AND ALL OTHER MINERALS, WHETHER SIMILAR TO THOSE HEREIN SPECIFIED OR NOT, WITHIN OR THAT MAY BE PRODUCED FROM THE PROPERTY; PROVIDED, HOWEVER THAT ALL RIGHTS AND INTEREST IN THE SURFACE OF THE PROPERTY HAVE BEEN CONVEYED TO GRANTEE, NO RIGHTS OR INTEREST OF ANY KIND THEREIN, EXPRESS OR IMPLIED, BEING EXCEPTED OR RESERVED TO GRANTOR EXCEPT AS THEREINAFTER EXPRESSLY SET FORTH THEREIN.

ALSO EXCEPT THE SOLE AND EXCLUSIVE RIGHT FROM TIME TO TIME TO DRILL AND MAINTAIN WELLS OR OTHER WORKS INTO OR THROUGH PROPERTY BELOW A DEPTH OF 500 FEET AND TO PRODUCE, INJECT, STORE AND REMOVE FROM OR THROUGH SUCH WELLS OR WORKS, OIL, GAS AND OTHER SUBSTANCES OF WHATEVER NATURE, INCLUDING THE RIGHT TO PERFORM ANY AND ALL OPERATIONS DEEMED NECESSARY OR CONVENIENT FOR THE EXERCISE OF SUCH RIGHTS, AS RESERVED BY CHEVRON U.S.A., INC., A CALIFORNIA CORPORATION, RECORDED APRIL 19, 1979 AS INSTRUMENT' NO. 79-424732 OF OFFICIAL RECORDS, AS TO THAT PORTION OF SAID LAND, ACQUIRED BY DEED RECORDED APRIL 2, 1923 IN BOOK 1993 PAGE 351, OFFICIAL RECORDS.

PARCEL 3:

PARCEL 2, IN THE CITY OF MANHATTAN BEACH, COUNTY OF LOS ANGELES, STATE OF CALIFORNIA, AS SHOWN ON PARCEL MAP NO. 12219, FILED IN BOOK 122 PAGES 33 THROUGH 35 INCLUSIVE OF PARCEL MAPS, IN THE OFFICE OF THE COUNTY RECORDER OF SAID COUNTY.

TOGETHER WITH THAT PORTION OF PARCEL 22 OF SAID PARCEL MAP NO. 12219 DESCRIBED AS FOLLOWS:

BEGINNING AT THE SOUTHWEST CORNER OF SAID PARCEL 2, THENCE ALONG THE WESTERLY PROLONGATION OF THE SOUTH LINE OF SAID PARCEL 2, S89°56'03"W, 17.50 FEET; THENCE N0°03'57"W, 66.00 FEET TO THE SOUTH LINE OF PARCEL 3 OF SAID PARCEL MAP NO. 12219; THENCE ALONG SAID SOUTH LINE, N89°56'03"E, 17.50 FEET TO THE NORTHWEST CORNER OF SAID PARCEL 2; THENCE ALONG THE WEST LINE OF SAID PARCEL 2, S0°03'57"E, 66.00 FEET TO THE POINT OF BEGINNING.

ALSO TOGETHER WITH THAT PORTION OF PARCEL 22 OF SAID PARCEL MAP NO. 12219 DESCRIBED AS FOLLOWS:

City of Manhattan Beach Environmental Information Form
And Master Application Form
Manhattan Village Shopping Center MUP Amendment

BEGINNING AT THE SOUTHEAST CORNER OF SAID PARCEL 2, THENCE ALONG THE EASTERLY PROLONGATION OF THE SOUTH LINE OF SAID PARCEL 2, N89°56'03"E, 5.86 FEET; THENCE N0°03'57"W, 59.99 FEET; THENCE S89°56'03"W, 3.30 FEET; THENCE N0°03'57"W, 6.01 TO THE SOUTH LINE OF PARCEL 3 OF SAID PARCEL MAP NO. 12219; THENCE ALONG SAID SOUTH LINE, S89°56'03"W, 2.56 FEET TO THE NORTHEAST CORNER OF SAID PARCEL 2; THENCE ALONG THE EAST LINE OF SAID PARCEL 2, S0°03'57"E, 66.00 FEET TO THE POINT OF BEGINNING.

SAID DESCRIPTION DESCRIBED AS "EXHIBIT A LEGAL DESCRIPTION FOR PARCEL 2" OF THE CERTIFICATE OF LOT LINE ADJUSTMENT RECORDED NOVEMBER 29, 2018 AS INSTRUMENT NO. 20181203208, OF OFFICIAL RECORDS.

ALSO EXCEPT ALL OIL, GAS AND OTHER HYDROCARBONS, GEOTHERMAL RESOURCES AS DEFINED IN SECTION 6903 OF THE CALIFORNIA PUBLIC RESOURCES CODE AND ALL OTHER MINERALS, WHETHER SIMILAR TO THOSE HEREIN SPECIFIED OR NOT, WITHIN OR THAT MAY BE PRODUCED FROM THE PROPERTY; PROVIDED, HOWEVER THAT ALL RIGHTS AND INTEREST IN THE SURFACE OF THE PROPERTY HAVE BEEN CONVEYED TO GRANTEE, NO RIGHTS OR INTEREST OF ANY KIND THEREIN, EXPRESS OR IMPLIED, BEING EXCEPTED OR RESERVED TO GRANTOR EXCEPT AS THEREINAFTER EXPRESSLY SET FORTH THEREIN.

ALSO EXCEPT THE SOLE AND EXCLUSIVE RIGHT FROM TIME TO TIME TO DRILL AND MAINTAIN WELLS OR OTHER WORKS INTO OR THROUGH PROPERTY BELOW A DEPTH OF 500 FEET AND TO PRODUCE, INJECT, STORE AND REMOVE FROM OR THROUGH SUCH WELLS OR WORKS, OIL, GAS AND OTHER SUBSTANCES OF WHATEVER NATURE, INCLUDING THE RIGHT TO PERFORM ANY AND ALL OPERATIONS DEEMED NECESSARY OR CONVENIENT FOR THE EXERCISE OF SUCH RIGHTS, AS RESERVED BY CHEVRON U.S.A., INC., A CALIFORNIA CORPORATION, RECORDED APRIL 19, 1979 AS INSTRUMENT NO. 79-424732 OF OFFICIAL RECORDS, AS TO THAT PORTION OF SAID LAND, ACQUIRED BY DEED RECORDED APRIL 2, 1923 IN BOOK 1993 PAGE 351, OFFICIAL RECORDS.

PARCEL 4:

PARCEL 11, IN THE CITY OF MANHATTAN BEACH, COUNTY OF LOS ANGELES, STATE OF CALIFORNIA, AS SHOWN ON PARCEL MAP NO. 12219, FILED IN BOOK 122 PAGES 33 THROUGH 35 INCLUSIVE OF PARCEL MAPS, IN THE OFFICE OF THE COUNTY RECORDER OF SAID COUNTY.

EXCEPTING THEREFROM THAT PORTION OF PARCEL 11, IN THE CITY OF MANHATTAN BEACH, COUNTY OF LOS ANGELES, STATE OF CALIFORNIA, AS SHOWN ON PARCEL MAP NO. 12219, FILED IN BOOK 122 PAGES 33 THROUGH 35 INCLUSIVE OF PARCEL MAPS, IN THE OFFICE OF THE COUNTY RECORDER OF SAID COUNTY, DESCRIBED AS FOLLOWS:

BEGINNING AT THE SOUTHEAST CORNER OF SAID PARCEL 11, THENCE ALONG THE EASTERLY LINE OF SAID PARCEL 11, N0°03'58"W 56.54 FEET TO THE BEGINNING OF A TANGENT CURVE CONCAVE TO THE SOUTHEAST HAVING A RADIUS OF 278.00 FEET; THENCE NORTHERLY THROUGH A CENTRAL ANGLE OF 10°53'16", AN ARC LENGTH OF 52.83 FEET; THENCE LEAVING SAID EASTERLY LINE, S89°57'42"W 92.94 FEET; THENCE N0°00'23"W 56.93 FEET; THENCE N29°26'38"W 57.53 FEET TO THE NORTHWESTERLY LINE OF SAID PARCEL 11; THENCE ALONG THE LINES OF SAID PARCEL 11, S57°53'02"W 193.37 FEET; THENCE S0°03'57"E 113.53 FEET; THENCE N89°56'03"E 280.00 FEET TO THE POINT OF BEGINNING.

SAID DESCRIPTION DESCRIBED AS "EXHIBIT A LEGAL DESCRIPTION FOR PARCEL 3" OF THE CERTIFICATE OF LOT LINE ADJUSTMENT RECORDED SEPTEMBER 13, 2018 AS INSTRUMENT NO. 20180943008, OF OFFICIAL RECORDS.

**City of Manhattan Beach Environmental Information Form
And Master Application Form
Manhattan Village Shopping Center MUP Amendment**

ALSO EXCEPT ALL OIL, GAS AND OTHER HYDROCARBONS, GEOTHERMAL RESOURCES AS DEFINED IN SECTION 6903 OF THE CALIFORNIA PUBLIC RESOURCES CODE AND ALL OTHER MINERALS, WHETHER SIMILAR TO THOSE HEREIN SPECIFIED OR NOT, WITHIN OR THAT MAY BE PRODUCED FROM THE PROPERTY; PROVIDED, HOWEVER THAT ALL RIGHTS AND INTEREST IN THE SURFACE OF THE PROPERTY HAVE BEEN CONVEYED TO GRANTEE, NO RIGHTS OR INTEREST OF ANY KIND THEREIN, EXPRESS OR IMPLIED, BEING EXCEPTED OR RESERVED TO GRANTOR EXCEPT AS THEREINAFTER EXPRESSLY SET FORTH THEREIN.

ALSO EXCEPT THE SOLE AND EXCLUSIVE RIGHT FROM TIME TO TIME TO DRILL AND MAINTAIN WELLS OR OTHER WORKS INTO OR THROUGH PROPERTY BELOW A DEPTH OF 500 FEET AND TO PRODUCE, INJECT, STORE AND REMOVE FROM OR THROUGH SUCH WELLS OR WORKS, OIL, GAS AND OTHER SUBSTANCES OF WHATEVER NATURE, INCLUDING THE RIGHT TO PERFORM ANY AND ALL OPERATIONS DEEMED NECESSARY OR CONVENIENT FOR THE EXERCISE OF SUCH RIGHTS, AS RESERVED BY CHEVRON U.S.A., INC., A CALIFORNIA CORPORATION, RECORDED APRIL 19, 1979 AS INSTRUMENT' NO. 79-424732 OF OFFICIAL RECORDS, AS TO THAT PORTION OF SAID LAND, ACQUIRED BY DEED RECORDED APRIL 2, 1923 IN BOOK 1993 PAGE 351, OFFICIAL RECORDS.

PARCEL 5:

THAT PORTION OF PARCEL 23, IN THE CITY OF MANHATTAN BEACH, COUNTY OF LOS ANGELES, STATE OF CALIFORNIA, AS SHOWN ON PARCEL MAP NO. 12219, FILED IN BOOK 122 PAGES 33 THROUGH 35 INCLUSIVE OF PARCEL MAPS, IN THE OFFICE OF THE COUNTY RECORDER OF SAID COUNTY DESCRIBED AS FOLLOWS:

THAT PORTION OF PARCEL 1, IN THE CITY OF MANHATTAN BEACH, COUNTY OF LOS ANGELES, STATE OF CALIFORNIA, AS SHOWN ON LOT LINE ADJUSTMENT RECORDED AUGUST 2, 2018 AS INSTRUMENT NO. 20180779112, IN THE OFFICE OF THE COUNTY RECORDER OF SAID COUNTY BEING DESCRIBED AS FOLLOWS:

BEGINNING AT THE EASTERLY TERMINUS OF THAT CERTAIN COURSE IN PARCEL 2 OF SAID INSTRUMENT NO. 20180779112, HAVING A BEARING OF N89°56'03"E, A DISTANCE OF 49.81 FEET; THENCE ALONG THE LINES OF SAID PARCEL 2, S89°56'03"W 49.81 FEET; THENCE N0°05'28"W 109.09 FEET; THENCE S89°54'32"W 266.25 FEET; THENCE S0°05'28"W 105.52 FEET; THENCE LEAVING SAID PARCEL 2, WEST 61.23 FEET TO THE TRUE POINT OF BEGINNING; THENCE WEST 194.46 FEET; THENCE S45°00'00"W 9.90 FEET; THENCE SOUTH 317.30 FEET; THENCE WEST 73.76 FEET; THENCE SOUTH 174.69 FEET; THENCE EAST 70.37 FEET; THENCE NORTH 61.65 FEET; THENCE S89°48'25"E 204.85 FEET; THENCE NORTH 438.02 FEET TO THE TRUE POINT OF BEGINNING.

SAID DESCRIPTION DESCRIBED AS "EXHIBIT A LEGAL DESCRIPTION FOR PARCEL 4" OF THE CERTIFICATE OF LOT LINE ADJUSTMENT RECORDED SEPTEMBER 13, 2018 AS INSTRUMENT NO. 20180943008, OF OFFICIAL RECORDS.

EXCEPT ALL OIL, GAS AND OTHER HYDROCARBONS, GEOTHERMAL RESOURCES AS DEFINED IN SECTION 6903 OF THE CALIFORNIA PUBLIC RESOURCES CODE AND ALL OTHER MINERALS, WHETHER SIMILAR TO THOSE HEREIN SPECIFIED OR NOT, WITHIN OR THAT MAY BE PRODUCED FROM THE PROPERTY; PROVIDED, HOWEVER THAT ALL RIGHTS AND INTEREST IN THE SURFACE OF THE PROPERTY HAVE BEEN CONVEYED TO GRANTEE, NO RIGHTS OR INTEREST OF ANY KIND THEREIN, EXPRESS OR IMPLIED, BEING EXCEPTED OR RESERVED TO GRANTOR EXCEPT AS THEREINAFTER EXPRESSLY SET FORTH THEREIN.

ALSO EXCEPT THE SOLE AND EXCLUSIVE RIGHT FROM TIME TO TIME TO DRILL AND MAINTAIN WELLS OR OTHER WORKS INTO OR THROUGH PROPERTY BELOW A DEPTH OF 500 FEET AND TO PRODUCE, INJECT, STORE AND REMOVE FROM OR THROUGH SUCH WELLS OR WORKS, OIL, GAS

City of Manhattan Beach Environmental Information Form
And Master Application Form
Manhattan Village Shopping Center MUP Amendment

AND OTHER SUBSTANCES OF WHATEVER NATURE, INCLUDING THE RIGHT TO PERFORM ANY AND ALL OPERATIONS DEEMED NECESSARY OR CONVENIENT FOR THE EXERCISE OF SUCH RIGHTS, AS RESERVED BY CHEVRON U.S.A., INC., A CALIFORNIA CORPORATION, RECORDED APRIL 19, 1979 AS INSTRUMENT' NO. 79-424732 OF OFFICIAL RECORDS, AS TO THAT PORTION OF SAID LAND, ACQUIRED BY DEED RECORDED APRIL 2, 1923 IN BOOK 1993 PAGE 351, OFFICIAL RECORDS.

PARCEL 6:

PARCEL 1, IN THE CITY OF MANHATTAN BEACH, COUNTY OF LOS ANGELES, STATE OF CALIFORNIA, AS SHOWN ON LOT LINE ADJUSTMENT RECORDED SEPTEMBER 13, 2018 AS INSTRUMENT NO. 20180943008, IN THE OFFICE OF THE COUNTY RECORDER OF SAID COUNTY.

TOGETHER WITH PARCEL 1, IN THE CITY OF MANHATTAN BEACH, COUNTY OF LOS ANGELES, STATE OF CALIFORNIA, AS SHOWN ON LOT LINE ADJUSTMENT RECORDED NOVEMBER 29, 2018 AS INSTRUMENT NO. 20181203208, IN THE OFFICE OF THE COUNTY RECORDER OF SAID COUNTY.

TOGETHER WITH PARCEL 14, IN THE CITY OF MANHATTAN BEACH, COUNTY OF LOS ANGELES, STATE OF CALIFORNIA, AS SHOWN ON PARCEL MAP NO. 12219, FILED IN BOOK 122 PAGES 33 THROUGH 35 INCLUSIVE OF PARCEL MAPS, IN THE OFFICE OF THE COUNTY RECORDER OF SAID COUNTY.

TOGETHER WITH PARCEL 17, IN THE CITY OF MANHATTAN BEACH, COUNTY OF LOS ANGELES, STATE OF CALIFORNIA, AS SHOWN ON PARCEL MAP NO. 12219, FILED IN BOOK 122 PAGES 33 THROUGH 35 INCLUSIVE OF PARCEL MAPS, IN THE OFFICE OF THE COUNTY RECORDER OF SAID COUNTY.

EXCEPTING THEREFROM THAT PORTION OF THE ABOVE DESCRIBED LAND, DESCRIBED AS FOLLOWS:

BEGINNING AT THE NORTHEAST CORNER OF PARCEL 16 OF SAID PARCEL MAP NO. 12219; THENCE SOUTH 0°03'57" EAST 117.50 FEET TO THE SOUTHEAST CORNER OF SAID PARCEL 16; THENCE SOUTH 44°24'03" EAST 67.33 FEET TO THE TRUE POINT OF BEGINNING; THENCE EAST 76.17 FEET; THENCE NORTH 65.93 FEET; THENCE NORTH 89°59'50" WEST 76.17 FEET; THENCE SOUTH 65.94 FEET TO THE TRUE POINT OF BEGINNING.

ALSO EXCEPTING THEREFROM THAT PORTION OF THE ABOVE DESCRIBED LAND, DESCRIBED AS FOLLOWS:

BEGINNING AT THE NORTHEAST CORNER OF PARCEL 16 OF SAID PARCEL MAP NO. 12219; THENCE SOUTH 44°24'03" EAST 117.50 FEET TO THE SOUTHEAST CORNER OF SAID PARCEL 16; THENCE SOUTH 44°24'03" EAST 67.33 FEET; THENCE EAST 76.17 FEET TO THE TRUE POINT OF BEGINNING; THENCE NORTH 65.93 FEET; THENCE NORTH 89°59'50" WEST 5.46 FEET; THENCE, NORTH 186.50 FEET; THENCE WEST 31.43 FEET; THENCE NORTH 184.33 FEET; THENCE NORTH 89°59'50" WEST 39.28 FEET; THENCE NORTH 150.93 FEET; THENCE EAST 277.64 FEET; THENCE SOUTH 0°00'10" WEST 122.34 FEET; THENCE SOUTH 11°33'20" WEST 474.99 FEET; THENCE WEST 106.31 FEET TO THE TRUE POINT OF BEGINNING.

ALSO EXCEPTING THEREFROM THAT PORTION OF THE ABOVE DESCRIBED LAND, DESCRIBED AS FOLLOWS:

BEGINNING AT THE NORTHWEST CORNER OF PARCEL 19 OF SAID PARCEL MAP NO. 12219; THENCE SOUTH 0°03'57" EAST 100.00 FEET TO THE SOUTHWEST CORNER OF SAID PARCEL 19; THENCE SOUTH 0°03'57" EAST 61.00 FEET TO THE TRUE POINT OF BEGINNING; THENCE SOUTH 0°03'57"

City of Manhattan Beach Environmental Information Form
And Master Application Form
Manhattan Village Shopping Center MUP Amendment

EAST 100.00 FEET; THENCE NORTH 89°56'03" EAST 99.00 FEET; THENCE NORTH 0°03'57" WEST 100.00 FEET; THENCE SOUTH 89°56'03" WEST 99.00 FEET TO THE TRUE POINT OF BEGINNING.

ALSO EXCEPTING THEREFROM THAT PORTION OF SAID LAND, ALL THAT PORTION OF THE REAL PROPERTY CONVEYED TO THE CITY OF MANHATTAN BEACH PER THAT CERTAIN INSTRUMENT ENTITLED "GRANT DEED-DEED OF DEDICATION" RECORDED JUNE 25, 2002 AS INSTRUMENT NO. 02-1439469, OF OFFICIAL RECORDS.

ALSO EXCEPTING THEREFROM THAT PORTION OF SAID PARCEL 1 LOT LINE ADJUSTMENT RECORDED NOVEMBER 29, 2018 AS INSTRUMENT NO. 20181203208, THAT PORTION AS DESCRIBED IN THE OFFER OF DEDICATION RECORDED FEBRUARY 4, 1980 AS INSTRUMENT NO. 80-125713, OF OFFICIAL RECORDS.

SAID DESCRIPTION DESCRIBED AS "EXHIBIT A LEGAL DESCRIPTION FOR PARCEL 1" OF THE CERTIFICATE OF LOT LINE ADJUSTMENT RECORDED MAY 10, 2019 AS INSTRUMENT NO. 20190429681, OF OFFICIAL RECORDS.

ALSO EXCEPT ALL OIL, GAS AND OTHER HYDROCARBONS, GEOTHERMAL RESOURCES AS DEFINED IN SECTION 6903 OF THE CALIFORNIA PUBLIC RESOURCES CODE AND ALL OTHER MINERALS, WHETHER SIMILAR TO THOSE HEREIN SPECIFIED OR NOT, WITHIN OR THAT MAY BE PRODUCED FROM THE PROPERTY; PROVIDED, HOWEVER THAT ALL RIGHTS AND INTEREST IN THE SURFACE OF THE PROPERTY HAVE BEEN CONVEYED TO GRANTEE, NO RIGHTS OR INTEREST OF ANY KIND THEREIN, EXPRESS OR IMPLIED, BEING EXCEPTED OR RESERVED TO GRANTOR EXCEPT AS THEREINAFTER EXPRESSLY SET FORTH THEREIN.

ALSO EXCEPT THE SOLE AND EXCLUSIVE RIGHT FROM TIME TO TIME TO DRILL AND MAINTAIN WELLS OR OTHER WORKS INTO OR THROUGH PROPERTY BELOW A DEPTH OF 500 FEET AND TO PRODUCE, INJECT, STORE AND REMOVE FROM OR THROUGH SUCH WELLS OR WORKS, OIL, GAS AND OTHER SUBSTANCES OF WHATEVER NATURE, INCLUDING THE RIGHT TO PERFORM ANY AND ALL OPERATIONS DEEMED NECESSARY OR CONVENIENT FOR THE EXERCISE OF SUCH RIGHTS, AS RESERVED BY CHEVRON U.S.A., INC., A CALIFORNIA CORPORATION, RECORDED APRIL 19, 1979 AS INSTRUMENT NO. 79-424732 OF OFFICIAL RECORDS, AS TO THAT PORTION OF SAID LAND, ACQUIRED BY DEED RECORDED APRIL 2, 1923 IN BOOK 1993 PAGE 351, OFFICIAL RECORDS.

PARCEL 7:

THAT PORTION OF PARCELS 14 AND 17, IN THE CITY OF MANHATTAN BEACH, COUNTY OF LOS ANGELES, STATE OF CALIFORNIA, AS SHOWN ON PARCEL MAP NO. 12219, FILED IN BOOK 122 PAGES 33 THROUGH 35 INCLUSIVE OF PARCEL MAPS, IN THE OFFICE OF THE COUNTY RECORDER OF SAID COUNTY AND THAT PORTION OF PARCEL 1, IN THE CITY OF MANHATTAN BEACH, COUNTY OF LOS ANGELES, STATE OF CALIFORNIA, AS SHOWN ON LOT LINE ADJUSTMENT RECORDED SEPTEMBER 13, 2018 AS INSTRUMENT NO. 20180943008, IN THE OFFICE OF THE COUNTY RECORDER OF SAID COUNTY AND THAT PORTION OF PARCEL 1, IN THE CITY OF MANHATTAN BEACH, COUNTY OF LOS ANGELES, STATE OF CALIFORNIA, AS SHOWN ON LOT LINE ADJUSTMENT RECORDED NOVEMBER 29, 2018 AS INSTRUMENT NO. 20181203208, IN THE OFFICE OF THE COUNTY RECORDER OF SAID COUNTY, DESCRIBED AS FOLLOWS:

BEGINNING AT THE NORTHEAST CORNER OF PARCEL 16 OF SAID PARCEL MAP NO. 12219; THENCE SOUTH 0°03'57" EAST 117.50 FEET TO THE SOUTHEAST CORNER OF SAID PARCEL 16; THENCE SOUTH 44°24'03" EAST 67.33 FEET; THENCE EAST 76.17 FEET TO THE TRUE POINT OF BEGINNING; THENCE NORTH 65.93 FEET; THENCE NORTH 89°59'50" WEST 5.46 FEET; THENCE, NORTH 186.50 FEET; THENCE WEST 31.43 FEET; THENCE NORTH 184.33 FEET; THENCE NORTH 89°59'50" WEST 39.28 FEET; THENCE NORTH 150.93 FEET; THENCE EAST 277.64 FEET; THENCE SOUTH 0°00'10" WEST

City of Manhattan Beach Environmental Information Form
And Master Application Form
Manhattan Village Shopping Center MUP Amendment

122.34 FEET; THENCE SOUTH 11°33'20" WEST 474.99 FEET; THENCE WEST 106.31 FEET TO THE TRUE POINT OF BEGINNING.

SAID DESCRIPTION DESCRIBED AS "EXHIBIT A LEGAL DESCRIPTION FOR PARCEL 2" OF THE CERTIFICATE OF LOT LINE ADJUSTMENT RECORDED MAY 10, 2019 AS INSTRUMENT NO. 20190429681, OF OFFICIAL RECORDS.

EXCEPT ALL OIL, GAS AND OTHER HYDROCARBONS, GEOTHERMAL RESOURCES AS DEFINED IN SECTION 6903 OF THE CALIFORNIA PUBLIC RESOURCES CODE AND ALL OTHER MINERALS, WHETHER SIMILAR TO THOSE HEREIN SPECIFIED OR NOT, WITHIN OR THAT MAY BE PRODUCED FROM THE PROPERTY; PROVIDED, HOWEVER THAT ALL RIGHTS AND INTEREST IN THE SURFACE OF THE PROPERTY HAVE BEEN CONVEYED TO GRANTEE, NO RIGHTS OR INTEREST OF ANY KIND THEREIN, EXPRESS OR IMPLIED, BEING EXCEPTED OR RESERVED TO GRANTOR EXCEPT AS THEREINAFTER EXPRESSLY SET FORTH THEREIN.

ALSO EXCEPT THE SOLE AND EXCLUSIVE RIGHT FROM TIME TO TIME TO DRILL AND MAINTAIN WELLS OR OTHER WORKS INTO OR THROUGH PROPERTY BELOW A DEPTH OF 500 FEET AND TO PRODUCE, INJECT, STORE AND REMOVE FROM OR THROUGH SUCH WELLS OR WORKS, OIL, GAS AND OTHER SUBSTANCES OF WHATEVER NATURE, INCLUDING THE RIGHT TO PERFORM ANY AND ALL OPERATIONS DEEMED NECESSARY OR CONVENIENT FOR THE EXERCISE OF SUCH RIGHTS, AS RESERVED BY CHEVRON U.S.A., INC., A CALIFORNIA CORPORATION, RECORDED APRIL 19, 1979 AS INSTRUMENT NO. 79-424732 OF OFFICIAL RECORDS, AS TO THAT PORTION OF SAID LAND, ACQUIRED BY DEED RECORDED APRIL 2, 1923 IN BOOK 1993 PAGE 351, OFFICIAL RECORDS.

PARCEL 8:

THAT PORTION OF PARCEL 17, IN THE CITY OF MANHATTAN BEACH, COUNTY OF LOS ANGELES, STATE OF CALIFORNIA, AS SHOWN ON PARCEL MAP NO. 12219, FILED IN BOOK 122 PAGES 33 THROUGH 35 INCLUSIVE OF PARCEL MAPS, IN THE OFFICE OF THE COUNTY RECORDER OF SAID COUNTY AND THAT PORTION OF PARCEL 1, IN THE CITY OF MANHATTAN BEACH, COUNTY OF LOS ANGELES, STATE OF CALIFORNIA, AS SHOWN ON LOT LINE ADJUSTMENT RECORDED SEPTEMBER 13, 2018 AS INSTRUMENT NO. 20180943008, IN THE OFFICE OF THE COUNTY RECORDER OF SAID COUNTY AND THAT PORTION OF PARCEL 1, IN THE CITY OF MANHATTAN BEACH, COUNTY OF LOS ANGELES, STATE OF CALIFORNIA, AS SHOWN ON LOT LINE ADJUSTMENT RECORDED NOVEMBER 29, 2018 AS INSTRUMENT NO. 20181203208, IN THE OFFICE OF THE COUNTY RECORDER OF SAID COUNTY DESCRIBED AS FOLLOWS:

BEGINNING AT THE NORTHEAST CORNER OF PARCEL 16 OF SAID PARCEL MAP NO. 12219; THENCE SOUTH 0°03'57" EAST 117.50 FEET TO THE SOUTHEAST CORNER OF SAID PARCEL 16; THENCE SOUTH 44°24'03" EAST 67.33 FEET TO THE TRUE POINT OF BEGINNING; THENCE EAST 76.17 FEET; THENCE NORTH 65.93 FEET; THENCE NORTH 89°59'50" WEST 76.17 FEET; THENCE SOUTH 65.94 FEET TO THE TRUE POINT OF BEGINNING.

SAID DESCRIPTION DESCRIBED AS "EXHIBIT A LEGAL DESCRIPTION FOR PARCEL 3" OF THE CERTIFICATE OF LOT LINE ADJUSTMENT RECORDED MAY 10, 2019 AS INSTRUMENT NO. 20190429681, OF OFFICIAL RECORDS.

EXCEPT ALL OIL, GAS AND OTHER HYDROCARBONS, GEOTHERMAL RESOURCES AS DEFINED IN SECTION 6903 OF THE CALIFORNIA PUBLIC RESOURCES CODE AND ALL OTHER MINERALS, WHETHER SIMILAR TO THOSE HEREIN SPECIFIED OR NOT, WITHIN OR THAT MAY BE PRODUCED FROM THE PROPERTY; PROVIDED, HOWEVER THAT ALL RIGHTS AND INTEREST IN THE SURFACE OF THE PROPERTY HAVE BEEN CONVEYED TO GRANTEE, NO RIGHTS OR INTEREST OF ANY KIND

City of Manhattan Beach Environmental Information Form
And Master Application Form
Manhattan Village Shopping Center MUP Amendment

THEREIN, EXPRESS OR IMPLIED, BEING EXCEPTED OR RESERVED TO GRANTOR EXCEPT AS THEREINAFTER EXPRESSLY SET FORTH THEREIN.

ALSO EXCEPT THE SOLE AND EXCLUSIVE RIGHT FROM TIME TO TIME TO DRILL AND MAINTAIN WELLS OR OTHER WORKS INTO OR THROUGH PROPERTY BELOW A DEPTH OF 500 FEET AND TO PRODUCE, INJECT, STORE AND REMOVE FROM OR THROUGH SUCH WELLS OR WORKS, OIL, GAS AND OTHER SUBSTANCES OF WHATEVER NATURE, INCLUDING THE RIGHT TO PERFORM ANY AND ALL OPERATIONS DEEMED NECESSARY OR CONVENIENT FOR THE EXERCISE OF SUCH RIGHTS, AS RESERVED BY CHEVRON U.S.A., INC., A CALIFORNIA CORPORATION, RECORDED APRIL 19, 1979 AS INSTRUMENT' NO. 79-424732 OF OFFICIAL RECORDS, AS TO THAT PORTION OF SAID LAND, ACQUIRED BY DEED RECORDED APRIL 2, 1923 IN BOOK 1993 PAGE 351, OFFICIAL RECORDS.

PARCEL 9:

A PORTION OF PARCEL 1, IN THE CITY OF MANHATTAN BEACH, COUNTY OF LOS ANGELES, STATE OF CALIFORNIA, AS SHOWN ON LOT LINE ADJUSTMENT RECORDED NOVEMBER 29, 2018 AS INSTRUMENT NO. 20181203208, IN THE OFFICE OF THE COUNTY RECORDER OF SAID COUNTY, DESCRIBED AS FOLLOWS:

BEGINNING AT THE NORTHWEST CORNER OF PARCEL 19 OF SAID PARCEL MAP NO. 12219; THENCE SOUTH 0°03'57" EAST 100.00 FEET TO THE SOUTHWEST CORNER OF SAID PARCEL 19; THENCE SOUTH 0°03'57" EAST 61.00 FEET TO THE TRUE POINT OF BEGINNING; THENCE SOUTH 0°03'57" EAST 100.00 FEET; THENCE NORTH 89°56'03" EAST 99.00 FEET; THENCE NORTH 0°03'57" WEST 100.00 FEET; THENCE SOUTH 89°56'03" WEST 99.00 FEET TO THE TRUE POINT OF BEGINNING.

SAID DESCRIPTION DESCRIBED AS "EXHIBIT A LEGAL DESCRIPTION FOR PARCEL 4" OF THE CERTIFICATE OF LOT LINE ADJUSTMENT RECORDED MAY 10, 2019 AS INSTRUMENT NO. 20190429681, OF OFFICIAL RECORDS.

EXCEPT ALL OIL, GAS AND OTHER HYDROCARBONS, GEOTHERMAL RESOURCES AS DEFINED IN SECTION 6903 OF THE CALIFORNIA PUBLIC RESOURCES CODE AND ALL OTHER MINERALS, WHETHER SIMILAR TO THOSE HEREIN SPECIFIED OR NOT, WITHIN OR THAT MAY BE PRODUCED FROM THE PROPERTY; PROVIDED, HOWEVER THAT ALL RIGHTS AND INTEREST IN THE SURFACE OF THE PROPERTY HAVE BEEN CONVEYED TO GRANTEE, NO RIGHTS OR INTEREST OF ANY KIND THEREIN, EXPRESS OR IMPLIED, BEING EXCEPTED OR RESERVED TO GRANTOR EXCEPT AS THEREINAFTER EXPRESSLY SET FORTH THEREIN.

ALSO EXCEPT THE SOLE AND EXCLUSIVE RIGHT FROM TIME TO TIME TO DRILL AND MAINTAIN WELLS OR OTHER WORKS INTO OR THROUGH PROPERTY BELOW A DEPTH OF 500 FEET AND TO PRODUCE, INJECT, STORE AND REMOVE FROM OR THROUGH SUCH WELLS OR WORKS, OIL, GAS AND OTHER SUBSTANCES OF WHATEVER NATURE, INCLUDING THE RIGHT TO PERFORM ANY AND ALL OPERATIONS DEEMED NECESSARY OR CONVENIENT FOR THE EXERCISE OF SUCH RIGHTS, AS RESERVED BY CHEVRON U.S.A., INC., A CALIFORNIA CORPORATION, RECORDED APRIL 19, 1979 AS INSTRUMENT' NO. 79-424732 OF OFFICIAL RECORDS, AS TO THAT PORTION OF SAID LAND, ACQUIRED BY DEED RECORDED APRIL 2, 1923 IN BOOK 1993 PAGE 351, OFFICIAL RECORDS.

PARCEL 9A:

THE NON-EXCLUSIVE EASEMENTS FOR, INGRESS, EGRESS, PARKING, UTILITIES AND MAINTENANCE IN, TO, OVER, UNDER AND ACROSS, THE "COMMON AREA" ALL AS DESCRIBED AND SHOWN IN THAT CERTAIN CONSTRUCTION, OPERATION AND RECIPROCAL EASEMENT AGREEMENT DATED NOVEMBER 1, 1980, EXECUTED BY MANHATTAN BEACH COMMERCIAL

**City of Manhattan Beach Environmental Information Form
And Master Application Form
Manhattan Village Shopping Center MUP Amendment**

PROPERTIES, A GENERAL PARTNERSHIP, MANHATTAN HACIENDA PROPERTY CO., A GENERAL PARTNERSHIP AND FEDERATED DEPARTMENT STORES, INC., A DELAWARE CORPORATION, WITH ADDENDUM EXECUTED BY BUFFUMS, INC., A CALIFORNIA CORPORATION, RECORDED ON NOVEMBER 25, 1980 AS INSTRUMENT NO. 80-1188655 OF OFFICIAL RECORDS.

THE INTEREST OF MANHATTAN BEACH COMMERCIAL PROPERTIES, A CALIFORNIA GENERAL PARTNERSHIP UNDER SAID AGREEMENT HAS BEEN ASSIGNED TO BANK OF AMERICA NATIONAL TRUST AND SAVINGS TRUST AND SAVINGS ASSOCIATION, AS TRUSTEE OF THE MASTER PENSION TRUST OF THE PACIFIC TELESIS GROUP, BY ASSIGNMENT DATED DECEMBER 24, 1986 AND RECORDED DECEMBER 24, 1986 AS INSTRUMENT NO. 86-1800316, OFFICIAL RECORDS.

THE INTEREST OF BANK OF AMERICA NATIONAL TRUST AND SAVINGS ASSOCIATION, AS TRUSTEE OF THE MASTER PENSION TRUST OF THE PACIFIC TELESIS GROUP UNDER SAID AGREEMENT HAS BEEN ASSIGNED TO MANHATTAN ORE HOLDING COMPANY, INC. BY UNRECORDED ASSIGNMENTS NOT APPEARING IN THE PUBLIC RECORD.

THE INTEREST OF MANHATTAN QRE HOLDING COMPANY, INC. UNDER SAID AGREEMENT HAS BEEN ASSIGNED TO MANHATTAN VILLAGE, LLC, A CALIFORNIA LIMITED LIABILITY COMPANY BY ASSIGNMENT DATED AUGUST 19, 1997 AND RECORDED AUGUST 20, 1997 AS INSTRUMENT NO. 97-1291551 OF OFFICIAL RECORDS.

THE EASEMENT RIGHTS OF MANHATTAN VILLAGE, LLC, A CALIFORNIA LIMITED LIABILITY COMPANY UNDER SAID AGREEMENT HAVE BEEN CONVEYED TO MADISON MANHATTAN VILLAGE L.P., A DELAWARE LIMITED PARTNERSHIP BY DEED RECORDED OCTOBER 3, 2000 AS INSTRUMENT NO. 00-1548302 OF OFFICIAL RECORDS.

THE INTEREST OF MADISON MANHATTAN VILLAGE L.P., A DELAWARE LIMITED PARTNERSHIP UNDER SAID AGREEMENT HAS BEEN ASSIGNED TO MADISON MANHATTAN VILLAGE, LLC, A DELAWARE LIMITED LIABILITY COMPANY BY ASSIGNMENT DATED JUNE 28, 2002 AND RECORDED JULY 8, 2002 AS INSTRUMENT NO. 02-1536001 OF OFFICIAL RECORDS.

THE INTEREST OF MADISON MANHATTAN VILLAGE, LLC HAS BEEN ASSIGNED OF RECORD TO RREEF AMERICA REIT II CORP. BBB, A MARYLAND CORPORATION, BY AN ASSIGNMENT AND ASSUMPTION OF GROUND LEASE RECORDED MAY 5, 2004 AS INSTRUMENT NO. 04-1123082 OF OFFICIAL RECORDS.

PARCEL 10:

PARCEL 12, IN THE CITY OF MANHATTAN BEACH, COUNTY OF LOS ANGELES, STATE OF CALIFORNIA,

AS SHOWN ON PARCEL MAP NO. 12219, FILED IN BOOK 122, PAGES 33 TO 35 INCLUSIVE OF PARCEL MAPS, IN THE OFFICE OF THE COUNTY RECORDER OF SAID COUNTY.

PARCEL 10A:

AN EASEMENT FOR THE MAINTENANCE AND USE OF A GASOLINE DOCK AND UNDERGROUND

GASOLINE STORAGE TANK OVER THAT PORTION OF PARCEL 23 IN THE CITY OF MANHATTAN BEACH, COUNTY OF LOS ANGELES, STATE OF CALIFORNIA, AS SHOWN ON A MAP FILED IN BOOK 122, PAGES 33 THROUGH 35 INCLUSIVE OF PARCEL MAPS, IN THE OFFICE OF THE COUNTY RECORDER OF LOS ANGELES COUNTY, CALIFORNIA, DESCRIBED AS FOLLOWS:

City of Manhattan Beach Environmental Information Form
And Master Application Form
Manhattan Village Shopping Center MUP Amendment

BEGINNING AT A PORTION ON THE WESTERLY LINE OF PARCEL 12 AS SHOWN ON SAID PARCEL MAP, DISTANT THEREON SOUTH 00°03'57" EAST 90.35 FEET FROM THE NORTHWESTERLY CORNER OF SAID PARCEL; THENCE NORTH 89° 56' 03" EAST 49.29 FEET TO A NON-TANGENT CURVE CONCAVE

SOUTHEASTERLY HAVING A RADIUS OF 240.00 FEET, A RADIAL TO SAID POINT BEARS NORTH 70°09'15" WEST; THENCE NORTHEASTERLY ALONG THE ARC OF SAID CURVE THROUGH A CENTRAL ANGLE OF 18°46'11" AN ARC DISTANCE OF 78.62 FEET; THENCE ALONG THE NORTHWESTERLY PROLONGATION OF A LINE RADIAL TO SAID CURVE NORTH 51° 23'04" WEST 43.89 FEET; THENCE SOUTH 52°39'30" WEST 67.02 FEET TO THE WESTERLY LINE OF SAID PARCEL 12; THENCE ALONG SAID WESTERLY LINE SOUTH 00°03'57" EAST 55.10 FEET, MORE OR LESS, TO THE POINT OF BEGINNING.

EXCEPT ALL OIL, GAS AND OTHER HYDROCARBONS, GEOTHERMAL RESOURCES AS DEFINED IN SECTION 6903 OF THE CALIFORNIA PUBLIC RESOURCES CODE AND ALL OTHER MINERALS, WHETHER SIMILAR TO THOSE HEREIN SPECIFIED OR NOT, WITHIN OR THAT MAY BE PRODUCED FROM THE PROPERTY; PROVIDED, HOWEVER, THAT ALL RIGHTS AND INTEREST IN THE SURFACE OF THE PROPERTY ARE HEREBY CONVEYED TO GRANTEE, NO RIGHTS OR INTEREST OF ANY KIND THEREIN, EXPRESS OR IMPLIED, BEING EXCEPTED OR RESERVED TO GRANTOR EXCEPT AS HEREINAFTER EXPRESSLY SET FORTH THEREIN.

ALSO EXCEPT, THE SOLE AND EXCLUSIVE RIGHT FROM TIME TO TIME TO DRILL AND MAINTAIN WELLS OR OTHER WORKS INTO OR THROUGH THE PROPERTY BELOW A DEPTH OF 500 FEET AND TO PRODUCE, INJECT, STORE AND REMOVE FROM OR THROUGH SUCH WELLS OR WORKS, OIL, GAS, AND OTHER SUBSTANCES OF WHATEVER NATURE, INCLUDING THE RIGHT TO PERFORM ANY AND ALL OPERATIONS DEEMED BY GRANTOR NECESSARY OR CONVENIENT FOR THE EXERCISE OF SUCH RIGHTS, AS RESERVED BY CHEVRON U.S.A. INC., A CALIFORNIA CORPORATION, RECORDED APRIL 19, 1979 AS INSTRUMENT NO. 79-424732, OFFICIAL RECORDS, AS TO THAT PORTION OF SAID LAND ACQUIRED BY DEED RECORDED APRIL 2, 1923 IN BOOK 1993, PAGE 351, OFFICIAL RECORDS.

PARCEL 10B:

THE NON-EXCLUSIVE EASEMENTS FOR INGRESS, EGRESS, PARKING, UTILITIES AND CONSTRUCTION IN, TO, OVER, UNDER AND ACROSS THE "COMMON AREA" ALL AS DESCRIBED AND SHOWN IN THAT CERTAIN CONSTRUCTION, OPERATION AND RECIPROCAL EASEMENT AGREEMENT DATED NOVEMBER 1, 1980, EXECUTED BY MANHATTAN BEACH COMMERCIAL PROPERTIES, A GENERAL PARTNERSHIP, MANHATTAN HACIENDA PROPERTY CO., A GENERAL PARTNERSHIP AND FEDERATED DEPARTMENTS STORES, INC., A DELAWARE CORPORATION, WITH ADDENDUM EXECUTED BY BUFFUMS, INC., A CALIFORNIA CORPORATION, RECORDED ON NOVEMBER 25, 1980 AS INSTRUMENT NO. 80-1188655 OFFICIAL RECORDS.

PARCEL 10C:

THE NON-EXCLUSIVE EASEMENT FOR VEHICULAR AND PEDESTRIAN ACCESS AND PARKING OVER THAT PORTION OF PARCEL 23 IN THE CITY OF MANHATTAN BEACH, COUNTY OF LOS ANGELES, STATE OF CALIFORNIA, AS SHOWN ON A MAP FILED IN BOOK 122, PAGES 33 THROUGH 35 INCLUSIVE OF PARCEL MAPS, IN THE OFFICE OF THE COUNTY RECORDER OF LOS ANGELES COUNTY, CALIFORNIA, AS DESCRIBED IN THAT CERTAIN GRANT DEED AND GRANT OF EASEMENT WITH COVENANTS RUNNING WITH THE LAND RECORDED NOVEMBER 25, 1980 AS INSTRUMENT NO. 80-1188654, OFFICIAL RECORDS.

ASSESSOR'S PARCEL NUMBER: 4138-020-014

City of Manhattan Beach Environmental Information Form
And Master Application Form
Manhattan Village Shopping Center MUP Amendment

PARCEL 11:

PARCEL 10, IN THE CITY OF MANHATTAN BEACH, COUNTY OF LOS ANGELES, STATE OF CALIFORNIA, AS SHOWN ON PARCEL MAP NO. 12219, FILED IN BOOK 122 PAGES 33 THROUGH 35 INCLUSIVE OF PARCEL MAPS, IN THE OFFICE OF THE COUNTY RECORDER OF SAID COUNTY.

TOGETHER WITH THAT PORTION OF PARCEL 23 OF SAID PARCEL MAP NO. 12219, BEING DESCRIBED AS FOLLOWS:

BEGINNING AT THE NORTHEAST CORNER OF SAID PARCEL 10, THENCE, ALONG THE NORTHERLY LINE OF SAID PARCEL 10, S89°56'03"W 49.81 FEET TO THE TRUE POINT OF BEGINNING;

THENCE CONTINUING ALONG SAID NORTHERLY LINE OF SAID PARCEL 10, S89°56'03"W 266.25 FEET;

THENCE N0°05'28"W 108.98 FEET;

THENCE N89°54'32"E 266.25 FEET;

THENCE S0°05'28"E 109.09 FEET TO THE TRUE POINT OF BEGINNING.

EXCEPTING THEREFROM THAT PORTION OF SAID PARCEL 10 LYING WESTERLY OF THE FOLLOWING DESCRIBED LINE:

BEGINNING AT THE NORTHEAST CORNER OF SAID PARCEL 10,

THENCE, ALONG THE NORTHERLY LINE OF SAID PARCEL 10, S89°56'03"W, 316.06 FEET TO THE TRUE POINT OF BEGINNING;

THENCE S0°05'28"E 40.07 FEET;

THENCE N90°00'00"E 6.52 FEET;

THENCE S0°01'30"E 64.00 FEET;

THENCE N90°00'00"W 5.00 FEET;

THENCE S0°01'30"E 35.00 FEET;

THENCE N90°00'00"E 5.00 FEET;

THENCE S0°01'30"E 78.36 FEET;

THENCE S45°27'45"E 24.30 FEET TO THE SOUTHERLY LINE OF SAID PARCEL 10.

SAID LAND IS SHOWN AS PARCEL 2 IN THAT CERTIFICATE OF LOT LINE ADJUSTMENT RECORDED ON JUNE 21, 2018 INSTRUMENT NO. 623215 AND AUGUST 2, 2018 INSTRUMENT NO. 779112.

PARCEL 11A:

A NON-EXCLUSIVE EASEMENT, OVER PORTIONS OF THE COMMON AREA OF THE RESPECTIVE OWNER'S "TRACTS", AS SAID "TRACTS" ARE DEFINED IN THAT CERTAIN INSTRUMENT ENTITLED "CONSTRUCTION, OPERATION AND RECIPROCAL EASEMENT AGREEMENT" RECORDED

**City of Manhattan Beach Environmental Information Form
And Master Application Form
Manhattan Village Shopping Center MUP Amendment**

NOVEMBER 25, 1980 AS INSTRUMENT NO. 80-1188655 OFFICIAL RECORDS, FOR THE PASSAGE AND ACCOMMODATION OF PEDESTRIANS AND VEHICLES, SANITARY SEWERS, STORM DRAINS, UNDERGROUND CONDUITS, AND FOR DEVELOPMENT AND CONSTRUCTION THEREOF.

PARCEL 11B: (EASEMENT ARTICLE I)

A NON-EXCLUSIVE EASEMENT FOR VEHICULAR AND PEDESTRIAN ACCESS, PARKING, DRIVEWAYS, SIGNS, UTILITIES, LOADING AND UNLOADING, AND OTHER USES AS MORE PARTICULARLY DESCRIBED IN DEED FROM MANHATTAN BEACH COMMERCIAL PROPERTIES, A GENERAL PARTNERSHIP, RECORDED NOVEMBER 25, 1980 AS INSTRUMENT NO. 80-1188654, OFFICIAL RECORDS, OVER THE LAND DESCRIBED THEREIN.

PARCEL 11C: (EASEMENT ARTICLE II)

A NON-EXCLUSIVE EASEMENT FOR INGRESS AND EGRESS INCLUDING VEHICULAR AND PEDESTRIAN TRAFFIC, UTILITIES, CONDUITS, CURBS, GUTTERS, DRIVEWAYS, LIGHT STANDARDS, OVER THE LAND DESCRIBED IN A GRANT DEED AND GRANT OF EASEMENTS WITH COVENANTS RUNNING WITH THE LAND RECORDED NOVEMBER 25, 1980 AS INSTRUMENT NO. 80-1188654.

APN: 4138-020-059

PARCEL 12:

PARCEL 4 OF PARCEL MAP NO. 12010, IN THE CITY OF MANHATTAN BEACH, COUNTY OF LOS ANGELES, STATE OF CALIFORNIA, AS PER MAP FILED IN BOOK 116, PAGES 75-76 IN THE OFFICE OF THE COUNTY RECORDER OF SAID COUNTY.

APN: 4138-018-024

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LATHAM & WATKINS LLP

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May 10, 2024

VIA CSS ONLINE PORTAL

Attn: Angelica Ochoa
 Community Development Department
 Manhattan Beach City Hall
 1400 Highland Avenue
 Manhattan Beach, CA 90266

Re: Manhattan Village Shopping Center: Application for Master Use Permit
 Amendment (Plan PE-24-00151) Response to Public Works Department
 Comments

Dear Ms. Ochoa:

We write on behalf of applicant RREEF America REIT II Corporation BBB (“RREEF”), the principal owner¹ and developer of the Manhattan Village Shopping Center (“Manhattan Village” or the “Shopping Center”) expansion project (“Project”). RREEF recently submitted an application to modify the Manhattan Village Master Use Permit (“MUP”). The February 5, 2024 submittal is a targeted request that seeks relief from Condition Nos. 39 and 50 which require a right-turn only lane from northbound Sepulveda Boulevard at 33rd Street (the “Right Turn Condition”) into the Shopping Center. The Right Turn Condition was included in the MUP back in December 2014 when the Shopping Center expansion project was originally approved.

This letter and the May 8, 2024 memorandum *Response to Public Works Department Comment on the Sepulveda Boulevard & 33rd Street Improvement Manhattan Village Shopping Center Project Manhattan Beach, California* prepared by traffic expert Gibson Transportation (“Gibson Response Memorandum”), attached hereto as **Exhibit A**, respond to comments received from the Public Works Department regarding the proposed Right Turn Condition amendment.

¹ The Hacienda Building parcel and Macy’s parcel are separately owned.

First, Public Works commented that “the updated findings . . . were previously known at the time of Project approval,” and that “pursu[ing] completion of the approved COAs . . . will allow better distribution of traffic . . . even if the effects do not show up in the capacity calculation.” (Gibson Response Memorandum, p. 1.) The comment suggests that evidence of actual traffic volumes should simply be ignored. But, imposing the Right Turn Condition, at great cost, in the face of evidence it is not needed to address traffic effects from the Project appears arbitrary and without nexus to effects caused the Shopping Center expansion. As discussed in the Gibson *Memorandum: Updated Transportation Review of the Sepulveda Boulevard & 33rd Street Right-turn Lane, Manhattan Village Shopping Center Project* dated January 22, 2024 submitted with RREEF’s application and attached as **Exhibit B** for convenience, the traffic volumes observed in 2023 were even lower than the traffic volumes predicted in the Project’s conservative environmental impact report traffic analysis. Consequently, we now know the actual effects in the real world from the Bridge Widening and the Shopping Center expansion. The Right Turn Condition was never intended to be a mitigation measure, and because the Sepulveda Bridge widening project had not been constructed when the Project was approved in 2014, its positive effect on traffic flow was not fully known. According to the Gibson Response Memorandum, based on the recently collected traffic data, the current 2023 traffic conditions during all peak hours operate at LOS B (“Very Good” conditions per the HCM² definition), and the Right Turn Condition is not needed to achieve “Very Good” LOS Conditions. (Gibson Response Memorandum, p. 2.) Perhaps most importantly, CalTrans has requested evidence that the right-turn only lane is warranted from a traffic and safety standpoint; such evidence cannot be provided. (*Ibid.*) In fact, the evidence demonstrates the opposite, the right-turn only lane is not warranted.

Second, although we understand that the nearby Gelson’s and Skechers projects constructed widened shoulder requirements along Sepulveda Boulevard, the turning movement data for those projects justified the addition of the widened shoulders, whereas the data here does not. As discussed in the Gibson Response Memorandum, the Gelson’s Market and Skechers Design Center and Offices have different site access schemes in terms of number of access points along Sepulveda Boulevard and traffic controls. (Gibson Response Memorandum, pp. 2-3.) Accordingly, Gelson’s and Skechers do not provide comparable precedent since their physical site access characteristics are not similar to MVSC. (*Ibid.*)

Third, we note that any deferral of Condition 39 (either to 5-year or 10-year increments, or to Phase III) would be inappropriate. It has already been 10 years since the 2014 City Council approval of Phases I and II of the Project, and approximately 3 years since the Phases I and II have been constructed. As already noted above and in the attached Gibson Transportation analysis, the traffic volumes observed in 2023 were *lower* than the traffic volumes predicted in the Project’s environmental impact report. Phase III was already included in the Project’s traffic analysis and even with Phase III, the Right Turn Condition was not needed as a mitigation measure. Nothing in the actual data collected suggests that the traffic conditions will warrant a right turn pocket in the future, even with Phase III. Accordingly, any deferral of Right Turn

² Highway Capacity Manual, 6th Edition (Transportation Research Board, 2016).

LATHAM & WATKINS LLP

Condition would be inconsistent with the evidence presented in this application. No evidence suggests otherwise.

Based on the foregoing and RREEF's prior submittal, we respectfully request that the City remove from Conditions Nos. 39 and 50 the work associated with the Right Turn Condition and release the Condition 39 related performance bond required under Condition 53.

Very truly yours,



Peter Gutierrez
of LATHAM & WATKINS LLP

Enclosures

cc:
Talyn Mirzakhaniyan, Community Development Director
Erik Zandvliet, Traffic Engineer
Mike Guerrero, Public Works Department
Quinn Barrow, City Attorney
Mark Cermak
Nicholas Zaharov

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MEMORANDUM

TO: Angelica Ochoa, City of Manhattan Beach
Mike Guerrero, City of Manhattan Beach

FROM: Sarah M. Drobis, P.E. and Casey Le, P.E.

DATE: May 8, 2024

RE: Response to Public Works Department Comment on the
Sepulveda Boulevard & 33rd Street Improvement
Manhattan Village Shopping Center Project
Manhattan Beach, California

Ref: J1106a

The City of Manhattan Beach (City) Public Works Department issued the following comment on March 20, 2024 regarding the Sepulveda Boulevard & 33rd Street right-turn lane improvement described in the Conditions of Approval (Council Resolution No. 14-0026) for the Manhattan Village Shopping Center (MVSC) Project (Project):

“The updated findings as summarized in the submitted Master Application Form were previously known at the time of the Project approval. None of the updated findings modify the conditions under which the Project was originally approved. Therefore, the Applicant should continue to pursue completion of the approved COAs to improve public safety and traffic circulation, as this will allow better distribution of traffic across the through lanes on Sepulveda Boulevard so the intersection will improve, even if the effects do not show up in the capacity calculation.”

The comment suggests that the latest findings of the Project’s Master Application Form, and in turn the findings of the latest transportation review, were previously known at the time of the Project approval and, as such, would not modify the conditions under which the Project was originally approved.

However, the comment does not consider the recent construction of the Sepulveda Boulevard bridge seismic retrofit and widening project, or Sepulveda Bridge Widening Project, undertaken by the City in coordination with the California Department of Transportation (Caltrans).

Subsequent to the preparation of the EIR and Project construction, the Sepulveda Bridge Widening Project converted the preexisting Sepulveda Boulevard northbound right-turn lane into MVSC at 33rd Street to a shared through/right-turn lane. This resulted in additional vehicle throughput capacity and improved overall operations along Sepulveda Boulevard through the Sepulveda Bridge to Rosecrans Avenue, as compared to the analysis of the environmental impact report (EIR).

It should be noted that the EIR and subsequent addenda prepared for the Project determined all transportation-related impacts would be less than significant, and, as such, the northbound right-turn lane improvement at Sepulveda Boulevard & 33rd Street is not a mitigation measure required as part of the Project.

Since the approval of the Project and EIR, the Project Applicant has continued to work with the City and Caltrans to comply with plan approvals and schedules related to the Sepulveda Boulevard & 33rd Street improvement measure. At the last meeting with Caltrans on August 3, 2023, which included the Project Applicant and City representatives, Caltrans could not support the Purpose and Need and requested additional documentation and technical review based on current traffic volumes to provide the justification of a widened shoulder along northbound Sepulveda Boulevard at 33rd Street.

Based on Caltrans' request, *Memorandum: Updated Transportation Review of the Sepulveda Boulevard & 33rd Street Right-turn Lane, Manhattan Village Shopping Center Project* (Gibson Transportation Consulting, Inc., January 22, 2024) (Updated Transportation Review) considered and analyzed the current lane geometry, vehicular and pedestrian volume conditions, and collision history, as well as the completion of both the Sepulveda Bridge Widening Project and the Project. Based on the findings of the Updated Transportation Review, it was concluded that a widened shoulder for a northbound right-turn lane is not warranted and would not meet Caltrans requirements to support the Purpose and Need. According to Caltrans, "[a] project's 'Need' is an identified transportation deficiency or problem, and its 'Purpose' is the set of objectives that will be met to address the transportation deficiency." (<https://dot.ca.gov/programs/environmental-analysis/environmental-management/purpose-need>) Here, there is no "transportation deficiency or problem," so there is no "Need".

The Updated Transportation Review details that the analyses previously identified at the time of the Project approval overestimated the anticipated traffic volumes that would utilize the Project driveway at Sepulveda Boulevard & 33rd Street, as well as the resulting level of service (LOS) operating conditions. MVSC provides multiple driveways along Sepulveda Boulevard, including two signalized driveways, at 30th Street and 33rd Street, and an additional driveway that provides access to the former Fry's Electronics. When traveling northbound on Sepulveda Boulevard from Marine Avenue, the signalized driveway at 33rd Street is the third and final driveway access to MVSC. As detailed in the Updated Transportation Review, the traffic volumes observed in October 2023 at Sepulveda Boulevard & 33rd Street were lower than the traffic volumes projected in the EIR under both Existing Conditions and projected Future Conditions. By comparison, the 2023 traffic volumes equate to approximately 24% fewer vehicles during the weekday afternoon peak hour and approximately 55% fewer vehicles during the Saturday midday peak hour as compared to the anticipated projected Future Conditions. The current 2023 traffic conditions during all peak hours operate at LOS B ("Very Good" conditions per the *Highway Capacity Manual, 6th Edition* [Transportation Research Board, 2016] definition) with a northbound approach of three through lanes and one shared through/right-turn lane.

Given the nominal right-turn volumes, a dedicated northbound right-turn lane at Sepulveda Boulevard & 33rd Street is not warranted to achieve "Very Good" LOS conditions or improvement measures under current or future conditions.

As described above, MVSC has several access points along Sepulveda Boulevard, two of which are signalized. By comparison, the Gelson's Market and Skechers Design Center and Offices

have different site access schemes in terms of traffic controls and the number of access points along Sepulveda Boulevard. As part of the Gelson's Market project, a widened shoulder was proposed and implemented along southbound Sepulveda Boulevard at its driveway. However, the driveway is unsignalized and, furthermore, this driveway is the only access point along Sepulveda Boulevard that provides direct access to the Gelson's Market. Similar to the Gelson's Market, Skechers Design Center and Offices has concentrated traffic at one driveway that provides direct access to the parking garage on Sepulveda Boulevard. These two developments do not provide comparable precedent since their physical site access characteristics are not similar to MVSC.

As such, a dedicated northbound right-turn lane at the signalized Project driveway located at Sepulveda Boulevard & 33rd Street is not warranted based on comparison of the site access and traffic conditions at these two nearby developments.

EXHIBIT B

MEMORANDUM

TO: Erik Zandvliet, City of Manhattan Beach Traffic Engineer

FROM: Sarah M. Drobis, P.E, and Casey Le, P.E.

DATE: January 22, 2024

RE: Updated Transportation Review of the
Sepulveda Boulevard & 33rd Street Right-turn Lane
Manhattan Village Shopping Center Project
Manhattan Beach, California

Ref: J1106a

Gibson Transportation Consulting, Inc. (GTC) was asked to provide an updated review of the Sepulveda Boulevard & 33rd Street intersection located in the City of Manhattan Beach (City) related to the need to construct a northbound right-turn only lane. This memorandum provides a summary of our review and evaluation.

BACKGROUND

The Manhattan Village Shopping Center (MVSC) project included the redevelopment of the approximately 44-acre shopping center (Project) located at 3200-3600 Sepulveda Boulevard in the City. The Project has been completed and is fully operational. Access to MVSC is provided via the existing driveways along Sepulveda Boulevard, Rosecrans Avenue, and Marine Avenue, including a driveway at the intersection of Sepulveda Boulevard & 33rd Street.

As part of the Sepulveda Boulevard (SR 1) bridge seismic retrofit and widening project, or Sepulveda Bridge Widening Project, undertaken by the City in coordination with the California Department of Transportation, the preexisting northbound right-turn lane on Sepulveda Boulevard into the MVSC at 33rd Street was converted to a shared through/right-turn lane. The resulting roadway configuration along northbound Sepulveda Boulevard is illustrated below.



As part of the Project's Conditions of Approval (Council Resolution No. 14-0026), the City required MVSC to provide an irrevocable offer to dedicate at the southeast corner of Sepulveda Boulevard & 33rd Street to accommodate the construction of a northbound right-turn only lane. It should be noted that the right-turn lane improvement is not a mitigation measure required as part of the Project's environmental impact report, which was certified by the City Council in December 2014 (Council Resolution No. 14-0026), nor in the subsequent addendums¹. The improvement measure described in Condition of Approval No. 39 is as follows:

RREEF shall provide an irrevocable offer to dedicate (IOD) at the southeast corner of Sepulveda Boulevard and 33rd Street to accommodate improvements for a future right-turn pocket-deceleration lane from northbound Sepulveda Boulevard to eastbound 33rd Street prior to the issuance of permits for Phase I. RREEF shall submit plans for the improvements to the Public Works, Fire, Police and Community Development Departments and the City Traffic Engineer, for review and approval with the submittal of plans for Phase I. RREEF shall dedicate the property and construct the improvements per plans approved by the City in connection with the construction of Phase I.

GTC has conducted this updated transportation review of recent traffic volumes and collision data at Sepulveda Boulevard & 33rd Street to evaluate whether the construction of a northbound right-turn lane is warranted now that the Project has been constructed and in operation.

ANALYSIS

Existing Traffic Volumes

Weekday morning and afternoon peak hour traffic counts were conducted on October 5, 2023 and Saturday midday peak hour traffic counts were conducted on October 7, 2023 at Sepulveda Boulevard & 33rd Street. The traffic count data is provided in Attachment A. The following hourly northbound right-turn vehicles from Sepulveda Boulevard onto 33rd Street, as well as the number of hourly pedestrians/bicyclists crossing the east leg of the intersection as the conflicting movement, were observed:

- Weekday morning peak hour (8 AM): 25 right-turning vehicles (approximately 1% of the total northbound approach volume), four pedestrians/bicyclists
- Weekday afternoon peak hour (4:30 PM): 57 right-turning vehicles (approximately 3% of the total northbound approach volume), seven pedestrians/ bicyclists
- Saturday midday peak hour (12 PM): 62 right-turning vehicles (approximately 4% of the total northbound approach volume), three pedestrians/ bicyclists

A total of 62 vehicles, or approximately one vehicle per minute, was observed making a right turn during a one-hour period (Saturday midday peak hour).

By comparison, the 2023 traffic volumes were lower than the traffic volumes outlined in the Project's environmental impact report under both Existing and Future Cumulative Conditions (i.e.,

¹ First Addendum approved in December 2016, Second Addendum approved in September 2017, Third Addendum approved in September 2019

Existing: 68 weekday afternoon peak hour, 126 Saturday midday peak hour and Future: 75 weekday afternoon peak hour, 138 Saturday midday peak hour).

Further, the number of pedestrians/bicyclists observed crossing 33rd Street (i.e., the conflicting movement with the northbound right-turn vehicles) is minimal. Although right-turn vehicles typically decelerate at the approach of a turn, the observed right-turn volumes do not substantially affect the flow of through-traffic along northbound Sepulveda Boulevard.

Level of Service (LOS) Operational Analysis

The existing Year 2023 traffic volumes were used as the basis for the LOS operational analysis. The *Highway Capacity Manual, 6th Edition* (Transportation Research Board, 2016) (HCM) methodology, which was implemented using Synchro software, was used to calculate the intersection delay and corresponding LOS operation at the signalized intersection. The HCM signalized methodology calculates the average delay, in seconds, for each vehicle passing through the intersection. The LOS categories range from excellent, nearly free-flow traffic (i.e., LOS A) to congested, stop and-go conditions (i.e., LOS F). As summarized below, the signalized intersection currently operates at acceptable LOS B conditions:

- Weekday morning peak hour (8 AM): 12.8 seconds of delay, LOS B
- Weekday afternoon peak hour (4:30 PM): 17.7 seconds of delay, LOS B
- Saturday midday peak hour (12 PM): 19.5 seconds of delay, LOS B

The LOS worksheets for each scenario are provided in Attachment B.

Collision History

Review of recent 2023 collision history data along Sepulveda Boulevard at and near 33rd Street was also conducted. Per the California Highway Patrol Statewide Integrated Traffic Records System database, and as summarized in Attachment C, only one accident along northbound Sepulveda Boulevard at and/or near 33rd Street was reported between January 2023 and October 2023. The reported accident occurred during the morning peak hour period (7:38 AM) and involved unsafe speeds that resulted in a rear-end collision. All other reported accidents either occurred further from the analyzed intersection or on the opposing side of the street (i.e., southbound Sepulveda Boulevard). The recent 2023 collision data does not show substantial evidence that the reported accidents were congestion-related incidents along northbound Sepulveda Boulevard during commuter peak hours.

Thus, the current lane configurations along northbound Sepulveda Boulevard, which includes three through lanes and one shared through/right-turn lane, do not substantially result in unsafe conditions along the adjacent roadway.

CONCLUSION

As detailed above, the existing (Year 2023) traffic volumes do not warrant a dedicated northbound right-turn lane at Sepulveda Boulevard & 33rd Street. The intersection currently operates at acceptable LOS conditions under existing conditions. Additionally, the recent collision history data does not demonstrate that reported accidents at and/or near the analyzed intersection result from congestion-related incidents along northbound Sepulveda Boulevard. As such, an exclusive northbound right-turn lane along Sepulveda Boulevard to 33rd Street is not required.

The Project's environmental impact report and subsequent addenda determined all transportation-related impacts would be less than significant. The modifications to the Project's Conditions of Approval would not change the square footages or uses approved for the Project and, thus, the modifications would not result in an increase of vehicle trips. In addition, the traffic volumes observed in 2023 were lower than the traffic volumes outlined in the Project's environmental impact report and, as such, the analyses and conclusions of the Project's environmental impact report were conservative. Furthermore, the right-turn lane along northbound Sepulveda Boulevard is not a mitigation measure required as part of the Project's environmental impact report. For these reasons, the findings of less than significant transportation-related impacts remain valid and the proposed modifications to the Project's Conditions of Approval would not result in any new or substantially greater environmental effects.

Attachment A
Traffic Count Data

Turning Movement Count Report AM

Location ID: 1
 North/South: Sepulveda Boulevard
 East/West: 33rd Street

Date: 10/05/23
 City: Manhattan Beach, CA

	Southbound			Westbound			Northbound			Eastbound			Totals:
	1	2	3	4	5	6	7	8	9	10	11	12	
Movements:	R	T	L	R	T	L	R	T	L	R	T	L	
7:00	0	199	6	2	0	2	3	518	0	2	6	18	756
7:15	0	255	4	3	0	1	6	447	0	0	5	28	749
7:30	0	282	1	4	0	1	2	542	0	3	4	24	863
7:45	0	334	6	2	0	1	2	488	0	4	9	56	902
8:00	0	308	5	6	0	3	7	622	0	2	10	36	999
8:15	0	307	8	3	0	0	6	617	0	0	13	48	1002
8:30	0	306	14	7	0	1	6	694	0	5	8	55	1096
8:45	0	303	13	7	0	4	6	617	0	2	20	55	1027

Total Volume:	0	2294	57	34	0	13	38	4545	0	18	75	320	7394
Approach %	0%	98%	2%	72%	0%	28%	1%	99%	0%	4%	18%	77%	

Peak Hr Begin:	8:00												
PHV	0	1224	40	23	0	8	25	2550	0	9	51	194	4124
PHF	0.988			0.705			0.920			0.825			

Prepared by City Count, LLC. (www.citycount.com)

Turning Movement Count Report PM

Location ID: 1
 North/South: Sepulveda Boulevard
 East/West: 33rd Street

Date: 10/05/23
 City: Manhattan Beach, CA

	Southbound			Westbound			Northbound			Eastbound			Totals:
	1	2	3	4	5	6	7	8	9	10	11	12	
Movements:	R	T	L	R	T	L	R	T	L	R	T	L	
16:00	0	576	28	23	0	11	10	361	0	9	8	41	1067
16:15	0	537	44	18	0	20	10	367	0	5	12	55	1068
16:30	0	585	32	22	0	13	20	378	0	7	17	46	1120
16:45	0	572	32	25	0	17	13	421	0	10	20	45	1155
17:00	0	564	32	20	0	17	14	353	0	7	16	45	1068
17:15	0	610	32	27	0	14	10	447	0	5	9	41	1195
17:30	0	564	46	32	0	15	18	387	0	5	16	31	1114
17:45	0	570	46	25	0	6	21	407	0	6	12	58	1151

Total Volume:	0	4578	292	192	0	113	116	3121	0	54	110	362	8938
Approach %	0%	94%	6%	63%	0%	37%	4%	96%	0%	10%	21%	69%	

Peak Hr Begin:	16:30												
PHV	0	2331	128	94	0	61	57	1599	0	29	62	177	4538
PHF	0.958			0.923			0.906			0.893			

Prepared by City Count, LLC. (www.citycount.com)

Pedestrian/Bicycle Count Report

Location ID: 1
 North/South: Sepulveda Boulevard
 East/West: 33rd Street

Date: 10/05/23
 City: Manhattan Beach, CA

Leg:	North		East		South		West	
Class:	Peds	Bicycle	Peds	Bicycle	Peds	Bicycle	Peds	Bicycle
7:00	0	0	0	0	0	0	0	0
7:15	0	0	0	0	1	0	0	0
7:30	0	0	1	0	1	1	0	0
7:45	0	0	0	0	0	2	0	2
8:00	0	0	1	0	0	0	0	0
8:15	0	0	0	0	0	1	0	0
8:30	0	0	0	0	1	0	1	0
8:45	0	0	2	1	2	0	2	0

Leg:	North		East		South		West	
Class:	Peds	Bicycle	Peds	Bicycle	Peds	Bicycle	Peds	Bicycle
16:00	0	0	0	1	1	0	1	0
16:15	0	0	0	0	0	0	0	0
16:30	0	0	1	0	1	0	1	0
16:45	0	0	3	0	3	1	1	1
17:00	0	0	1	1	2	0	4	0
17:15	0	0	1	0	1	0	3	0
17:30	0	0	0	0	5	2	3	1
17:45	0	0	0	0	4	0	1	0

Prepared by City Count, LLC. (www.citycount.com)

Turning Movement Count Report Mid-Day

Location ID: 1
 North/South: Sepulveda Boulevard
 East/West: 33rd Street

Date: 10/07/23
 City: Manhattan Beach, CA

	Southbound			Westbound			Northbound			Eastbound			Totals:
	1	2	3	4	5	6	7	8	9	10	11	12	
Movements:	R	T	L	R	T	L	R	T	L	R	T	L	
12:00	0	409	37	37	0	12	19	361	0	6	10	39	930
12:15	0	400	31	34	0	13	13	418	0	3	8	21	941
12:30	0	347	37	24	0	28	10	406	0	10	9	53	924
12:45	0	362	38	33	0	17	20	388	0	14	6	36	914
1:00	0	363	50	29	0	11	8	402	0	4	6	35	908
1:15	0	358	44	30	0	28	9	390	0	6	14	26	905
1:30	0	361	39	41	0	22	7	386	0	7	11	29	903
1:45	0	386	41	29	0	15	21	429	0	10	8	30	969

Total Volume:	0	2986	317	257	0	146	107	3180	0	60	72	269	7394
Approach %	0%	90%	10%	64%	0%	36%	3%	97%	0%	15%	18%	67%	

Peak Hr Begin:	12:00												
PHV	0	1518	143	128	0	70	62	1573	0	33	33	149	3709
PHF	0.931			0.952			0.948			0.747			

Prepared by City Count, LLC. (www.citycount.com)

Pedestrian/Bicycle Count Report

Location ID: 1
 North/South: Sepulveda Boulevard
 East/West: 33rd Street

Date: 10/07/23
 City: Manhattan Beach, CA

Leg:	<i>North</i>		<i>East</i>		<i>South</i>		<i>West</i>	
Class:	<i>Peds</i>	<i>Bicycle</i>	<i>Peds</i>	<i>Bicycle</i>	<i>Peds</i>	<i>Bicycle</i>	<i>Peds</i>	<i>Bicycle</i>
12:00	0	0	1	0	1	1	0	1
12:15	0	0	0	0	0	0	0	0
12:30	0	0	1	1	1	1	3	0
12:45	0	0	0	0	1	2	0	0
1:00	0	0	0	1	5	1	2	0
1:15	0	0	0	0	0	1	1	0
1:30	0	0	0	2	0	0	0	1
1:45	0	0	0	0	4	2	4	0

Prepared by City Count, LLC. (www.citycount.com)

Attachment B
LOS Worksheets

HCM 6th Signalized Intersection Summary

1: Sepulveda Blvd & 33rd St

10/24/2023



Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↔↔	↔		↔	↔↔	↔		↑↑↑		↔	↑↑↑	
Traffic Volume (veh/h)	194	51	9	8	0	23	0	2550	25	40	1224	0
Future Volume (veh/h)	194	51	9	8	0	23	0	2550	25	40	1224	0
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1870	1870	1870	1870	1870	1870	0	1870	1870	1870	1870	0
Adj Flow Rate, veh/h	211	55	10	6	0	28	0	2772	27	43	1330	0
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Percent Heavy Veh, %	2	2	2	2	2	2	0	2	2	2	2	0
Cap, veh/h	267	119	22	45	0	80	0	4938	48	56	4123	0
Arrive On Green	0.08	0.08	0.08	0.03	0.00	0.03	0.00	0.75	0.75	0.03	0.81	0.00
Sat Flow, veh/h	3456	1540	280	1781	0	3170	0	6882	64	1781	5274	0
Grp Volume(v), veh/h	211	0	65	6	0	28	0	2020	779	43	1330	0
Grp Sat Flow(s),veh/h/ln	1728	0	1820	1781	0	1585	0	1609	1859	1781	1702	0
Q Serve(g_s), s	9.0	0.0	5.1	0.5	0.0	1.3	0.0	27.4	27.5	3.6	10.2	0.0
Cycle Q Clear(g_c), s	9.0	0.0	5.1	0.5	0.0	1.3	0.0	27.4	27.5	3.6	10.2	0.0
Prop In Lane	1.00		0.15	1.00		1.00	0.00		0.03	1.00		0.00
Lane Grp Cap(c), veh/h	267	0	141	45	0	80	0	3599	1386	56	4123	0
V/C Ratio(X)	0.79	0.00	0.46	0.13	0.00	0.35	0.00	0.56	0.56	0.77	0.32	0.00
Avail Cap(c_a), veh/h	415	0	218	214	0	380	0	3599	1386	422	4123	0
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	0.00	1.00	1.00	0.00	1.00	0.00	1.00	1.00	1.00	1.00	0.00
Uniform Delay (d), s/veh	68.0	0.0	66.2	71.5	0.0	71.9	0.0	8.3	8.3	72.1	3.8	0.0
Incr Delay (d2), s/veh	5.5	0.0	2.3	1.3	0.0	2.6	0.0	0.6	1.7	19.1	0.2	0.0
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	4.2	0.0	2.5	0.2	0.0	0.6	0.0	9.2	11.0	1.9	3.2	0.0
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	73.5	0.0	68.6	72.8	0.0	74.5	0.0	9.0	10.0	91.2	4.0	0.0
LnGrp LOS	E	A	E	E	A	E	A	A	A	F	A	A
Approach Vol, veh/h		276			34			2799			1373	
Approach Delay, s/veh		72.3			74.2			9.3			6.7	
Approach LOS		E			E			A			A	
Timer - Assigned Phs	1	2		4		6		8				
Phs Duration (G+Y+Rc), s	9.2	116.4		16.1		125.6		8.3				
Change Period (Y+Rc), s	4.5	4.5		4.5		4.5		4.5				
Max Green Setting (Gmax), s	35.5	60.5		18.0		100.5		18.0				
Max Q Clear Time (g_c+I1), s	5.6	29.5		11.0		12.2		3.3				
Green Ext Time (p_c), s	0.1	26.2		0.6		14.8		0.0				

Intersection Summary

HCM 6th Ctrl Delay	12.8
HCM 6th LOS	B

Notes

User approved volume balancing among the lanes for turning movement.

HCM 6th Signalized Intersection Summary

1: Sepulveda Blvd & 33rd St

10/24/2023



Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations	↔↔	↔		↔	↔↔	↔		↑↑↑		↔	↑↑↑	
Traffic Volume (veh/h)	177	62	29	61	0	94	0	1599	57	128	2331	0
Future Volume (veh/h)	177	62	29	61	0	94	0	1599	57	128	2331	0
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1870	1870	1870	1870	1870	1870	0	1870	1870	1870	1870	0
Adj Flow Rate, veh/h	192	67	32	44	0	126	0	1738	62	139	2534	0
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Percent Heavy Veh, %	2	2	2	2	2	2	0	2	2	2	2	0
Cap, veh/h	253	88	42	98	0	174	0	4241	151	164	3992	0
Arrive On Green	0.07	0.07	0.07	0.05	0.00	0.05	0.00	0.66	0.66	0.09	0.78	0.00
Sat Flow, veh/h	3456	1196	571	1781	0	3170	0	6687	229	1781	5274	0
Grp Volume(v), veh/h	192	0	99	44	0	126	0	1305	495	139	2534	0
Grp Sat Flow(s),veh/h/ln	1728	0	1768	1781	0	1585	0	1609	1829	1781	1702	0
Q Serve(g_s), s	8.2	0.0	8.2	3.6	0.0	5.9	0.0	18.9	18.9	11.5	32.2	0.0
Cycle Q Clear(g_c), s	8.2	0.0	8.2	3.6	0.0	5.9	0.0	18.9	18.9	11.5	32.2	0.0
Prop In Lane	1.00		0.32	1.00		1.00	0.00		0.13	1.00		0.00
Lane Grp Cap(c), veh/h	253	0	129	98	0	174	0	3185	1207	164	3992	0
V/C Ratio(X)	0.76	0.00	0.77	0.45	0.00	0.72	0.00	0.41	0.41	0.85	0.63	0.00
Avail Cap(c_a), veh/h	417	0	213	215	0	383	0	3185	1207	338	3992	0
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	0.00	1.00	1.00	0.00	1.00	0.00	1.00	1.00	1.00	1.00	0.00
Uniform Delay (d), s/veh	68.2	0.0	68.2	68.7	0.0	69.8	0.0	11.9	11.9	67.1	7.1	0.0
Incr Delay (d2), s/veh	4.6	0.0	9.0	3.2	0.0	5.6	0.0	0.4	1.0	11.5	0.8	0.0
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	3.8	0.0	4.1	1.7	0.0	2.5	0.0	6.8	8.0	5.8	10.7	0.0
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	72.9	0.0	77.3	71.9	0.0	75.4	0.0	12.3	12.9	78.6	7.9	0.0
LnGrp LOS	E	A	E	E	A	E	A	B	B	E	A	A
Approach Vol, veh/h		291			170			1800			2673	
Approach Delay, s/veh		74.4			74.5			12.5			11.5	
Approach LOS		E			E			B			B	
Timer - Assigned Phs	1	2		4		6		8				
Phs Duration (G+Y+Rc), s	18.3	103.5		15.5		121.8		12.7				
Change Period (Y+Rc), s	4.5	4.5		4.5		4.5		4.5				
Max Green Setting (Gmax), s	28.5	67.3		18.1		100.3		18.1				
Max Q Clear Time (g_c+I1), s	13.5	20.9		10.2		34.2		7.9				
Green Ext Time (p_c), s	0.3	20.2		0.7		45.9		0.4				

Intersection Summary

HCM 6th Ctrl Delay	17.7
HCM 6th LOS	B

Notes

User approved volume balancing among the lanes for turning movement.

HCM 6th Signalized Intersection Summary

1: Sepulveda Blvd & 33rd St

10/24/2023



Movement	EBL	EBT	EBR	WBL	WBT	WBR	NBL	NBT	NBR	SBL	SBT	SBR
Lane Configurations												
Traffic Volume (veh/h)	149	33	33	70	0	128	0	1573	62	143	1518	0
Future Volume (veh/h)	149	33	33	70	0	128	0	1573	62	143	1518	0
Initial Q (Qb), veh	0	0	0	0	0	0	0	0	0	0	0	0
Ped-Bike Adj(A_pbT)	1.00		1.00	1.00		1.00	1.00		1.00	1.00		1.00
Parking Bus, Adj	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Work Zone On Approach		No			No			No			No	
Adj Sat Flow, veh/h/ln	1870	1870	1870	1870	1870	1870	0	1870	1870	1870	1870	0
Adj Flow Rate, veh/h	162	36	36	51	0	166	0	1710	67	155	1650	0
Peak Hour Factor	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92	0.92
Percent Heavy Veh, %	2	2	2	2	2	2	0	2	2	2	2	0
Cap, veh/h	219	54	54	121	0	215	0	4145	162	180	3976	0
Arrive On Green	0.06	0.06	0.06	0.07	0.00	0.07	0.00	0.65	0.65	0.10	0.78	0.00
Sat Flow, veh/h	3456	858	858	1781	0	3170	0	6662	251	1781	5274	0
Grp Volume(v), veh/h	162	0	72	51	0	166	0	1289	488	155	1650	0
Grp Sat Flow(s),veh/h/ln	1728	0	1716	1781	0	1585	0	1609	1825	1781	1702	0
Q Serve(g_s), s	6.9	0.0	6.2	4.1	0.0	7.7	0.0	19.3	19.3	12.9	15.9	0.0
Cycle Q Clear(g_c), s	6.9	0.0	6.2	4.1	0.0	7.7	0.0	19.3	19.3	12.9	15.9	0.0
Prop In Lane	1.00		0.50	1.00		1.00	0.00		0.14	1.00		0.00
Lane Grp Cap(c), veh/h	219	0	109	121	0	215	0	3125	1182	180	3976	0
V/C Ratio(X)	0.74	0.00	0.66	0.42	0.00	0.77	0.00	0.41	0.41	0.86	0.42	0.00
Avail Cap(c_a), veh/h	417	0	207	215	0	383	0	3125	1182	338	3976	0
HCM Platoon Ratio	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Upstream Filter(I)	1.00	0.00	1.00	1.00	0.00	1.00	0.00	1.00	1.00	1.00	1.00	0.00
Uniform Delay (d), s/veh	69.0	0.0	68.7	67.1	0.0	68.8	0.0	12.7	12.7	66.4	5.4	0.0
Incr Delay (d2), s/veh	4.8	0.0	6.7	2.3	0.0	5.8	0.0	0.4	1.1	11.4	0.3	0.0
Initial Q Delay(d3),s/veh	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
%ile BackOfQ(50%),veh/ln	3.2	0.0	2.9	2.0	0.0	3.3	0.0	7.0	8.2	6.4	5.3	0.0
Unsig. Movement Delay, s/veh												
LnGrp Delay(d),s/veh	73.8	0.0	75.3	69.4	0.0	74.5	0.0	13.1	13.8	77.8	5.8	0.0
LnGrp LOS	E	A	E	E	A	E	A	B	B	E	A	A
Approach Vol, veh/h		234			217			1777			1805	
Approach Delay, s/veh		74.3			73.3			13.3			11.9	
Approach LOS		E			E			B			B	
Timer - Assigned Phs	1	2		4		6		8				
Phs Duration (G+Y+Rc), s	19.7	101.6		14.0		121.3		14.7				
Change Period (Y+Rc), s	4.5	4.5		4.5		4.5		4.5				
Max Green Setting (Gmax), s	28.5	67.3		18.1		100.3		18.1				
Max Q Clear Time (g_c+I1), s	14.9	21.3		8.9		17.9		9.7				
Green Ext Time (p_c), s	0.3	19.7		0.6		22.2		0.5				

Intersection Summary

HCM 6th Ctrl Delay	19.5
HCM 6th LOS	B

Notes

User approved volume balancing among the lanes for turning movement.

Attachment C
SWITRS Collision Data



REPORT 8 - TOTAL COLLISIONS

01/01/2023 thru 10/04/2023

Total Count: 106

Jurisdiction(s): Manhattan Beach

Include State Highways cases

Report Run On: 10/04/2023

Primary Rd 11TH ST		Distance (ft) 0.00	Direction	Secondary Rd PECK AV	NCIC 1944	State Hwy? N	Route	Postmile Prefix	Postmile	Side of Hwy															
City Manhattan Beach	County Los Angeles	Population 4	Rpt Dist M7	Beat 0M1	Type 0	CalTrans	Badge 389	Collision Date 20230619	Time 1825	Day MON															
Primary Collision Factor STOP SGN SIG		Violation 22450	Collision Type BROADSIDE	Severity PDO	#Killed 0	#Injured 0	Tow Away? Y	Process Date 20230728																	
Weather1 CLEAR		Weather2	Rdwy Surface DRY	Rdwy Cond1 NO UNUSL CND	Rdwy Cond2	Spec Cond 0																			
Hit and Run MSDMNR		Motor Vehicle Involved With OTHER MV		Lighting DAYLIGHT	Ped Action	Cntrl Dev	FUNCTNG	Loc Type	Ramp/Int																
Party Info											Victim Info														
Party Type	Age	Sex	Race	Sobriety1	Sobriety2	Move Pre	Dir	SW Veh	CHP Veh	Make	Year	SP Info	OAF1	Viol	OAF2	Safety Equip	ROLE	Ext Of Inj	AGE	Sex	Seat Pos	Safety	EQUIP	Ejected	
1F	DRVR	31	F	B	IMP UNK	IMP UNK	PROC ST	E	A	0700	-	2016	-	A	21802	F	L	B							
2	DRVR	40	M	W	HNBD		PROC ST	S	A	0700	-	2022	-	3	N	-	M	G	PASS	6	F	4	0	M	Q
																	PASS	4	M	6	0	M	Q		
Primary Rd 11TH ST		Distance (ft) 2.00	Direction E	Secondary Rd SEPULVEDA BL	NCIC 1944	State Hwy? Y	Route 1	Postmile Prefix -	Postmile 22.841	Side of Hwy N															
City Manhattan Beach	County Los Angeles	Population 4	Rpt Dist M22	Beat 0M1	Type 0	CalTrans 7	Badge 329	Collision Date 20230609	Time 0734	Day FRI															
Primary Collision Factor OTHER HAZ		Violation 21663	Collision Type BROADSIDE	Severity INJURY	#Killed 0	#Injured 1	Tow Away? N	Process Date 20230705																	
Weather1 CLEAR		Weather2	Rdwy Surface DRY	Rdwy Cond1 NO UNUSL CND	Rdwy Cond2	Spec Cond 0																			
Hit and Run		Motor Vehicle Involved With BICYCLE		Lighting DAYLIGHT	Ped Action	Cntrl Dev	FUNCTNG	Loc Type I	Ramp/Int 5																
Party Info											Victim Info														
Party Type	Age	Sex	Race	Sobriety1	Sobriety2	Move Pre	Dir	SW Veh	CHP Veh	Make	Year	SP Info	OAF1	Viol	OAF2	Safety Equip	ROLE	Ext Of Inj	AGE	Sex	Seat Pos	Safety	EQUIP	Ejected	
1F	BICY	34	M	H	HNBD		PROC ST	N	L	0400	-	3	A	22350	M	-	-	BICY	OTH VIS	34	M	1	1	V	-
2	DRVR	32	M	H	HNBD		SLOWING	W	D	2200	FORD	2016	-	N	-	M	G	PASS		62	M	3	0	M	G
Primary Rd 12TH ST		Distance (ft) 142.	Direction E	Secondary Rd HIGHLAND AV	NCIC 1944	State Hwy? N	Route	Postmile Prefix	Postmile	Side of Hwy															
City Manhattan Beach	County Los Angeles	Population 4	Rpt Dist M12	Beat 0M3	Type 0	CalTrans	Badge 373	Collision Date 20230804	Time 1706	Day FRI															
Primary Collision Factor IMPROP TURN		Violation 22107	Collision Type AUTO/PED	Severity INJURY	#Killed 0	#Injured 1	Tow Away? N	Process Date 20230928																	
Weather1 CLEAR		Weather2	Rdwy Surface DRY	Rdwy Cond1 NO UNUSL CND	Rdwy Cond2	Spec Cond 0																			
Hit and Run		Motor Vehicle Involved With PED		Lighting DAYLIGHT	Ped Action NOT IN RD	Cntrl Dev	FUNCTNG	Loc Type	Ramp/Int																
Party Info											Victim Info														
Party Type	Age	Sex	Race	Sobriety1	Sobriety2	Move Pre	Dir	SW Veh	CHP Veh	Make	Year	SP Info	OAF1	Viol	OAF2	Safety Equip	ROLE	Ext Of Inj	AGE	Sex	Seat Pos	Safety	EQUIP	Ejected	
1F	DRVR	19	M		HNBD		LFT TURN	S	A	0100	-	2001	-	F	-	M	G	PASS		21	F	3	0	M	G
2	PED	35	M	H	HNBD		PROC ST	E	N	6000	-	-	-	N	-	-	-	PED	OTH VIS	35	M	-	-	P	-
Primary Rd 14TH ST		Distance (ft) 48.0	Direction W	Secondary Rd JOHN ST	NCIC 1944	State Hwy? N	Route	Postmile Prefix	Postmile	Side of Hwy															
City Manhattan Beach	County Los Angeles	Population 4	Rpt Dist M14	Beat 0M2	Type 0	CalTrans	Badge 386	Collision Date 20230610	Time 1923	Day SAT															
Primary Collision Factor UNSAFE SPEED		Violation 22350	Collision Type HIT OBJECT	Severity PDO	#Killed 0	#Injured 0	Tow Away? N	Process Date 20230721																	
Weather1 CLEAR		Weather2	Rdwy Surface DRY	Rdwy Cond1 NO UNUSL CND	Rdwy Cond2	Spec Cond 0																			
Hit and Run MSDMNR		Motor Vehicle Involved With FIXED OBJ		Lighting DAYLIGHT	Ped Action	Cntrl Dev	NT PRS/FCTR	Loc Type	Ramp/Int																
Party Info											Victim Info														
Party Type	Age	Sex	Race	Sobriety1	Sobriety2	Move Pre	Dir	SW Veh	CHP Veh	Make	Year	SP Info	OAF1	Viol	OAF2	Safety Equip	ROLE	Ext Of Inj	AGE	Sex	Seat Pos	Safety	EQUIP	Ejected	
1F	DRVR	998	-		IMP UNK	IMP UNK	PROC ST	W	A	0100	-	-	-	A	22450	-	-	-							

Include State Highways cases

Report Run On: 10/04/2023

Primary Rd 15TH ST		Distance (ft) 2.00	Direction E	Secondary Rd HIGHLAND AV	NCIC 1944	State Hwy? N	Route	Postmile Prefix	Postmile	Side of Hwy															
City Manhattan Beach	County Los Angeles	Population 4	Rpt Dist M12	Beat 003	Type 0	CalTrans	Badge 378	Collision Date 20230422	Time 0919	Day SAT															
Primary Collision Factor R-O-W AUTO		Violation 21801A	Collision Type HEAD-ON	Severity INJURY	#Killed 0	#Injured 1	Tow Away? Y	Process Date 20230608																	
Weather1 CLEAR		Weather2	Rdwy Surface DRY	Rdwy Cond1 NO UNUSL CND	Rdwy Cond2	Spec Cond 0	Hit and Run																		
Motor Vehicle Involved With OTHER MV		Lighting DAYLIGHT	Ped Action	Cntrl Dev	FUNCTNG	Loc Type	Ramp/Int																		
Party Info										Victim Info															
Party Type	Age	Sex	Race	Sobriety1	Sobriety2	Move Pre	Dir	SW Veh	CHP Veh	Make	Year	SP Info	OAF1	Viol	OAF2	Safety Equip	ROLE	Ext Of Inj	AGE	Sex	Seat Pos	Safety	EQUIP	Ejected	
1F	DRVR	27	F	H	HNBD	LFT TURN	W	A	0700	-	2013	-	3	N	-	M	G								
2	DRVR	71	F	W	HNBD	PROC ST	E	A	0100	-	2008	-	3	N	-	L	-	PASS	MINOR	85	M	3	0	L	G
Primary Rd 19TH ST		Distance (ft) 0.00	Direction	Secondary Rd OAK AV	NCIC 1944	State Hwy? N	Route	Postmile Prefix	Postmile	Side of Hwy															
City Manhattan Beach	County Los Angeles	Population 4	Rpt Dist M14	Beat 0M2	Type 0	CalTrans	Badge 383	Collision Date 20230531	Time 0804	Day WED															
Primary Collision Factor STOP SGN SIG		Violation 22450A	Collision Type BROADSIDE	Severity PDO	#Killed 0	#Injured 0	Tow Away? Y	Process Date 20230626																	
Weather1 CLEAR		Weather2	Rdwy Surface DRY	Rdwy Cond1 NO UNUSL CND	Rdwy Cond2	Spec Cond 0	Hit and Run																		
Motor Vehicle Involved With OTHER MV		Lighting DAYLIGHT	Ped Action	Cntrl Dev	FUNCTNG	Loc Type	Ramp/Int																		
Party Info										Victim Info															
Party Type	Age	Sex	Race	Sobriety1	Sobriety2	Move Pre	Dir	SW Veh	CHP Veh	Make	Year	SP Info	OAF1	Viol	OAF2	Safety Equip	ROLE	Ext Of Inj	AGE	Sex	Seat Pos	Safety	EQUIP	Ejected	
1F	DRVR	34	M	O	HNBD	PROC ST	S	A	0100	-	2021	-	3	A	22350	-	L	G							
2	DRVR	48	F	W	HNBD	PROC ST	E	A	0700	-	2023	-	3	N	-	M	G								
Primary Rd 1ST STREET		Distance (ft) 46.0	Direction W	Secondary Rd VALLEY DR	NCIC 1944	State Hwy? N	Route	Postmile Prefix	Postmile	Side of Hwy															
City Manhattan Beach	County Los Angeles	Population 4	Rpt Dist M19	Beat 0M2	Type 0	CalTrans	Badge 389	Collision Date 20230106	Time 1835	Day FRI															
Primary Collision Factor IMPROP TURN		Violation 22107	Collision Type SIDESWIPE	Severity PDO	#Killed 0	#Injured 0	Tow Away? N	Process Date 20230301																	
Weather1 CLEAR		Weather2	Rdwy Surface DRY	Rdwy Cond1 NO UNUSL CND	Rdwy Cond2	Spec Cond 0	Hit and Run																		
Motor Vehicle Involved With PKD MV		Lighting DARK - ST	Ped Action	Cntrl Dev	NT PRS/FCTR	Loc Type	Ramp/Int																		
Party Info										Victim Info															
Party Type	Age	Sex	Race	Sobriety1	Sobriety2	Move Pre	Dir	SW Veh	CHP Veh	Make	Year	SP Info	OAF1	Viol	OAF2	Safety Equip	ROLE	Ext Of Inj	AGE	Sex	Seat Pos	Safety	EQUIP	Ejected	
1F	DRVR	76	M	W	HBD-UNK	PROC ST	E	D	2200	FORD	2007	-	-	N	-	M	B								
2	PRKD	998	-	-	-	PARKED	E	D	2200	-	2019	-	-	N	-	-	-								
Primary Rd 27TH ST		Distance (ft) 5.00	Direction W	Secondary Rd SEPULVEDA BL	NCIC 1944	State Hwy? N	Route	Postmile Prefix	Postmile	Side of Hwy															
City Manhattan Beach	County Los Angeles	Population 4	Rpt Dist M7	Beat 0M4	Type 0	CalTrans	Badge 329	Collision Date 20230322	Time 0808	Day WED															
Primary Collision Factor R-O-W AUTO		Violation 21802A	Collision Type AUTO/PED	Severity INJURY	#Killed 0	#Injured 1	Tow Away?	Process Date 20230428																	
Weather1 CLEAR		Weather2	Rdwy Surface DRY	Rdwy Cond1 NO UNUSL CND	Rdwy Cond2	Spec Cond 0	Hit and Run																		
Motor Vehicle Involved With PED		Lighting DAYLIGHT	Ped Action X-WLK AT	Cntrl Dev	FUNCTNG	Loc Type	Ramp/Int																		
Party Info										Victim Info															
Party Type	Age	Sex	Race	Sobriety1	Sobriety2	Move Pre	Dir	SW Veh	CHP Veh	Make	Year	SP Info	OAF1	Viol	OAF2	Safety Equip	ROLE	Ext Of Inj	AGE	Sex	Seat Pos	Safety	EQUIP	Ejected	
1F	DRVR	50	M	W	HNBD	STOPPED	E	A	0100	AUDI	2018	-	-	N	-	M	G	PASS		18	F	3	0	M	G
2	PED	69	M	W	HNBD	PROC ST	N	N	6000	-	-	-	-	A	21954	-	-	PED	POSSIBL	69	M	-	0	-	P
Primary Rd 29TH AV		Distance (ft) 250.	Direction E	Secondary Rd BLANCHE RD	NCIC 1944	State Hwy? N	Route	Postmile Prefix	Postmile	Side of Hwy															
City Manhattan Beach	County Los Angeles	Population 4	Rpt Dist	Beat 003	Type 0	CalTrans	Badge 383	Collision Date 20230816	Time 1345	Day WED															
Primary Collision Factor STRTNG BCKNG		Violation 22106	Collision Type REAR END	Severity PDO	#Killed 0	#Injured 0	Tow Away? N	Process Date 20230919																	
Weather1 CLEAR		Weather2	Rdwy Surface DRY	Rdwy Cond1 CONS ZONE	Rdwy Cond2	Spec Cond 0	Hit and Run																		
Motor Vehicle Involved With OTHER MV		Lighting DAYLIGHT	Ped Action	Cntrl Dev	FUNCTNG	Loc Type	Ramp/Int																		
Party Info										Victim Info															
Party Type	Age	Sex	Race	Sobriety1	Sobriety2	Move Pre	Dir	SW Veh	CHP Veh	Make	Year	SP Info	OAF1	Viol	OAF2	Safety Equip	ROLE	Ext Of Inj	AGE	Sex	Seat Pos	Safety	EQUIP	Ejected	
1F	DRVR	56	M	H	IMP UNK	IMP UNK	BACKING	W	I	1100	-	2022	-	-	N	-	P	B							
2	DRVR	49	M	W	IMP UNK	IMP UNK	STOPPED	E	A	0100	-	2018	-	-	N	-	M	G							

Include State Highways cases

Report Run On: 10/04/2023

Primary Rd 35TH ST Distance (ft) 16.0 Direction W Secondary Rd ALMA AV NCIC 1944 State Hwy? N Route Postmile Prefix Postmile Side of Hwy City Manhattan Beach County Los Angeles Population 4 Rpt Dist M3 Beat 0M3 Type 0 CalTrans Badge 386 Collision Date 20230403 Time 2232 Day MON Primary Collision Factor IMPROP TURN Violation 22107 Collision Type REAR END Severity PDO #Killed 0 #Injured 0 Tow Away? N Process Date 20230609 Weather1 CLEAR Weather2 Rdwy Surface DRY Rdwy Cond1 NO UNUSL CND Rdwy Cond2 Spec Cond 0 Hit and Run MSDMNR Motor Vehicle Involved With PKD MV Lighting DARK - ST Ped Action Cntrl Dev NT PRS/FCTR Loc Type Ramp/Int														
Party Info Victim Info Party Type Age Sex Race Sobriety1 Sobriety2 Move Pre Dir SW Veh CHP Veh Make Year SP Info OAF1 Viol OAF2 Safety Equip ROLE Ext Of Inj AGE Sex Seat Pos Safety EQUIP Ejected 1F DRVR 998 - IMP UNK IMP UNK RGT TURN W E 2236 - 2018 - - N - - - 2 PRKD 998 - PARKED W A 0800 - 2004 - - N - - -														
Primary Rd 35TH ST Distance (ft) 0.00 Direction Secondary Rd MANHATTAN AV NCIC 1944 State Hwy? N Route Postmile Prefix Postmile Side of Hwy City Manhattan Beach County Los Angeles Population 4 Rpt Dist M2 Beat 0M8 Type 0 CalTrans Badge 386 Collision Date 20230528 Time 2125 Day SUN Primary Collision Factor OTHER HAZ Violation 21663 Collision Type HIT OBJECT Severity PDO #Killed 0 #Injured 0 Tow Away? N Process Date 20230626 Weather1 CLEAR Weather2 Rdwy Surface DRY Rdwy Cond1 NO UNUSL CND Rdwy Cond2 Spec Cond 0 Hit and Run MSDMNR Motor Vehicle Involved With FIXED OBJ Lighting DARK - ST Ped Action Cntrl Dev NT PRS/FCTR Loc Type Ramp/Int														
Party Info Victim Info Party Type Age Sex Race Sobriety1 Sobriety2 Move Pre Dir SW Veh CHP Veh Make Year SP Info OAF1 Viol OAF2 Safety Equip ROLE Ext Of Inj AGE Sex Seat Pos Safety EQUIP Ejected 1F DRVR 998 - IMP UNK IMP UNK OTHER E A 0700 - - - N - B - 2 PRKD 998 - PARKED N A 0100 - 2018 - - N - - -														
Primary Rd 36TH ST Distance (ft) 0.00 Direction Secondary Rd BAYVIEW DR NCIC 1944 State Hwy? N Route Postmile Prefix Postmile Side of Hwy City Manhattan Beach County Los Angeles Population 4 Rpt Dist M2 Beat 0M8 Type 0 CalTrans Badge 294 Collision Date 20230418 Time 1345 Day TUE Primary Collision Factor R-O-W AUTO Violation 21802A Collision Type BROADSIDE Severity PDO #Killed 0 #Injured 0 Tow Away? Y Process Date 20230523 Weather1 CLEAR Weather2 Rdwy Surface DRY Rdwy Cond1 NO UNUSL CND Rdwy Cond2 Spec Cond 0 Hit and Run Motor Vehicle Involved With OTHER MV Lighting DAYLIGHT Ped Action Cntrl Dev FNCTNG Loc Type Ramp/Int														
Party Info Victim Info Party Type Age Sex Race Sobriety1 Sobriety2 Move Pre Dir SW Veh CHP Veh Make Year SP Info OAF1 Viol OAF2 Safety Equip ROLE Ext Of Inj AGE Sex Seat Pos Safety EQUIP Ejected 1F DRVR 18 F W HNBD PROC ST S A 0100 - 2021 - 3 N - M G 2 DRVR 64 F W HNBD PROC ST E A 0100 - 2014 - 3 N - M G														
Primary Rd 6TH ST Distance (ft) 0.00 Direction Secondary Rd SEPULVEDA BL NCIC 1944 State Hwy? Y Route 1 Postmile Prefix - Postmile 22.571 Side of Hwy N City Manhattan Beach County Los Angeles Population 4 Rpt Dist M22 Beat 0M1 Type 0 CalTrans 7 Badge 389 Collision Date 20230517 Time 0826 Day WED Primary Collision Factor R-O-W AUTO Violation 21801A Collision Type BROADSIDE Severity PDO #Killed 0 #Injured 0 Tow Away? Y Process Date 20230630 Weather1 CLEAR Weather2 Rdwy Surface DRY Rdwy Cond1 NO UNUSL CND Rdwy Cond2 Spec Cond 0 Hit and Run Motor Vehicle Involved With OTHER MV Lighting DAYLIGHT Ped Action Cntrl Dev NT PRS/FCTR Loc Type I Ramp/Int 5														
Party Info Victim Info Party Type Age Sex Race Sobriety1 Sobriety2 Move Pre Dir SW Veh CHP Veh Make Year SP Info OAF1 Viol OAF2 Safety Equip ROLE Ext Of Inj AGE Sex Seat Pos Safety EQUIP Ejected 1F DRVR 55 M W HNBD LFT TURN E A 0700 - 2009 - 3 N - M G 2 DRVR 41 M A HNBD PROC ST N C 0200 - 2003 - 3 N - M -														
Primary Rd ARDMORE AV Distance (ft) 110. Direction S Secondary Rd 17TH ST NCIC 1944 State Hwy? N Route Postmile Prefix Postmile Side of Hwy City Manhattan Beach County Los Angeles Population 4 Rpt Dist M13 Beat 002 Type 0 CalTrans Badge 378 Collision Date 20230128 Time 1053 Day SAT Primary Collision Factor IMPROP TURN Violation 22107 Collision Type SIDESWIPE Severity INJURY #Killed 0 #Injured 1 Tow Away? Y Process Date 20230307 Weather1 CLEAR Weather2 Rdwy Surface DRY Rdwy Cond1 NO UNUSL CND Rdwy Cond2 Spec Cond 0 Hit and Run Motor Vehicle Involved With PKD MV Lighting DAYLIGHT Ped Action Cntrl Dev NT PRS/FCTR Loc Type Ramp/Int														
Party Info Victim Info Party Type Age Sex Race Sobriety1 Sobriety2 Move Pre Dir SW Veh CHP Veh Make Year SP Info OAF1 Viol OAF2 Safety Equip ROLE Ext Of Inj AGE Sex Seat Pos Safety EQUIP Ejected 1F DRVR 74 F W HNBD PROC ST N A 0700 - 2003 - 3 N - L G 2 PRKD 998 - PARKED N A 0700 - 2013 - 3 N - - - 3 PRKD 998 - PARKED N A 0100 - 2004 - 3 N - - -														

Primary Rd		ARTESIA BL		Distance (ft)	136.	Direction	W	Secondary Rd		AVIATION BL		NCIC	1944	State Hwy?	N	Route	Postmile Prefix	Postmile	Side of Hwy							
City		Manhattan Beach		County	Los Angeles	Population	4	Rpt Dist	M28	Beat	0M1	Type	0	CalTrans	Badge	294	Collision Date	20230227	Time	1700	Day	MON				
Primary Collision Factor		WRONG SIDE		Violation	21202A	Collision Type	BROADSIDE	Severity	INJURY	#Killed	0	#Injured	1	Tow Away?	N	Process Date	20230411									
Weather1		CLEAR		Weather2		Rdwy Surface	DRY	Rdwy Cond1	NO UNUSL CND	Rdwy Cond2		Spec Cond	0													
Hit and Run				Motor Vehicle Involved With		BICYCLE		Lighting	DAYLIGHT	Ped Action		Cntrl Dev	NT PRS/FCTR	Loc Type		Ramp/Int										
Party Info													Victim Info													
Party	Type	Age	Sex	Race	Sobriety1	Sobriety2	Move Pre	Dir	SW Veh	CHP Veh	Make	Year	SP Info	OAF1	Viol	OAF2	Safety Equip	ROLE	Ext Of Inj	AGE	Sex	Seat Pos	Safety	EQUIP	Ejected	
1F	BICY	14	M	H	HNBD		PROC ST	E	L	0400	-	-	3	A	30100	M	-	BICY	COMP PN	14	M	1	1	W	-	
2	DRVR	53	M	W	HNBD		OTHER	S	A	0700	-	2022	-	3	E	-	M	G	PASS		14	F	3	0	M	G
																		PASS		17	M	6	0	M	G	
Primary Rd		ARTESIA BL		Distance (ft)	0.00	Direction		Secondary Rd		REDOND AV		NCIC	1944	State Hwy?	N	Route	Postmile Prefix	Postmile	Side of Hwy							
City		Manhattan Beach		County	Los Angeles	Population	4	Rpt Dist	SOUTH	Beat	001	Type	0	CalTrans	Badge	329	Collision Date	20230701	Time	1245	Day	SAT				
Primary Collision Factor		R-O-W AUTO		Violation	21801A	Collision Type	BROADSIDE	Severity	INJURY	#Killed	0	#Injured	1	Tow Away?	Y	Process Date	20230913									
Weather1		CLEAR		Weather2		Rdwy Surface	DRY	Rdwy Cond1	NO UNUSL CND	Rdwy Cond2		Spec Cond	0													
Hit and Run				Motor Vehicle Involved With		OTHER MV		Lighting	DAYLIGHT	Ped Action		Cntrl Dev	NT PRS/FCTR	Loc Type		Ramp/Int										
Party Info													Victim Info													
Party	Type	Age	Sex	Race	Sobriety1	Sobriety2	Move Pre	Dir	SW Veh	CHP Veh	Make	Year	SP Info	OAF1	Viol	OAF2	Safety Equip	ROLE	Ext Of Inj	AGE	Sex	Seat Pos	Safety	EQUIP	Ejected	
1F	DRVR	56	F	H	HNBD		LFT TURN	N	A	0100	-	2014	-	3	N	-	L	G	DRVR	MINOR	56	F	1	0	L	G
2	DRVR	19	M	W	HNBD		PROC ST	W	A	0100	FORD	2005	-	3	N	-	L	G								
Primary Rd		AVIATION BL		Distance (ft)	0.00	Direction		Secondary Rd		19TH ST		NCIC	1944	State Hwy?	N	Route	Postmile Prefix	Postmile	Side of Hwy							
City		Manhattan Beach		County	Los Angeles	Population	4	Rpt Dist	M17	Beat	0M4	Type	0	CalTrans	Badge	294	Collision Date	20230501	Time	0939	Day	MON				
Primary Collision Factor		R-O-W AUTO		Violation	21801A	Collision Type	BROADSIDE	Severity	INJURY	#Killed	0	#Injured	1	Tow Away?	Y	Process Date	20230622									
Weather1		CLEAR		Weather2		Rdwy Surface	DRY	Rdwy Cond1	NO UNUSL CND	Rdwy Cond2		Spec Cond	0													
Hit and Run				Motor Vehicle Involved With		OTHER MV		Lighting	DAYLIGHT	Ped Action		Cntrl Dev	NT PRS/FCTR	Loc Type		Ramp/Int										
Party Info													Victim Info													
Party	Type	Age	Sex	Race	Sobriety1	Sobriety2	Move Pre	Dir	SW Veh	CHP Veh	Make	Year	SP Info	OAF1	Viol	OAF2	Safety Equip	ROLE	Ext Of Inj	AGE	Sex	Seat Pos	Safety	EQUIP	Ejected	
1F	DRVR	38	F	W	HNBD		LFT TURN	N	A	0700	-	2008	-	3	E	-	M	G								
2	DRVR	49	F	W	HNBD		PROC ST	S	A	0700	-	2022	-	3	E	-	L	G	DRVR	MINOR	49	F	1	0	L	G
Primary Rd		AVIATION BL		Distance (ft)	0.00	Direction		Secondary Rd		1ST ST		NCIC	1944	State Hwy?	N	Route	Postmile Prefix	Postmile	Side of Hwy							
City		Manhattan Beach		County	Los Angeles	Population	4	Rpt Dist	M28	Beat	0M1	Type	0	CalTrans	Badge	294	Collision Date	20230801	Time	1705	Day	TUE				
Primary Collision Factor		UNSAFE SPEED		Violation	22350	Collision Type	REAR END	Severity	INJURY	#Killed	0	#Injured	1	Tow Away?	N	Process Date	20230918									
Weather1		CLEAR		Weather2		Rdwy Surface	DRY	Rdwy Cond1	NO UNUSL CND	Rdwy Cond2		Spec Cond	0													
Hit and Run		FELONY		Motor Vehicle Involved With		OTHER MV		Lighting	DAYLIGHT	Ped Action		Cntrl Dev	FNCTNG	Loc Type		Ramp/Int										
Party Info													Victim Info													
Party	Type	Age	Sex	Race	Sobriety1	Sobriety2	Move Pre	Dir	SW Veh	CHP Veh	Make	Year	SP Info	OAF1	Viol	OAF2	Safety Equip	ROLE	Ext Of Inj	AGE	Sex	Seat Pos	Safety	EQUIP	Ejected	
1F	DRVR	998	M	B	IMP UNK	IMP UNK	PROC ST	S	A	0700	-	2014	-	-	N	-	-									
2	DRVR	31	F	H	HNBD		STOPPED	S	A	0100	-	2016	-	3	N	-	M	G	DRVR	POSSIBL	31	F	1	0	M	G
Primary Rd		AVIATION BL		Distance (ft)	12.0	Direction	N	Secondary Rd		2ND ST		NCIC	1944	State Hwy?	N	Route	Postmile Prefix	Postmile	Side of Hwy							
City		Manhattan Beach		County	Los Angeles	Population	4	Rpt Dist	M24	Beat	001	Type	0	CalTrans	Badge	329	Collision Date	20230329	Time	1130	Day	WED				
Primary Collision Factor		R-O-W PED		Violation	21950A	Collision Type	AUTO/PED	Severity	INJURY	#Killed	0	#Injured	1	Tow Away?		Process Date	20230428									
Weather1		CLEAR		Weather2		Rdwy Surface	DRY	Rdwy Cond1	NO UNUSL CND	Rdwy Cond2		Spec Cond	0													
Hit and Run				Motor Vehicle Involved With		PED		Lighting	DAYLIGHT	Ped Action	X-WLK AT	Cntrl Dev	FNCTNG	Loc Type		Ramp/Int										
Party Info													Victim Info													
Party	Type	Age	Sex	Race	Sobriety1	Sobriety2	Move Pre	Dir	SW Veh	CHP Veh	Make	Year	SP Info	OAF1	Viol	OAF2	Safety Equip	ROLE	Ext Of Inj	AGE	Sex	Seat Pos	Safety	EQUIP	Ejected	
1F	DRVR	62	F	W	HNBD		LFT TURN	N	A	0700	-	2022	A	-	N	-	M	G								
2	PED	20	M	W	HNBD		PROC ST	E	N	6000	-	-	-	-	N	-	-		PED	MINOR	20	M	-	0	-	P

Include State Highways cases

Report Run On: 10/04/2023

Primary Rd CHABELA DR Distance (ft) 24.0 Direction N Secondary Rd KEATS ST NCIC 1944 State Hwy? N Route Postmile Prefix Postmile Side of Hwy City Manhattan Beach County Los Angeles Population 4 Rpt Dist 1944 Beat Type 0 CalTrans Badge 239 Collision Date 20230821 Time 1447 Day MON Primary Collision Factor UNSAFE SPEED Violation 22350 Collision Type HIT OBJECT Severity INJURY #Killed 0 #Injured 1 Tow Away? Y Process Date 20231003 Weather1 CLEAR Weather2 Rdwy Surface DRY Rdwy Cond1 NO UNUSL CND Rdwy Cond2 Spec Cond 0 Hit and Run Motor Vehicle Involved With FIXED OBJ Lighting DAYLIGHT Ped Action Cntrl Dev FUNCTNG Loc Type Ramp/Int																											
Party Info														Victim Info													
Party	Type	Age	Sex	Race	Sobriety1	Sobriety2	Move Pre	Dir	SW Veh	CHP Veh	Make	Year	SP Info	OAF1	Viol	OAF2	Safety Equip	ROLE	Ext Of Inj	AGE	Sex	Seat Pos	Safety	EQUIP	Ejected		
1F	DRVR	31	M	H	IMP UNK	IMP UNK	LFT TURN	N	D	2200	-	2002	-	-	A	22107	N	L	B	DRVR	COMP PN	31	M	1	0	L	B
Primary Rd DIANTHUS ST Distance (ft) 0.00 Direction Secondary Rd 9TH ST NCIC 1944 State Hwy? N Route Postmile Prefix Postmile Side of Hwy City Manhattan Beach County Los Angeles Population 4 Rpt Dist N21 Beat 0M2 Type 0 CalTrans Badge 294 Collision Date 20230410 Time 1520 Day MON Primary Collision Factor R-O-W AUTO Violation 21800A Collision Type BROADSIDE Severity PDO #Killed 0 #Injured 0 Tow Away? Y Process Date 20230523 Weather1 CLEAR Weather2 Rdwy Surface DRY Rdwy Cond1 NO UNUSL CND Rdwy Cond2 Spec Cond 0 Hit and Run Motor Vehicle Involved With OTHER MV Lighting DAYLIGHT Ped Action Cntrl Dev FUNCTNG Loc Type Ramp/Int																											
Party Info														Victim Info													
Party	Type	Age	Sex	Race	Sobriety1	Sobriety2	Move Pre	Dir	SW Veh	CHP Veh	Make	Year	SP Info	OAF1	Viol	OAF2	Safety Equip	ROLE	Ext Of Inj	AGE	Sex	Seat Pos	Safety	EQUIP	Ejected		
1F	DRVR	19	M	H	HNBD		PROC ST	W	A	0100	-	2012	-	3	A	22450	-	L	G								
2	DRVR	54	M		HNBD		PROC ST	S	A	0100	JEEP	2021	-	3	N	-	M	G									
Primary Rd GRANDVIEW AV Distance (ft) 31.0 Direction N Secondary Rd 26TH PL NCIC 1944 State Hwy? N Route Postmile Prefix Postmile Side of Hwy City Manhattan Beach County Los Angeles Population 4 Rpt Dist M3 Beat 0M3 Type 0 CalTrans Badge 389 Collision Date 20230328 Time 2207 Day TUE Primary Collision Factor IMPROP TURN Violation 22107 Collision Type SIDESWIPE Severity PDO #Killed 0 #Injured 0 Tow Away? N Process Date 20230510 Weather1 CLEAR Weather2 Rdwy Surface DRY Rdwy Cond1 NO UNUSL CND Rdwy Cond2 Spec Cond 0 Hit and Run Motor Vehicle Involved With PKD MV Lighting DARK - ST Ped Action Cntrl Dev NT PRS/FCTR Loc Type Ramp/Int																											
Party Info														Victim Info													
Party	Type	Age	Sex	Race	Sobriety1	Sobriety2	Move Pre	Dir	SW Veh	CHP Veh	Make	Year	SP Info	OAF1	Viol	OAF2	Safety Equip	ROLE	Ext Of Inj	AGE	Sex	Seat Pos	Safety	EQUIP	Ejected		
1F	DRVR	54	M	A	HNBD		RGT TURN	S	F	2600	-	2018	-	3	A	22106	J	M	G								
2	PRKD	998	-				PARKED	N	A	0700	-	2022	-	3	N	-	-	-									
Primary Rd HERRIN ST Distance (ft) 6.00 Direction N Secondary Rd 2ND ST NCIC 1944 State Hwy? N Route Postmile Prefix Postmile Side of Hwy City Manhattan Beach County Los Angeles Population 4 Rpt Dist M23 Beat 0M1 Type 0 CalTrans Badge 373 Collision Date 20230526 Time 1259 Day FRI Primary Collision Factor UNSAFE SPEED Violation 22350 Collision Type SIDESWIPE Severity INJURY #Killed 0 #Injured 1 Tow Away? Y Process Date 20230802 Weather1 CLEAR Weather2 Rdwy Surface DRY Rdwy Cond1 NO UNUSL CND Rdwy Cond2 Spec Cond 0 Hit and Run Motor Vehicle Involved With PKD MV Lighting DAYLIGHT Ped Action Cntrl Dev FUNCTNG Loc Type Ramp/Int																											
Party Info														Victim Info													
Party	Type	Age	Sex	Race	Sobriety1	Sobriety2	Move Pre	Dir	SW Veh	CHP Veh	Make	Year	SP Info	OAF1	Viol	OAF2	Safety Equip	ROLE	Ext Of Inj	AGE	Sex	Seat Pos	Safety	EQUIP	Ejected		
1F	DRVR	76	F	W	HNBD		PROC ST	E	A	0700	-	2019	-	-	A	22107	-	L	B	DRVR	SERIOUS	76	F	1	0	L	B
2	PRKD	998	-					E	A	0700	-	2021	-	-	-	-	-	-									
3	PRKD	998	-					E	A	0700	-	2016	-	-	-	-	-	-									
4	DRVR	77	F	W				E	A	0700	-	2018	-	-	-	-	M	G									
Primary Rd HERRIN ST Distance (ft) 112. Direction S Secondary Rd NELSON AV NCIC 1944 State Hwy? N Route Postmile Prefix Postmile Side of Hwy City Manhattan Beach County Los Angeles Population 4 Rpt Dist M27 Beat 0M1 Type 0 CalTrans Badge 379 Collision Date 20230623 Time 2314 Day FRI Primary Collision Factor DRVR ALC DRG Violation 23152A Collision Type SIDESWIPE Severity PDO #Killed 0 #Injured 0 Tow Away? Y Process Date 20230728 Weather1 CLEAR Weather2 Rdwy Surface DRY Rdwy Cond1 NO UNUSL CND Rdwy Cond2 Spec Cond 0 Hit and Run Motor Vehicle Involved With PKD MV Lighting DARK - NO Ped Action Cntrl Dev NT PRS/FCTR Loc Type Ramp/Int																											
Party Info														Victim Info													
Party	Type	Age	Sex	Race	Sobriety1	Sobriety2	Move Pre	Dir	SW Veh	CHP Veh	Make	Year	SP Info	OAF1	Viol	OAF2	Safety Equip	ROLE	Ext Of Inj	AGE	Sex	Seat Pos	Safety	EQUIP	Ejected		
1F	DRVR	47	F	A	HBD-UI		PROC ST	S	A	0100	-	2013	-	3	M	-	M	C									
2	PRKD	998	-				PARKED	S	A	0100	FORD	2005	-	3	N	-	-	-									

Include State Highways cases

Report Run On: 10/04/2023

Primary Rd HIGHLAND AV Distance (ft) 130. Direction S Secondary Rd 18TH PL NCIC 1944 State Hwy? N Route Postmile Prefix Postmile Side of Hwy City Manhattan Beach County Los Angeles Population 4 Rpt Dist M12 Beat 003 Type 0 CalTrans Badge 382 Collision Date 20230610 Time 2314 Day SAT Primary Collision Factor DRVR ALC DRG Violation 23152A Collision Type OTHER Severity PDO #Killed 0 #Injured 0 Tow Away? Y Process Date 20230721 Weather1 CLEAR Weather2 Rdwy Surface DRY Rdwy Cond1 NO UNUSL CND Rdwy Cond2 Spec Cond 0 Hit and Run Motor Vehicle Involved With PKD MV Lighting DARK - ST Ped Action Cntrl Dev NT PRS/FCTR Loc Type Ramp/Int														
Party Info Party Type Age Sex Race Sobriety1 Sobriety2 Move Pre Dir SW Veh CHP Veh Make Year SP Info OAF1 Viol OAF2 Safety Equip ROLE Ext Of Inj AGE Sex Seat Pos Safety EQUIP Ejected 1F DRVR 57 F W HBD-UI PROC ST S A 0100 - 2018 - - N - - L G 2 PRKD 998 - S A 0100 - 2022 - - N - - -														
Primary Rd HIGHLAND AV Distance (ft) 0.00 Direction Secondary Rd 35TH ST NCIC 1944 State Hwy? N Route Postmile Prefix Postmile Side of Hwy City Manhattan Beach County Los Angeles Population 4 Rpt Dist M2 Beat 0M8 Type 0 CalTrans Badge 381 Collision Date 20230408 Time 1019 Day SAT Primary Collision Factor OTHER HAZ Violation 22109 Collision Type HIT OBJECT Severity INJURY #Killed 0 #Injured 1 Tow Away? N Process Date 20230613 Weather1 CLEAR Weather2 Rdwy Surface DRY Rdwy Cond1 NO UNUSL CND Rdwy Cond2 Spec Cond 0 Hit and Run Motor Vehicle Involved With FIXED OBJ Lighting DAYLIGHT Ped Action Cntrl Dev NT PRS/FCTR Loc Type Ramp/Int														
Party Info Party Type Age Sex Race Sobriety1 Sobriety2 Move Pre Dir SW Veh CHP Veh Make Year SP Info OAF1 Viol OAF2 Safety Equip ROLE Ext Of Inj AGE Sex Seat Pos Safety EQUIP Ejected 1F DRVR 48 M H HNBD DRUG SLOWING S C 0200 - 2007 - 3 N - - P W DRVR MINOR 48 M 1 1 P W														
Primary Rd HIGHLAND AV Distance (ft) 8.00 Direction S Secondary Rd 36TH PL NCIC 1944 State Hwy? N Route Postmile Prefix Postmile Side of Hwy City Manhattan Beach County Los Angeles Population 4 Rpt Dist M1 Beat 0M3 Type 0 CalTrans Badge 382 Collision Date 20230615 Time 2228 Day THU Primary Collision Factor DRVR ALC DRG Violation 23152A Collision Type HIT OBJECT Severity PDO #Killed 0 #Injured 0 Tow Away? Y Process Date 20230728 Weather1 CLEAR Weather2 Rdwy Surface DRY Rdwy Cond1 NO UNUSL CND Rdwy Cond2 Spec Cond 0 Hit and Run Motor Vehicle Involved With FIXED OBJ Lighting DARK - ST Ped Action Cntrl Dev FNCTNG Loc Type Ramp/Int														
Party Info Party Type Age Sex Race Sobriety1 Sobriety2 Move Pre Dir SW Veh CHP Veh Make Year SP Info OAF1 Viol OAF2 Safety Equip ROLE Ext Of Inj AGE Sex Seat Pos Safety EQUIP Ejected 1F DRVR 28 F B HBD-UI PROC ST S A 0700 - 2019 - 3 A 22350 N M G														
Primary Rd HIGHLAND AV Distance (ft) 24.0 Direction S Secondary Rd 38TH ST NCIC 1944 State Hwy? N Route Postmile Prefix Postmile Side of Hwy City Manhattan Beach County Los Angeles Population 4 Rpt Dist M1 Beat 0M3 Type 0 CalTrans Badge 383 Collision Date 20230218 Time 0049 Day SAT Primary Collision Factor UNSAFE SPEED Violation 22350 Collision Type HIT OBJECT Severity PDO #Killed 0 #Injured 0 Tow Away? Y Process Date 20230425 Weather1 CLEAR Weather2 Rdwy Surface DRY Rdwy Cond1 NO UNUSL CND Rdwy Cond2 Spec Cond 0 Hit and Run Motor Vehicle Involved With FIXED OBJ Lighting DARK - ST Ped Action Cntrl Dev NT PRS/FCTR Loc Type Ramp/Int														
Party Info Party Type Age Sex Race Sobriety1 Sobriety2 Move Pre Dir SW Veh CHP Veh Make Year SP Info OAF1 Viol OAF2 Safety Equip ROLE Ext Of Inj AGE Sex Seat Pos Safety EQUIP Ejected 1F DRVR 18 M H HNBD PROC ST S A 0100 - 2003 - 1 A 23123 F L C														
Primary Rd HIGHLAND AV Distance (ft) 0.00 Direction Secondary Rd 49TH ST NCIC 1944 State Hwy? N Route Postmile Prefix Postmile Side of Hwy City Manhattan Beach County Los Angeles Population 4 Rpt Dist M1 Beat 0M3 Type 0 CalTrans Badge 294 Collision Date 20230130 Time 0925 Day MON Primary Collision Factor UNSAFE SPEED Violation 22350 Collision Type REAR END Severity PDO #Killed 0 #Injured 0 Tow Away? Y Process Date 20230307 Weather1 CLOUDY Weather2 Rdwy Surface DRY Rdwy Cond1 NO UNUSL CND Rdwy Cond2 Spec Cond 0 Hit and Run MSDMNR Motor Vehicle Involved With OTHER MV Lighting DAYLIGHT Ped Action Cntrl Dev FNCTNG Loc Type Ramp/Int														
Party Info Party Type Age Sex Race Sobriety1 Sobriety2 Move Pre Dir SW Veh CHP Veh Make Year SP Info OAF1 Viol OAF2 Safety Equip ROLE Ext Of Inj AGE Sex Seat Pos Safety EQUIP Ejected 1F DRVR 53 M H HNBD PROC ST N I 1100 - 2006 - - N - - M B 2 DRVR 84 M W HNBD STOPPED N A 0800 - 2007 - 3 N - - M G PASS 40 F 3 0 M B PASS 80 F 3 0 M G														

Primary Rd		HIGHLAND AV		Distance (ft)	0.00	Direction		Secondary Rd	EL PORTO ST		NCIC	1944	State Hwy?	N	Route		Postmile Prefix		Postmile		Side of Hwy					
City		Manhattan Beach		County	Los Angeles	Population	4	Rpt Dist	M1	Beat	0M3	Type	0	CalTrans	Badge	329	Collision Date	20230614	Time	1708	Day	WED				
Primary Collision Factor		WRONG SIDE		Violation	21460A	Collision Type	SIDESWIPE	Severity	INJURY	#Killed	0	#Injured	3	Tow Away?	N	Process Date	20230912									
Weather1		CLEAR		Weather2		Rdwy Surface	DRY	Rdwy Cond1	NO UNUSL CND	Rdwy Cond2		Spec Cond	0													
Hit and Run				Motor Vehicle Involved With	OTHER MV		Lighting	DAYLIGHT	Ped Action		Cntrl Dev	NT PRS/FCTR	Loc Type		Ramp/Int											
Party Info															Victim Info											
Party	Type	Age	Sex	Race	Sobriety1	Sobriety2	Move Pre	Dir	SW Veh	CHP Veh	Make	Year	SP Info	OAF1	Viol	OAF2	Safety Equip	ROLE	Ext Of Inj	AGE	Sex	Seat Pos	Safety	EQUIP	Ejected	
1F	DRVR	26	M	B	HNBD		WRONG WY	N	A	0100	-	2021	-	M	-	L	G	DRVR	POSSIBL	26	M	1	0	L	G	
																		PASS	POSSIBL	25	F	3	0	L	G	
2	DRVR	44	M	W	HNBD		LFT TURN	S	A	0100	-	2008	-	3	M	-	M	G								
3	DRVR	49	M	W	HNBD		STOPPED	N	A	0700	-	2022	-	3	G	-	M	G	PASS		10	F	3	0	M	G
4	DRVR	57	F	W	HNBD		STOPPED	N	A	0700	-	2012	-	3	G	-	M	G	DRVR	MINOR	57	F	1	0	M	G
																		PASS		21	F	3	0	M	G	
Primary Rd		HIGHLAND AV		Distance (ft)	28.0	Direction	S	Secondary Rd	SEAVIEW ST		NCIC	1944	State Hwy?	N	Route		Postmile Prefix		Postmile		Side of Hwy					
City		Manhattan Beach		County	Los Angeles	Population	4	Rpt Dist	M1	Beat	0M3	Type	0	CalTrans	Badge	361	Collision Date	20230503	Time	2317	Day	WED				
Primary Collision Factor		IMPROP TURN		Violation	22107	Collision Type	REAR END	Severity	PDO	#Killed	0	#Injured	0	Tow Away?	Y	Process Date	20230608									
Weather1		CLEAR		Weather2		Rdwy Surface	DRY	Rdwy Cond1	NO UNUSL CND	Rdwy Cond2		Spec Cond	0													
Hit and Run		MSDMNR		Motor Vehicle Involved With	PKD MV		Lighting	DARK - ST	Ped Action		Cntrl Dev	NT PRS/FCTR	Loc Type		Ramp/Int											
Party Info															Victim Info											
Party	Type	Age	Sex	Race	Sobriety1	Sobriety2	Move Pre	Dir	SW Veh	CHP Veh	Make	Year	SP Info	OAF1	Viol	OAF2	Safety Equip	ROLE	Ext Of Inj	AGE	Sex	Seat Pos	Safety	EQUIP	Ejected	
1F	DRVR	998	M		IMP UNK	IMP UNK	PROC ST	S	A	0100	-	-	-	N	-	-	-									
2	PRKD	998	-				PARKED	S	A	0100	-	2017	-	3	N	-	-									
3	PRKD	998	-				PARKED	S	A	0100	AUDI	2009	-	3	N	-	-									
4	PRKD	998	-				PARKED	S	A	0100	-	2020	-	3	N	-	-									
Primary Rd		INGLESIDE DR		Distance (ft)	37.0	Direction	N	Secondary Rd	4TH PL		NCIC	1944	State Hwy?	N	Route		Postmile Prefix		Postmile		Side of Hwy					
City		Manhattan Beach		County	Los Angeles	Population	4	Rpt Dist	M19	Beat	0M2	Type	0	CalTrans	Badge	389	Collision Date	20230211	Time	1818	Day	SAT				
Primary Collision Factor		DRVR ALC DRG		Violation	23152A	Collision Type	HIT OBJECT	Severity	PDO	#Killed	0	#Injured	0	Tow Away?	Y	Process Date	20230412									
Weather1		CLEAR		Weather2		Rdwy Surface	DRY	Rdwy Cond1	NO UNUSL CND	Rdwy Cond2		Spec Cond	0													
Hit and Run		MSDMNR		Motor Vehicle Involved With	FIXED OBJ		Lighting	DARK - ST	Ped Action		Cntrl Dev	NT PRS/FCTR	Loc Type		Ramp/Int											
Party Info															Victim Info											
Party	Type	Age	Sex	Race	Sobriety1	Sobriety2	Move Pre	Dir	SW Veh	CHP Veh	Make	Year	SP Info	OAF1	Viol	OAF2	Safety Equip	ROLE	Ext Of Inj	AGE	Sex	Seat Pos	Safety	EQUIP	Ejected	
1F	DRVR	35	M	H	HBD-UI		PROC ST	N	A	0700	-	2015	-	A	22107	-	M	B								
Primary Rd		MANHATTAN AV		Distance (ft)	55.0	Direction	S	Secondary Rd	36TH ST		NCIC	1944	State Hwy?	N	Route		Postmile Prefix		Postmile		Side of Hwy					
City		Manhattan Beach		County	Los Angeles	Population	4	Rpt Dist	M2	Beat	0M8	Type	0	CalTrans	Badge	294	Collision Date	20230628	Time	1008	Day	WED				
Primary Collision Factor		IMPROP TURN		Violation	22107	Collision Type	SIDESWIPE	Severity	PDO	#Killed	0	#Injured	0	Tow Away?		Process Date	20230727									
Weather1		CLEAR		Weather2		Rdwy Surface	DRY	Rdwy Cond1	NO UNUSL CND	Rdwy Cond2		Spec Cond	0													
Hit and Run				Motor Vehicle Involved With	PKD MV		Lighting	DAYLIGHT	Ped Action		Cntrl Dev	NT PRS/FCTR	Loc Type		Ramp/Int											
Party Info															Victim Info											
Party	Type	Age	Sex	Race	Sobriety1	Sobriety2	Move Pre	Dir	SW Veh	CHP Veh	Make	Year	SP Info	OAF1	Viol	OAF2	Safety Equip	ROLE	Ext Of Inj	AGE	Sex	Seat Pos	Safety	EQUIP	Ejected	
1F	DRVR	27	F	W	HNBD		PROC ST	S	D	7200	-	2023	-	3	N	-	M	G								
2	PRKD	998	-				PARKED	S	A	0800	FORD	2003	-	3	A	22500	-	-								

Primary Rd		MANHATTAN AV		Distance (ft)	0.00	Direction		Secondary Rd	6TH ST		NCIC	1944	State Hwy?	N	Route		Postmile Prefix		Postmile		Side of Hwy					
City		Manhattan Beach		County	Los Angeles	Population	4	Rpt Dist	M18	Beat	0M8	Type	0	CalTrans	Badge	380	Collision Date	20230715	Time	0723	Day	SAT				
Primary Collision Factor		NOT DRIVER		Violation		Collision Type	AUTO/PED		Severity	INJURY		#Killed	0	#Injured	1	Tow Away?	N	Process Date	20230906							
Weather1		CLOUDY		Weather2		Rdwy Surface	DRY		Rdwy Cond1	NO UNUSL CND		Rdwy Cond2		Spec Cond	0											
Hit and Run				Motor Vehicle Involved With		PED		Lighting	DAYLIGHT		Ped Action	IN RD,		Cntrl Dev	NT PRS/FCTR		Loc Type		Ramp/Int							
																		Victim Info								
Party	Type	Age	Sex	Race	Sobriety1	Sobriety2	Move Pre	Dir	SW Veh	CHP Veh	Make	Year	SP Info	OAF1	Viol	OAF2	Safety Equip	ROLE	Ext Of Inj	AGE	Sex	Seat Pos	Safety	EQUIP	Ejected	
1	PED	41	M	W	HNBD		OTHER	W	N	6000	-	-	3	F	-	-	-	PED	SEVERE	1	F	-	-	-	-	
2	DRVR	70	M	W	HNBD		PROC ST	S	-	0035	-	2006	-	3	N	-	M G									
Primary Rd		MANHATTAN AV		Distance (ft)	0.00	Direction		Secondary Rd	9TH ST		NCIC	1944	State Hwy?	N	Route		Postmile Prefix		Postmile		Side of Hwy					
City		Manhattan Beach		County	Los Angeles	Population	4	Rpt Dist	M18	Beat	0M8	Type	0	CalTrans	Badge	386	Collision Date	20230806	Time	1813	Day	SUN				
Primary Collision Factor		STOP SGN SIG		Violation	22450A	Collision Type	SIDESWIPE		Severity	PDO		#Killed	0	#Injured	0	Tow Away?	N	Process Date	20230908							
Weather1		CLEAR		Weather2		Rdwy Surface	DRY		Rdwy Cond1	NO UNUSL CND		Rdwy Cond2		Spec Cond	0											
Hit and Run		MSDMNR		Motor Vehicle Involved With		OTHER MV		Lighting	DAYLIGHT		Ped Action		Cntrl Dev	FNCTNG		Loc Type		Ramp/Int								
																		Victim Info								
Party	Type	Age	Sex	Race	Sobriety1	Sobriety2	Move Pre	Dir	SW Veh	CHP Veh	Make	Year	SP Info	OAF1	Viol	OAF2	Safety Equip	ROLE	Ext Of Inj	AGE	Sex	Seat Pos	Safety	EQUIP	Ejected	
1F	DRVR	998	M	W	IMP UNK	IMP UNK	PROC ST	N	A	0700	-	2023	-	-	N	-	-			7	M	6	0	M	Q	
2	DRVR	47	F	W	HNBD		LFT TURN	E	A	0100	-	2018	-	3	N	-	M G	PASS		59	M	3	0	M	G	
Primary Rd		MANHATTAN		Distance (ft)	29.0	Direction	W	Secondary Rd	ARDMORE AV		NCIC	1944	State Hwy?	N	Route		Postmile Prefix		Postmile		Side of Hwy					
City		Manhattan Beach		County	Los Angeles	Population	4	Rpt Dist	M13	Beat	0M2	Type	0	CalTrans	Badge	382	Collision Date	20230617	Time	2318	Day	SAT				
Primary Collision Factor		STOP SGN SIG		Violation	21453A	Collision Type	BROADSIDE		Severity	INJURY		#Killed	0	#Injured	1	Tow Away?	Y	Process Date	20230803							
Weather1		CLEAR		Weather2		Rdwy Surface	DRY		Rdwy Cond1	NO UNUSL CND		Rdwy Cond2		Spec Cond	0											
Hit and Run				Motor Vehicle Involved With		OTHER MV		Lighting	DARK - ST		Ped Action		Cntrl Dev	FNCTNG		Loc Type		Ramp/Int								
																		Victim Info								
Party	Type	Age	Sex	Race	Sobriety1	Sobriety2	Move Pre	Dir	SW Veh	CHP Veh	Make	Year	SP Info	OAF1	Viol	OAF2	Safety Equip	ROLE	Ext Of Inj	AGE	Sex	Seat Pos	Safety	EQUIP	Ejected	
1F	DRVR	20	F	A	HNBD		PROC ST	E	A	0100	-	2017	-	3	A	22107	-	L G	DRVR	MINOR	20	F	1	0	L	G
2	DRVR	35	M	W	HNBD		PROC ST	N	A	0700	-	2022	-	3	N	-	M G									
Primary Rd		MANHATTAN		Distance (ft)	0.00	Direction		Secondary Rd	AVIATION BL		NCIC	1944	State Hwy?	N	Route		Postmile Prefix		Postmile		Side of Hwy					
City		Manhattan Beach		County	Los Angeles	Population	4	Rpt Dist	M17	Beat	004	Type	0	CalTrans	Badge	353	Collision Date	20230126	Time	0626	Day	THU				
Primary Collision Factor		STOP SGN SIG		Violation	21453A	Collision Type	BROADSIDE		Severity	INJURY		#Killed	0	#Injured	1	Tow Away?	Y	Process Date	20230306							
Weather1		CLEAR		Weather2		Rdwy Surface	DRY		Rdwy Cond1	NO UNUSL CND		Rdwy Cond2		Spec Cond	0											
Hit and Run				Motor Vehicle Involved With		OTHER MV		Lighting	DARK - ST		Ped Action		Cntrl Dev	FNCTNG		Loc Type		Ramp/Int								
																		Victim Info								
Party	Type	Age	Sex	Race	Sobriety1	Sobriety2	Move Pre	Dir	SW Veh	CHP Veh	Make	Year	SP Info	OAF1	Viol	OAF2	Safety Equip	ROLE	Ext Of Inj	AGE	Sex	Seat Pos	Safety	EQUIP	Ejected	
1F	DRVR	69	F	H	HNBD		PROC ST	S	A	0100	-	2011	-	3	N	-	L G	DRVR	COMP PN	69	F	1	0	L	G	
2	DRVR	64	M	O	HNBD		PROC ST	W	A	0100	-	2020	-	3	N	-	L G									
Primary Rd		MANHATTAN		Distance (ft)	35.0	Direction	W	Secondary Rd	AVIATION BL		NCIC	1944	State Hwy?	N	Route		Postmile Prefix		Postmile		Side of Hwy					
City		Manhattan Beach		County	Los Angeles	Population	4	Rpt Dist	M24	Beat	001	Type	0	CalTrans	Badge	391	Collision Date	20230824	Time	2356	Day	THU				
Primary Collision Factor		DRVR ALC DRG		Violation	23152A	Collision Type	REAR END		Severity	INJURY		#Killed	0	#Injured	1	Tow Away?	Y	Process Date	20230918							
Weather1		CLEAR		Weather2		Rdwy Surface	DRY		Rdwy Cond1	NO UNUSL CND		Rdwy Cond2		Spec Cond	0											
Hit and Run				Motor Vehicle Involved With		OTHER MV		Lighting	DARK - ST		Ped Action		Cntrl Dev	FNCTNG		Loc Type		Ramp/Int								
																		Victim Info								
Party	Type	Age	Sex	Race	Sobriety1	Sobriety2	Move Pre	Dir	SW Veh	CHP Veh	Make	Year	SP Info	OAF1	Viol	OAF2	Safety Equip	ROLE	Ext Of Inj	AGE	Sex	Seat Pos	Safety	EQUIP	Ejected	
1F	DRVR	28	F	B	HBD-UI		PROC ST	E	A	0100	-	2019	-	3	A	22350	-	L B	DRVR	COMP PN	32	F	1	0	L	G
2	DRVR	32	F	H	HNBD		STOPPED	E	A	0700	-	2003	-	3	N	-	L G	DRVR	COMP PN	38	M	3	0	L	G	

Primary Rd MANHATTAN Distance (ft) 122. Direction W Secondary Rd DIANTHUS ST NCIC 1944 State Hwy? N Route Postmile Prefix Postmile Side of Hwy City Manhattan Beach County Los Angeles Population 4 Rpt Dist M21 Beat 0M2 Type 0 CalTrans Badge 294 Collision Date 20230201 Time 1705 Day WED Primary Collision Factor OTHER HAZ Violation 21235G Collision Type OTHER Severity INJURY #Killed 0 #Injured 1 Tow Away? Process Date 20230317 Weather1 CLEAR Weather2 Rdwy Surface DRY Rdwy Cond1 NO UNUSL CND Rdwy Cond2 Spec Cond 0 Hit and Run Motor Vehicle Involved With NON-CLSN Lighting DAYLIGHT Ped Action Cntrl Dev NT PRS/FCTR Loc Type Ramp/Int														
Party Info Victim Info Party Type Age Sex Race Sobriety1 Sobriety2 Move Pre Dir SW Veh CHP Veh Make Year SP Info OAF1 Viol OAF2 Safety Equip ROLE Ext Of Inj AGE Sex Seat Pos Safety EQUIP Ejected 1F OTHR 998 - HNBD PROC ST E M 9400 - - 3 N - - - OTHR COMP PN 23 F - - V - 2 DRVR 64 F W HNBD SLOWING N A 0100 - 2022 - 3 N - M G														
Primary Rd MANHATTAN Distance (ft) 150. Direction E Secondary Rd HARKNESS ST NCIC 1944 State Hwy? N Route Postmile Prefix Postmile Side of Hwy City Manhattan Beach County Los Angeles Population 4 Rpt Dist SOUTH Beat 001 Type 0 CalTrans Badge 316 Collision Date 20230404 Time 0858 Day TUE Primary Collision Factor UNKNOWN Violation Collision Type REAR END Severity INJURY #Killed 0 #Injured 1 Tow Away? N Process Date 20230517 Weather1 CLEAR Weather2 Rdwy Surface DRY Rdwy Cond1 NO UNUSL CND Rdwy Cond2 Spec Cond 0 Hit and Run Motor Vehicle Involved With BICYCLE Lighting DAYLIGHT Ped Action Cntrl Dev NT PRS/FCTR Loc Type Ramp/Int														
Party Info Victim Info Party Type Age Sex Race Sobriety1 Sobriety2 Move Pre Dir SW Veh CHP Veh Make Year SP Info OAF1 Viol OAF2 Safety Equip ROLE Ext Of Inj AGE Sex Seat Pos Safety EQUIP Ejected 1 DRVR 61 M H HNBD RGT TURN E A 0700 - 2008 - 3 N - M G 2 BICY 18 M W HNBD PROC ST E L 0400 - - 3 N - - - BICY MINOR 18 M 1 2 - W														
Primary Rd MANHATTAN Distance (ft) 84.0 Direction E Secondary Rd JOHN ST NCIC 1944 State Hwy? N Route Postmile Prefix Postmile Side of Hwy City Manhattan Beach County Los Angeles Population 4 Rpt Dist M14 Beat 0M2 Type 0 CalTrans Badge 386 Collision Date 20230812 Time 0106 Day SAT Primary Collision Factor UNSAFE SPEED Violation 22350 Collision Type REAR END Severity PDO #Killed 0 #Injured 0 Tow Away? Y Process Date 20230913 Weather1 CLEAR Weather2 Rdwy Surface DRY Rdwy Cond1 NO UNUSL CND Rdwy Cond2 Spec Cond 0 Hit and Run Motor Vehicle Involved With PKD MV Lighting DARK - ST Ped Action Cntrl Dev NT PRS/FCTR Loc Type Ramp/Int														
Party Info Victim Info Party Type Age Sex Race Sobriety1 Sobriety2 Move Pre Dir SW Veh CHP Veh Make Year SP Info OAF1 Viol OAF2 Safety Equip ROLE Ext Of Inj AGE Sex Seat Pos Safety EQUIP Ejected 1F DRVR 24 M B HBD-NUI PROC ST E A 0100 - 2018 - 3 N - L G PASS 24 M 3 0 L F 2 PRKD 998 - PARKED E A 0700 - 2022 - - N - - - 3 PRKD 998 - PARKED E A 0100 - 2015 - - N - - -														
Primary Rd MANHATTAN Distance (ft) 47.0 Direction W Secondary Rd MEADOWS AV NCIC 1944 State Hwy? N Route Postmile Prefix Postmile Side of Hwy City Manhattan Beach County Los Angeles Population 4 Rpt Dist M15 Beat 004 Type 0 CalTrans Badge 294 Collision Date 20230102 Time 1513 Day MON Primary Collision Factor IMPROP TURN Violation 22107 Collision Type HIT OBJECT Severity PDO #Killed 0 #Injured 0 Tow Away? N Process Date 20230224 Weather1 CLOUDY Weather2 Rdwy Surface DRY Rdwy Cond1 NO UNUSL CND Rdwy Cond2 Spec Cond 0 Hit and Run MSDMNR Motor Vehicle Involved With FIXED OBJ Lighting DAYLIGHT Ped Action Cntrl Dev FNCTNG Loc Type Ramp/Int														
Party Info Victim Info Party Type Age Sex Race Sobriety1 Sobriety2 Move Pre Dir SW Veh CHP Veh Make Year SP Info OAF1 Viol OAF2 Safety Equip ROLE Ext Of Inj AGE Sex Seat Pos Safety EQUIP Ejected 1F DRVR 998 - HBD-UNK RGT TURN S A 0100 - - - N - - -														
Primary Rd MANHATTAN Distance (ft) 0.00 Direction Secondary Rd MORNINGSIDE DR NCIC 1944 State Hwy? N Route Postmile Prefix Postmile Side of Hwy City Manhattan Beach County Los Angeles Population 4 Rpt Dist M19 Beat 002 Type 0 CalTrans Badge 294 Collision Date 20230126 Time 1528 Day THU Primary Collision Factor IMPROP TURN Violation 22107 Collision Type SIDESWIPE Severity PDO #Killed 0 #Injured 0 Tow Away? Y Process Date 20230308 Weather1 CLEAR Weather2 Rdwy Surface DRY Rdwy Cond1 NO UNUSL CND Rdwy Cond2 Spec Cond 0 Hit and Run MSDMNR Motor Vehicle Involved With OTHER MV Lighting DAYLIGHT Ped Action Cntrl Dev FNCTNG Loc Type Ramp/Int														
Party Info Victim Info Party Type Age Sex Race Sobriety1 Sobriety2 Move Pre Dir SW Veh CHP Veh Make Year SP Info OAF1 Viol OAF2 Safety Equip ROLE Ext Of Inj AGE Sex Seat Pos Safety EQUIP Ejected 1F DRVR 32 M B IMP UNK IMP UNK LFT TURN W A 0100 - 2006 - - A 22350 - M G 2 DRVR 69 M O HNBD PROC ST E A 0100 JEEP 2020 - 3 N - M G														

Primary Rd	MANHATTAN	Distance (ft)	29.0	Direction	N	Secondary Rd	MORNINGSIDE DR	NCIC	1944	State Hwy?	N	Route		Postmile Prefix		Postmile		Side of Hwy							
City	Manhattan Beach	County	Los Angeles	Population	4	Rpt Dist	M12	Beat	0M3	Type	0	CalTrans	Badge	386	Collision Date	20230512	Time	1352	Day	FRI					
Primary Collision Factor	STOP SGN SIG	Violation	22450A	Collision Type	BROADSIDE	Severity	INJURY	#Killed	0	#Injured	1	Tow Away?	N	Process Date	20230626										
Weather1	CLEAR	Weather2		Rdwy Surface	DRY	Rdwy Cond1	NO UNUSL CND	Rdwy Cond2		Spec Cond	0														
Hit and Run	FELONY	Motor Vehicle Involved With	OTHER MV	Lighting	DARK - ST	Ped Action		Cntrl Dev		FUNCTNG		Loc Type		Ramp/Int											
Party Info														Victim Info											
Party	Type	Age	Sex	Race	Sobriety1	Sobriety2	Move Pre	Dir	SW Veh	CHP Veh	Make	Year	SP Info	OAF1	Viol	OAF2	Safety Equip	ROLE	Ext Of Inj	AGE	Sex	Seat Pos	Safety	EQUIP	Ejected
1F	DRVR	998	F	W	IMP UNK	IMP UNK	PROC ST	N	A	0700	-	2011	-	-	N	-	-								
2	DRVR	54	F	O	HNBD		PROC ST	E	A	0100	-	2020	-	3	N	-	M G	DRVR	MINOR	54	F	1	0	M	G
																		PASS		998	M	6	0	M	B

Primary Rd	MANHATTAN	Distance (ft)	100.	Direction	E	Secondary Rd	OCEAN DR	NCIC	1944	State Hwy?	N	Route		Postmile Prefix		Postmile		Side of Hwy							
City	Manhattan Beach	County	Los Angeles	Population	4	Rpt Dist		Beat		Type	0	CalTrans	Badge	329	Collision Date	20230810	Time	1107	Day	THU					
Primary Collision Factor	OTHER IMPROP DRV	Violation		Collision Type	AUTO/PED	Severity	INJURY	#Killed	0	#Injured	1	Tow Away?	N	Process Date	20230926										
Weather1	CLEAR	Weather2		Rdwy Surface	DRY	Rdwy Cond1	NO UNUSL CND	Rdwy Cond2		Spec Cond	0														
Hit and Run		Motor Vehicle Involved With		Lighting	DAYLIGHT	Ped Action	NOT IN RD	Cntrl Dev		NT PRS/FCTR		Loc Type		Ramp/Int											
Party Info														Victim Info											
Party	Type	Age	Sex	Race	Sobriety1	Sobriety2	Move Pre	Dir	SW Veh	CHP Veh	Make	Year	SP Info	OAF1	Viol	OAF2	Safety Equip	ROLE	Ext Of Inj	AGE	Sex	Seat Pos	Safety	EQUIP	Ejected
1F	DRVR	62	M	W	HNBD		STOPPED	N	A	0100	-	2021	-	3	O	-	M H								
2	PED	37	F	W	HNBD		PROC ST	E	N	6000	-	-	-	-	N	-	-	PED	OTH VIS	37	F	-	-	-	P

Primary Rd	MANHATTAN	Distance (ft)	78.0	Direction	E	Secondary Rd	PACIFIC AV	NCIC	1944	State Hwy?	N	Route		Postmile Prefix		Postmile		Side of Hwy							
City	Manhattan Beach	County	Los Angeles	Population	4	Rpt Dist	M14	Beat	0M2	Type	0	CalTrans	Badge	382	Collision Date	20230218	Time	0010	Day	SAT					
Primary Collision Factor	UNSAFE SPEED	Violation	22350	Collision Type	HIT OBJECT	Severity	PDO	#Killed	0	#Injured	0	Tow Away?	Y	Process Date	20230412										
Weather1	CLEAR	Weather2		Rdwy Surface	DRY	Rdwy Cond1	NO UNUSL CND	Rdwy Cond2		Spec Cond	0														
Hit and Run		Motor Vehicle Involved With	FIXED OBJ	Lighting	DARK - ST	Ped Action		Cntrl Dev		FUNCTNG		Loc Type		Ramp/Int											
Party Info														Victim Info											
Party	Type	Age	Sex	Race	Sobriety1	Sobriety2	Move Pre	Dir	SW Veh	CHP Veh	Make	Year	SP Info	OAF1	Viol	OAF2	Safety Equip	ROLE	Ext Of Inj	AGE	Sex	Seat Pos	Safety	EQUIP	Ejected
1F	DRVR	30	M	H	IMP UNK	IMP UNK	RAN OFF RD	E	A	0700	-	2005	-	3	A	22107	-	L G							

Primary Rd	MANHATTAN	Distance (ft)	131.	Direction	W	Secondary Rd	PACIFIC AV	NCIC	1944	State Hwy?	N	Route		Postmile Prefix		Postmile		Side of Hwy								
City	Manhattan Beach	County	Los Angeles	Population	4	Rpt Dist	M14	Beat	0M2	Type	0	CalTrans	Badge	386	Collision Date	20230327	Time	0046	Day	MON						
Primary Collision Factor	DRVR ALC DRG	Violation	23152A	Collision Type	SIDESWIPE	Severity	PDO	#Killed	0	#Injured	0	Tow Away?	Y	Process Date	20230503											
Weather1	CLEAR	Weather2		Rdwy Surface	DRY	Rdwy Cond1	NO UNUSL CND	Rdwy Cond2		Spec Cond	0															
Hit and Run		Motor Vehicle Involved With	PKD MV	Lighting	DARK - ST	Ped Action		Cntrl Dev		FUNCTNG		Loc Type		Ramp/Int												
Party Info														Victim Info												
Party	Type	Age	Sex	Race	Sobriety1	Sobriety2	Move Pre	Dir	SW Veh	CHP Veh	Make	Year	SP Info	OAF1	Viol	OAF2	Safety Equip	ROLE	Ext Of Inj	AGE	Sex	Seat Pos	Safety	EQUIP	Ejected	
1F	DRVR	21	F	H	HBD-UI		PROC ST	E	A	0700	-	2021	-	3	A	22107	-	L G	PASS		29	M	3	0	L	G
2	PRKD	998	-				PARKED	E	A	0100	-	2020	-	-	N	-	-									
3	PRKD	998	-				PARKED	E	A	0100	-	2009	-	-	N	-	-									

Primary Rd	MANHATTAN	Distance (ft)	105.	Direction	W	Secondary Rd	POINSETTIA AV	NCIC	1944	State Hwy?	N	Route		Postmile Prefix		Postmile		Side of Hwy								
City	Manhattan Beach	County	Los Angeles	Population	4	Rpt Dist	M21	Beat	0M2	Type	0	CalTrans	Badge	294	Collision Date	20230420	Time	0700	Day	THU						
Primary Collision Factor	IMPROP TURN	Violation	22107	Collision Type	AUTO/PED	Severity	INJURY	#Killed	0	#Injured	1	Tow Away?	N	Process Date	20230608											
Weather1	CLEAR	Weather2		Rdwy Surface	DRY	Rdwy Cond1	NO UNUSL CND	Rdwy Cond2		Spec Cond	0															
Hit and Run	FELONY	Motor Vehicle Involved With	PED	Lighting	DAYLIGHT	Ped Action	IN RD,	Cntrl Dev		NT PRS/FCTR		Loc Type		Ramp/Int												
Party Info														Victim Info												
Party	Type	Age	Sex	Race	Sobriety1	Sobriety2	Move Pre	Dir	SW Veh	CHP Veh	Make	Year	SP Info	OAF1	Viol	OAF2	Safety Equip	ROLE	Ext Of Inj	AGE	Sex	Seat Pos	Safety	EQUIP	Ejected	
1F	DRVR	57	M	W	HNBD		PROC ST	W	A	0700	-	2020	-	3	A	22350	-	M B								
2	PED	58	M	W	HNBD		PROC ST	E	N	6000	-	-	-	3	A	21954	-	-	PED	MINOR	58	M	-	-	-	P

Include State Highways cases

Report Run On: 10/04/2023

Primary Rd MANHATTAN Distance (ft) 192. Direction W Secondary Rd REDONDO AV NCIC 1944 State Hwy? N Route Postmile Prefix Postmile Side of Hwy City Manhattan Beach County Los Angeles Population 4 Rpt Dist M23 Beat 0M1 Type 0 CalTrans Badge 384 Collision Date 20230804 Time 1155 Day FRI Primary Collision Factor IMPROP TURN Violation 22107 Collision Type SIDESWIPE Severity PDO #Killed 0 #Injured 0 Tow Away? N Process Date 20230913 Weather1 CLEAR Weather2 Rdwy Surface DRY Rdwy Cond1 NO UNUSL CND Rdwy Cond2 Spec Cond 0 Hit and Run MSDMNR Motor Vehicle Involved With PKD MV Lighting DAYLIGHT Ped Action Cntrl Dev FNCTNG Loc Type Ramp/Int														Party Info Party Type Age Sex Race Sobriety1 Sobriety2 Move Pre Dir SW Veh CHP Veh Make Year SP Info OAF1 Viol OAF2 Safety Equip				Victim Info ROLE Ext Of Inj AGE Sex Seat Pos Safety EQUIP Ejected			
1F DRVR 33 M DRUG PROC ST E A 0100 - 2014 - 3 A 22350 - M B 2 PRKD 998 - PARKED E A 0100 - 2017 - - N - - -																					
Primary Rd MARINE AV Distance (ft) 0.00 Direction Secondary Rd CEDAR AV NCIC 1944 State Hwy? N Route Postmile Prefix Postmile Side of Hwy City Manhattan Beach County Los Angeles Population 4 Rpt Dist SOUTH Beat 004 Type 0 CalTrans Badge 316 Collision Date 20230123 Time 1537 Day MON Primary Collision Factor PED VIOL Violation 21456B Collision Type BROADSIDE Severity INJURY #Killed 0 #Injured 1 Tow Away? Process Date 20230303 Weather1 CLEAR Weather2 Rdwy Surface DRY Rdwy Cond1 NO UNUSL CND Rdwy Cond2 Spec Cond 0 Hit and Run Motor Vehicle Involved With BICYCLE Lighting DAYLIGHT Ped Action Cntrl Dev FNCTNG Loc Type Ramp/Int														Party Info Party Type Age Sex Race Sobriety1 Sobriety2 Move Pre Dir SW Veh CHP Veh Make Year SP Info OAF1 Viol OAF2 Safety Equip				Victim Info ROLE Ext Of Inj AGE Sex Seat Pos Safety EQUIP Ejected			
1F BICY 26 M H HNBD PROC ST W L 0400 - - 3 M - - - 2 DRVR 64 M W HNBD RGT TURN S A 0700 - 2017 - 3 N - - M G																					
Primary Rd MARINE AV Distance (ft) 108. Direction E Secondary Rd CEDAR AV NCIC 1944 State Hwy? N Route Postmile Prefix Postmile Side of Hwy City Manhattan Beach County Los Angeles Population 4 Rpt Dist M15 Beat 0M4 Type 0 CalTrans Badge 389 Collision Date 20230225 Time 2205 Day SAT Primary Collision Factor IMPROP TURN Violation 22107 Collision Type HIT OBJECT Severity PDO #Killed 0 #Injured 0 Tow Away? Y Process Date 20230412 Weather1 RAINING Weather2 WIND Rdwy Surface SLIPPERY Rdwy Cond1 NO UNUSL CND Rdwy Cond2 Spec Cond 0 Hit and Run Motor Vehicle Involved With FIXED OBJ Lighting DARK - ST Ped Action Cntrl Dev FNCTNG Loc Type Ramp/Int														Party Info Party Type Age Sex Race Sobriety1 Sobriety2 Move Pre Dir SW Veh CHP Veh Make Year SP Info OAF1 Viol OAF2 Safety Equip				Victim Info ROLE Ext Of Inj AGE Sex Seat Pos Safety EQUIP Ejected			
1F DRVR 20 M H HNBD LFT TURN E A 0100 - 2004 - 3 N - - M B																					
Primary Rd MARINE AV Distance (ft) 0.00 Direction Secondary Rd HERRIN AV NCIC 1944 State Hwy? N Route Postmile Prefix Postmile Side of Hwy City Manhattan Beach County Los Angeles Population 4 Rpt Dist M16 Beat 0M4 Type 0 CalTrans Badge 387 Collision Date 20230127 Time 0747 Day FRI Primary Collision Factor R-O-W PED Violation 21950A Collision Type AUTO/PED Severity INJURY #Killed 0 #Injured 1 Tow Away? Process Date 20230303 Weather1 CLEAR Weather2 Rdwy Surface DRY Rdwy Cond1 NO UNUSL CND Rdwy Cond2 Spec Cond 0 Hit and Run Motor Vehicle Involved With PED Lighting DAYLIGHT Ped Action NOT IN X- Cntrl Dev NT PRS/FCTR Loc Type Ramp/Int														Party Info Party Type Age Sex Race Sobriety1 Sobriety2 Move Pre Dir SW Veh CHP Veh Make Year SP Info OAF1 Viol OAF2 Safety Equip				Victim Info ROLE Ext Of Inj AGE Sex Seat Pos Safety EQUIP Ejected			
1F DRVR 60 M W HNBD RGT TURN E A 0100 - 2017 - 2 N - - M G 2 PED 56 M H IMP UNK IMP UNK OTHER E N 6000 - - - N - - - PED COMP PN 56 M - 0 - P																					
Primary Rd MARINE AV Distance (ft) 84.0 Direction W Secondary Rd ROWELL AV NCIC 1944 State Hwy? N Route Postmile Prefix Postmile Side of Hwy City Manhattan Beach County Los Angeles Population 4 Rpt Dist M16 Beat 0M4 Type 0 CalTrans Badge 372 Collision Date 20230508 Time 2225 Day MON Primary Collision Factor DRVR ALC DRG Violation 23152A Collision Type SIDESWIPE Severity PDO #Killed 0 #Injured 0 Tow Away? Y Process Date 20230629 Weather1 CLEAR Weather2 Rdwy Surface DRY Rdwy Cond1 NO UNUSL CND Rdwy Cond2 Spec Cond 0 Hit and Run MSDMNR Motor Vehicle Involved With PKD MV Lighting DARK - ST Ped Action Cntrl Dev NT PRS/FCTR Loc Type Ramp/Int														Party Info Party Type Age Sex Race Sobriety1 Sobriety2 Move Pre Dir SW Veh CHP Veh Make Year SP Info OAF1 Viol OAF2 Safety Equip				Victim Info ROLE Ext Of Inj AGE Sex Seat Pos Safety EQUIP Ejected			
1F DRVR 23 M H DRUG UNS TURN E A 0100 - 2011 - 1 A 22107 - L H 2 PRKD 998 - PARKED E A 0100 - 2006 - - N - - - 3 PRKD 998 - PARKED E A 0100 - 2019 - - N - - - 4 PRKD 998 - PARKED E A 0700 - 2023 - - N - - -																					

Include State Highways cases

Report Run On: 10/04/2023

Primary Rd		MEADOWS AV		Distance (ft)	0.00	Direction		Secondary Rd		18TH ST		NCIC	1944	State Hwy?	N	Route		Postmile Prefix		Postmile		Side of Hwy				
City		Manhattan Beach		County	Los Angeles	Population	4	Rpt Dist	M15	Beat	004	Type	0	CalTrans		Badge	387	Collision Date	20230806	Time	1901	Day	SUN			
Primary Collision Factor		UNSAFE SPEED		Violation	22350	Collision Type	OTHER	Severity	INJURY	#Killed	0	#Injured	1	Tow Away?	N	Process Date	20230918									
Weather1		CLEAR		Weather2		Rdwy Surface	DRY	Rdwy Cond1	NO UNUSL CND	Rdwy Cond2		Spec Cond	0	Hit and Run												
Hit and Run				Motor Vehicle Involved With		NON-CLSN		Lighting	DAYLIGHT	Ped Action		Cntrl Dev	NT PRS/FCTR	Loc Type		Ramp/Int										
Party Info													Victim Info													
Party	Type	Age	Sex	Race	Sobriety1	Sobriety2	Move Pre	Dir	SW Veh	CHP Veh	Make	Year	SP Info	OAF1	Viol	OAF2	Safety Equip	ROLE	Ext Of Inj	AGE	Sex	Seat Pos	Safety	EQUIP	Ejected	
1F	BICY	15	M	W	HNBD		PROC ST	N	L	0400	-	-	-	N	-	-	-	BICY	MINOR	15	M	1	1	-	W	
Primary Rd		MEADOWS AV		Distance (ft)	0.00	Direction		Secondary Rd		MANHATTAN		NCIC	1944	State Hwy?	N	Route		Postmile Prefix		Postmile		Side of Hwy				
City		Manhattan Beach		County	Los Angeles	Population	4	Rpt Dist	M15	Beat	0M4	Type	0	CalTrans		Badge	388	Collision Date	20230303	Time	2128	Day	FRI			
Primary Collision Factor		UNSAFE SPEED		Violation	22350	Collision Type	REAR END	Severity	PDO	#Killed	0	#Injured	0	Tow Away?	Y	Process Date	20230501									
Weather1		CLEAR		Weather2		Rdwy Surface	DRY	Rdwy Cond1	NO UNUSL CND	Rdwy Cond2		Spec Cond	0	Hit and Run												
Hit and Run				Motor Vehicle Involved With		PKD MV		Lighting	DARK - ST	Ped Action		Cntrl Dev	FNCTNG	Loc Type		Ramp/Int										
Party Info													Victim Info													
Party	Type	Age	Sex	Race	Sobriety1	Sobriety2	Move Pre	Dir	SW Veh	CHP Veh	Make	Year	SP Info	OAF1	Viol	OAF2	Safety Equip	ROLE	Ext Of Inj	AGE	Sex	Seat Pos	Safety	EQUIP	Ejected	
1F	DRVR	16	F	W	HBD-NUI		PROC ST	S	A	0700	-	2021	-	3	N	-	L	G	PASS		16	F	4	0	L	G
2	PRKD	998	-				PARKED	S	A	0700	JEEP	2012	-	3	N	-	-	-	PASS		16	F	6	0	L	G
3	PRKD	998	-				PARKED	S	A	0700	JEEP	2007	-	3	N	-	-	-								
Primary Rd		N SEPULVEDA BL		Distance (ft)	30.0	Direction	S	Secondary Rd		14TH ST		NCIC	1944	State Hwy?	Y	Route	1	Postmile Prefix	-	Postmile	23.010	Side of Hwy	N			
City		Manhattan Beach		County	Los Angeles	Population	4	Rpt Dist	M14	Beat	002	Type	0	CalTrans	7	Badge	353	Collision Date	20230330	Time	0900	Day	THU			
Primary Collision Factor		UNSAFE SPEED		Violation	22350	Collision Type	REAR END	Severity	PDO	#Killed	0	#Injured	0	Tow Away?		Process Date	20230623									
Weather1		CLEAR		Weather2		Rdwy Surface	WET	Rdwy Cond1	NO UNUSL CND	Rdwy Cond2		Spec Cond	0	Hit and Run												
Hit and Run		MSDMNR		Motor Vehicle Involved With		OTHER MV		Lighting	DAYLIGHT	Ped Action		Cntrl Dev	FNCTNG	Loc Type	H	Ramp/Int										
Party Info													Victim Info													
Party	Type	Age	Sex	Race	Sobriety1	Sobriety2	Move Pre	Dir	SW Veh	CHP Veh	Make	Year	SP Info	OAF1	Viol	OAF2	Safety Equip	ROLE	Ext Of Inj	AGE	Sex	Seat Pos	Safety	EQUIP	Ejected	
1F	DRVR	23	F	W	HNBD		PROC ST	N	A	0100	JEEP	2020	-	3	N	-	M	G	PASS		24	F	3	0	M	G
2	DRVR	43	F	W	HNBD		PROC ST	N	A	0100	-	2022	-	3	N	-	M	-								
3	DRVR	998	-		IMP UNK	IMP UNK	PROC ST	N	A	0100	-	-	-	N	-	-	-									
Primary Rd		N SEPULVEDA BL		Distance (ft)	0.00	Direction	S	Secondary Rd		19TH ST		NCIC	1944	State Hwy?	Y	Route	1	Postmile Prefix	-	Postmile	23.220	Side of Hwy	S			
City		Manhattan Beach		County	Los Angeles	Population	4	Rpt Dist	M14	Beat	002	Type	0	CalTrans	7	Badge	294	Collision Date	20230329	Time	0623	Day	WED			
Primary Collision Factor		UNSAFE SPEED		Violation	22350	Collision Type	HIT OBJECT	Severity	PDO	#Killed	0	#Injured	0	Tow Away?	Y	Process Date	20230419									
Weather1		RAINING		Weather2		Rdwy Surface	WET	Rdwy Cond1	NO UNUSL CND	Rdwy Cond2		Spec Cond	0	Hit and Run												
Hit and Run				Motor Vehicle Involved With		FIXED OBJ		Lighting	DARK - ST	Ped Action		Cntrl Dev	NT PRS/FCTR	Loc Type	H	Ramp/Int										
Party Info													Victim Info													
Party	Type	Age	Sex	Race	Sobriety1	Sobriety2	Move Pre	Dir	SW Veh	CHP Veh	Make	Year	SP Info	OAF1	Viol	OAF2	Safety Equip	ROLE	Ext Of Inj	AGE	Sex	Seat Pos	Safety	EQUIP	Ejected	
1F	DRVR	46	M	HNBD			PROC ST	-	A	0700	-	2021	-	3	N	-	L	G								
2	DRVR	50	F	W	HNBD		PROC ST	S	A	0700	-	2020	-	3	N	-	M	G								
Primary Rd		N SEPULVEDA BL		Distance (ft)	100.	Direction	S	Secondary Rd		MARINE AV		NCIC	1944	State Hwy?	N	Route		Postmile Prefix		Postmile		Side of Hwy				
City		Manhattan Beach		County	Los Angeles	Population	4	Rpt Dist	M14	Beat	002	Type	0	CalTrans		Badge	353	Collision Date	20230322	Time	1057	Day	WED			
Primary Collision Factor		IMPROP TURN		Violation	22107	Collision Type	HIT OBJECT	Severity	INJURY	#Killed	0	#Injured	1	Tow Away?	Y	Process Date	20230508									
Weather1		CLEAR		Weather2		Rdwy Surface	WET	Rdwy Cond1	NO UNUSL CND	Rdwy Cond2		Spec Cond	0	Hit and Run												
Hit and Run				Motor Vehicle Involved With		FIXED OBJ		Lighting	DAYLIGHT	Ped Action		Cntrl Dev	FNCTNG	Loc Type		Ramp/Int										
Party Info													Victim Info													
Party	Type	Age	Sex	Race	Sobriety1	Sobriety2	Move Pre	Dir	SW Veh	CHP Veh	Make	Year	SP Info	OAF1	Viol	OAF2	Safety Equip	ROLE	Ext Of Inj	AGE	Sex	Seat Pos	Safety	EQUIP	Ejected	
1F	DRVR	27	F	H	HNBD		LFT TURN	S	A	0100	-	1999	A	-	N	-	M	G	DRVR	POSSIBL	27	F	1	0	M	G

Include State Highways cases

Report Run On: 10/04/2023

Primary Rd PACIFIC AV Distance (ft) 18.0 Direction N Secondary Rd 14TH ST NCIC 1944 State Hwy? N Route Postmile Prefix Postmile Side of Hwy City Manhattan Beach County Los Angeles Population 4 Rpt Dist M13 Beat 002 Type 0 CalTrans Badge 383 Collision Date 20230414 Time 1848 Day FRI Primary Collision Factor IMPROP TURN Violation 22107 Collision Type HIT OBJECT Severity PDO #Killed 0 #Injured 0 Tow Away? N Process Date 20230628 Weather1 CLEAR Weather2 Rdwy Surface DRY Rdwy Cond1 NO UNUSL CND Rdwy Cond2 Spec Cond 0 Hit and Run MSDMNR Motor Vehicle Involved With FIXED OBJ Lighting DAYLIGHT Ped Action Cntrl Dev FUNCTNG Loc Type Ramp/Int																	
Party Info Party Type Age Sex Race Sobriety1 Sobriety2 Move Pre Dir SW Veh CHP Veh Make Year SP Info OAF1 Viol OAF2 Safety Equip 1F DRVR 61 F W IMP UNK IMP UNK LFT TURN N A 0700 AUDI 2017 - - N - B -														Victim Info ROLE Ext Of Inj AGE Sex Seat Pos Safety EQUIP Ejected - - - - - - - -			
Primary Rd REDONDO AV Distance (ft) 347. Direction N Secondary Rd 33RD ST NCIC 1944 State Hwy? N Route Postmile Prefix Postmile Side of Hwy City Manhattan Beach County Los Angeles Population 4 Rpt Dist M10 Beat 0M4 Type 0 CalTrans Badge 382 Collision Date 20230603 Time 0110 Day SAT Primary Collision Factor DRVR ALC DRG Violation 23152A Collision Type HIT OBJECT Severity INJURY #Killed 0 #Injured 1 Tow Away? Y Process Date 20230727 Weather1 CLEAR Weather2 Rdwy Surface DRY Rdwy Cond1 NO UNUSL CND Rdwy Cond2 Spec Cond 0 Hit and Run Motor Vehicle Involved With OTHER OBJ Lighting DARK - ST Ped Action Cntrl Dev FUNCTNG Loc Type Ramp/Int																	
Party Info Party Type Age Sex Race Sobriety1 Sobriety2 Move Pre Dir SW Veh CHP Veh Make Year SP Info OAF1 Viol OAF2 Safety Equip 1F DRVR 28 M H HBD-UI PROC ST S C 0200 - 2022 - 3 A 22350 N P W														Victim Info ROLE Ext Of Inj AGE Sex Seat Pos Safety EQUIP Ejected DRVR OTH VIS 28 M 1 1 P W			
Primary Rd REDONDO AV Distance (ft) 0.00 Direction Secondary Rd MATHEWS AV NCIC 1944 State Hwy? N Route Postmile Prefix Postmile Side of Hwy City Manhattan Beach County Los Angeles Population 4 Rpt Dist M27 Beat 0M1 Type 0 CalTrans Badge 294 Collision Date 20230614 Time 1342 Day WED Primary Collision Factor R-O-W AUTO Violation 21802A Collision Type BROADSIDE Severity INJURY #Killed 0 #Injured 1 Tow Away? Y Process Date 20230728 Weather1 CLEAR Weather2 Rdwy Surface DRY Rdwy Cond1 NO UNUSL CND Rdwy Cond2 Spec Cond 0 Hit and Run Motor Vehicle Involved With OTHER MV Lighting DAYLIGHT Ped Action Cntrl Dev FUNCTNG Loc Type Ramp/Int																	
Party Info Party Type Age Sex Race Sobriety1 Sobriety2 Move Pre Dir SW Veh CHP Veh Make Year SP Info OAF1 Viol OAF2 Safety Equip 1F DRVR 49 F W HNBD PROC ST W A 0100 - 2020 - 3 N - M G														Victim Info ROLE Ext Of Inj AGE Sex Seat Pos Safety EQUIP Ejected DRVR POSSIBL 51 F 1 0 M G			
Party Info Party Type Age Sex Race Sobriety1 Sobriety2 Move Pre Dir SW Veh CHP Veh Make Year SP Info OAF1 Viol OAF2 Safety Equip 2 DRVR 51 F W HNBD PROC ST S A 0700 - 2021 - 3 N - M G														Victim Info ROLE Ext Of Inj AGE Sex Seat Pos Safety EQUIP Ejected PASS 57 M 3 0 M G			
Primary Rd ROSECRANS AV Distance (ft) 487. Direction W Secondary Rd BELL AV NCIC 1944 State Hwy? N Route Postmile Prefix Postmile Side of Hwy City Manhattan Beach County Los Angeles Population 4 Rpt Dist M3 Beat 0M3 Type 0 CalTrans Badge 391 Collision Date 20230727 Time 2210 Day THU Primary Collision Factor PED VIOL Violation 21954A Collision Type AUTO/PED Severity INJURY #Killed 0 #Injured 1 Tow Away? N Process Date 20230913 Weather1 CLEAR Weather2 Rdwy Surface DRY Rdwy Cond1 NO UNUSL CND Rdwy Cond2 Spec Cond 0 Hit and Run Motor Vehicle Involved With PED Lighting DARK - ST Ped Action IN RD, Cntrl Dev NT PRS/FCTR Loc Type Ramp/Int																	
Party Info Party Type Age Sex Race Sobriety1 Sobriety2 Move Pre Dir SW Veh CHP Veh Make Year SP Info OAF1 Viol OAF2 Safety Equip 1F PED 31 M W HNBD PROC ST E N 6000 - - 3 N - - -														Victim Info ROLE Ext Of Inj AGE Sex Seat Pos Safety EQUIP Ejected PED MINOR 31 M - - - P			
Party Info Party Type Age Sex Race Sobriety1 Sobriety2 Move Pre Dir SW Veh CHP Veh Make Year SP Info OAF1 Viol OAF2 Safety Equip 2 DRVR 20 M O HNBD PROC ST E A 0700 - 2020 - 3 A 22350 - M G														Victim Info ROLE Ext Of Inj AGE Sex Seat Pos Safety EQUIP Ejected PASS 18 M 4 0 M G			
Party Info Party Type Age Sex Race Sobriety1 Sobriety2 Move Pre Dir SW Veh CHP Veh Make Year SP Info OAF1 Viol OAF2 Safety Equip 2 DRVR 20 M O HNBD PROC ST E A 0700 - 2020 - 3 A 22350 - M G														Victim Info ROLE Ext Of Inj AGE Sex Seat Pos Safety EQUIP Ejected PASS 21 M 3 0 M G			
Primary Rd ROSECRANS AV Distance (ft) 0.00 Direction Secondary Rd FLOURNOY RD NCIC 1944 State Hwy? N Route Postmile Prefix Postmile Side of Hwy City Manhattan Beach County Los Angeles Population 4 Rpt Dist SOUTH Beat 003 Type 0 CalTrans Badge 316 Collision Date 20230123 Time 0751 Day MON Primary Collision Factor PED VIOL Violation 21950B Collision Type BROADSIDE Severity INJURY #Killed 0 #Injured 1 Tow Away? N Process Date 20230228 Weather1 CLEAR Weather2 Rdwy Surface DRY Rdwy Cond1 NO UNUSL CND Rdwy Cond2 Spec Cond 0 Hit and Run Motor Vehicle Involved With PED Lighting DAYLIGHT Ped Action Cntrl Dev NT PRS/FCTR Loc Type Ramp/Int																	
Party Info Party Type Age Sex Race Sobriety1 Sobriety2 Move Pre Dir SW Veh CHP Veh Make Year SP Info OAF1 Viol OAF2 Safety Equip 1F DRVR 29 M H HNBD PROC ST W - 0000 - - 3 E - P W														Victim Info ROLE Ext Of Inj AGE Sex Seat Pos Safety EQUIP Ejected DRVR OTH VIS 29 M 1 1 P V			
Party Info Party Type Age Sex Race Sobriety1 Sobriety2 Move Pre Dir SW Veh CHP Veh Make Year SP Info OAF1 Viol OAF2 Safety Equip 2 DRVR 45 M W HNBD RGT TURN N A 0700 - 2022 - 3 E - M G														Victim Info ROLE Ext Of Inj AGE Sex Seat Pos Safety EQUIP Ejected - - - - - - - -			

Include State Highways cases

Report Run On: 10/04/2023

Primary Rd		ROSECRANS AV		Distance (ft)	17.0	Direction	E	Secondary Rd		FLOURNOY RD		NCIC	1944	State Hwy?	N	Route	Postmile Prefix	Postmile	Side of Hwy							
City		Manhattan Beach		County	Los Angeles	Population	4	Rpt Dist	M4	Beat	0M4	Type	0	CalTrans	Badge	389	Collision Date	20230327	Time	1927	Day	MON				
Primary Collision Factor		R-O-W AUTO		Violation	21801A	Collision Type	BROADSIDE	Severity	INJURY	#Killed	0	#Injured	2	Tow Away?	Y	Process Date	20230508									
Weather1		CLEAR		Weather2		Rdwy Surface	DRY	Rdwy Cond1	NO UNUSL CND	Rdwy Cond2		Spec Cond	0													
Hit and Run				Motor Vehicle Involved With		OTHER MV		Lighting	DARK - ST	Ped Action		Cntrl Dev	NT PRS/FCTR	Loc Type		Ramp/Int										
Party Info													Victim Info													
Party	Type	Age	Sex	Race	Sobriety1	Sobriety2	Move Pre	Dir	SW Veh	CHP Veh	Make	Year	SP Info	OAF1	Viol	OAF2	Safety Equip	ROLE	Ext Of Inj	AGE	Sex	Seat Pos	Safety	EQUIP	Ejected	
1F	DRVR	64	F	A	HNBD		LFT TURN	W	A	0700	-	2017	-	A	22107	-	L	G	DRVR	SERIOUS	64	F	1	0	L	G
2	DRVR	23	M	H	HBD-NUI		PROC ST	E	A	0100	-	2011	-	A	22350	-	L	G	DRVR	POSSIBL	23	M	1	0	L	G
																			PASS		22	F	2	0	L	G
Primary Rd		ROSECRANS AV		Distance (ft)	0.00	Direction		Secondary Rd		REDONDO AV		NCIC	1944	State Hwy?	N	Route	Postmile Prefix	Postmile	Side of Hwy							
City		Manhattan Beach		County	Los Angeles	Population	4	Rpt Dist	M8	Beat	004	Type	0	CalTrans	Badge	373	Collision Date	20230109	Time	0554	Day	MON				
Primary Collision Factor		UNKNOWN		Violation		Collision Type	BROADSIDE	Severity	INJURY	#Killed	0	#Injured	1	Tow Away?	Y	Process Date	20230302									
Weather1		RAINING		Weather2		Rdwy Surface	WET	Rdwy Cond1	NO UNUSL CND	Rdwy Cond2		Spec Cond	0													
Hit and Run				Motor Vehicle Involved With		OTHER MV		Lighting	DARK - ST	Ped Action		Cntrl Dev	FUNCTNG	Loc Type		Ramp/Int										
Party Info													Victim Info													
Party	Type	Age	Sex	Race	Sobriety1	Sobriety2	Move Pre	Dir	SW Veh	CHP Veh	Make	Year	SP Info	OAF1	Viol	OAF2	Safety Equip	ROLE	Ext Of Inj	AGE	Sex	Seat Pos	Safety	EQUIP	Ejected	
1	DRVR	26	M	H	HNBD		LFT TURN	S	D	2200	FORD	2019	-	3	M	-	L	G	DRVR	COMP PN	26	M	1	0	L	G
2	DRVR	30	M		HNBD		PROC ST	E	A	0100	-	2020	-	3	M	-	L	G								
Primary Rd		ROSECRANS AV		Distance (ft)	82.0	Direction	W	Secondary Rd		SEPULVEDA BL		NCIC	1944	State Hwy?	Y	Route	1	Postmile Prefix	-	Postmile	23.924	Side of Hwy	S			
City		Manhattan Beach		County	Los Angeles	Population	4	Rpt Dist	M2	Beat	0M8	Type	0	CalTrans	7	Badge	390	Collision Date	20230111	Time	0107	Day	WED			
Primary Collision Factor		IMPROP TURN		Violation	22107	Collision Type	HIT OBJECT	Severity	PDO	#Killed	0	#Injured	0	Tow Away?	Y	Process Date	20230227									
Weather1		CLEAR		Weather2		Rdwy Surface	DRY	Rdwy Cond1	NO UNUSL CND	Rdwy Cond2		Spec Cond	0													
Hit and Run		MSDMNR		Motor Vehicle Involved With		FIXED OBJ		Lighting	DARK - ST	Ped Action		Cntrl Dev	NT PRS/FCTR	Loc Type	I	Ramp/Int						6				
Party Info													Victim Info													
Party	Type	Age	Sex	Race	Sobriety1	Sobriety2	Move Pre	Dir	SW Veh	CHP Veh	Make	Year	SP Info	OAF1	Viol	OAF2	Safety Equip	ROLE	Ext Of Inj	AGE	Sex	Seat Pos	Safety	EQUIP	Ejected	
1F	DRVR	998	-		IMP UNK	IMP UNK	PROC ST	E	A	0100	-	2019	-	-	A	22350	M	L	B							
Primary Rd		ROSECRANS AV		Distance (ft)	90.0	Direction	E	Secondary Rd		SEPULVEDA BL		NCIC	1944	State Hwy?	N	Route	Postmile Prefix	Postmile	Side of Hwy							
City		Manhattan Beach		County	Los Angeles	Population	4	Rpt Dist	M7	Beat	0M4	Type	0	CalTrans	Badge	382	Collision Date	20230617	Time	0007	Day	SAT				
Primary Collision Factor		DRVR ALC DRG		Violation	23152A	Collision Type	HIT OBJECT	Severity	INJURY	#Killed	0	#Injured	1	Tow Away?	Y	Process Date	20230728									
Weather1		CLEAR		Weather2		Rdwy Surface	DRY	Rdwy Cond1	NO UNUSL CND	Rdwy Cond2		Spec Cond	0													
Hit and Run				Motor Vehicle Involved With		FIXED OBJ		Lighting	DARK - ST	Ped Action		Cntrl Dev	FUNCTNG	Loc Type		Ramp/Int										
Party Info													Victim Info													
Party	Type	Age	Sex	Race	Sobriety1	Sobriety2	Move Pre	Dir	SW Veh	CHP Veh	Make	Year	SP Info	OAF1	Viol	OAF2	Safety Equip	ROLE	Ext Of Inj	AGE	Sex	Seat Pos	Safety	EQUIP	Ejected	
1F	DRVR	39	F	W	HBD-UI		PROC ST	E	A	0100	-	2021	-	-	N	-	L	G	DRVR	MINOR	39	F	1	0	L	G
Primary Rd		ROSECRANS AV		Distance (ft)	1.00	Direction	E	Secondary Rd		SEPULVEDA BL		NCIC	1944	State Hwy?	N	Route	Postmile Prefix	Postmile	Side of Hwy							
City		Manhattan Beach		County	Los Angeles	Population	4	Rpt Dist	M5	Beat	0M3	Type	0	CalTrans	Badge	388	Collision Date	20230827	Time	0800	Day	SUN				
Primary Collision Factor		R-O-W PED		Violation	21950A	Collision Type	AUTO/PED	Severity	INJURY	#Killed	0	#Injured	1	Tow Away?	N	Process Date	20230918									
Weather1		CLEAR		Weather2		Rdwy Surface	DRY	Rdwy Cond1	NO UNUSL CND	Rdwy Cond2		Spec Cond	0													
Hit and Run		FELONY		Motor Vehicle Involved With		PED		Lighting	DAYLIGHT	Ped Action	X-WLK AT	Cntrl Dev	FUNCTNG	Loc Type		Ramp/Int										
Party Info													Victim Info													
Party	Type	Age	Sex	Race	Sobriety1	Sobriety2	Move Pre	Dir	SW Veh	CHP Veh	Make	Year	SP Info	OAF1	Viol	OAF2	Safety Equip	ROLE	Ext Of Inj	AGE	Sex	Seat Pos	Safety	EQUIP	Ejected	
1F	DRVR	998	-		IMP UNK	IMP UNK	RGT TURN	E	-	0000	-	-	-	N	-	-	-									
2	PED	52	F	H	HBD-NUI		PROC ST	N	N	6000	-	-	-	N	-	-	-								P	

Primary Rd	ROSECRANS AV	Distance (ft)	0.00	Direction		Secondary Rd	VISTA DR	NCIC	1944	State Hwy?	N	Route		Postmile Prefix		Postmile		Side of Hwy		
City	Manhattan Beach	County	Los Angeles	Population	4	Rpt Dist	M3	Beat	0M3	Type	0	CalTrans	Badge	294	Collision Date	20230420	Time	1024	Day	THU
Primary Collision Factor	R-O-W AUTO	Violation	21800A	Collision Type	BROADSIDE	Severity	PDO	#Killed	0	#Injured	0	Tow Away?	Y	Process Date	20230609					
Weather1	CLEAR	Weather2		Rdwy Surface	DRY	Rdwy Cond1	NO UNUSL CND	Rdwy Cond2		Spec Cond	0									
Hit and Run		Motor Vehicle Involved With	MV ON OTHER RD	Lighting	DAYLIGHT	Ped Action		Cntrl Dev		FUNCTNG		Loc Type		Ramp/Int						

Party Info														Victim Info												
Party	Type	Age	Sex	Race	Sobriety1	Sobriety2	Move Pre	Dir	SW Veh	CHP Veh	Make	Year	SP Info	OAF1	Viol	OAF2	Safety Equip	ROLE	Ext Of Inj	AGE	Sex	Seat Pos	Safety	EQUIP	Ejected	
1F	DRVR	49	F		HNBD		RGT TURN	N	A	0100	-	2022	-	-	N	-	B	G								
2	DRVR	33	M	W	HNBD		PROC ST	E	A	0100	-	2014	-	-	N	-	L	G								
3	PRKD	998	-				PARKED	E	A	0800	-	2014	-	3	N	-	-	-								
4	PRKD	998	-				PARKED	E	A	0700	-	2010	-	3	N	-	-	-								

Primary Rd	ROSECRANS AV	Distance (ft)	1.00	Direction	E	Secondary Rd	VISTA DR	NCIC	1944	State Hwy?	N	Route		Postmile Prefix		Postmile		Side of Hwy		
City	Manhattan Beach	County	Los Angeles	Population	4	Rpt Dist	SOUTH	Beat	003	Type	0	CalTrans	Badge	329	Collision Date	20230701	Time	0906	Day	SAT
Primary Collision Factor	R-O-W AUTO	Violation	21802A	Collision Type	BROADSIDE	Severity	INJURY	#Killed	0	#Injured	1	Tow Away?	N	Process Date	20230811					
Weather1	CLEAR	Weather2		Rdwy Surface	DRY	Rdwy Cond1	NO UNUSL CND	Rdwy Cond2		Spec Cond	0									
Hit and Run		Motor Vehicle Involved With	OTHER MV	Lighting	DAYLIGHT	Ped Action		Cntrl Dev		FUNCTNG		Loc Type		Ramp/Int						

Party Info														Victim Info												
Party	Type	Age	Sex	Race	Sobriety1	Sobriety2	Move Pre	Dir	SW Veh	CHP Veh	Make	Year	SP Info	OAF1	Viol	OAF2	Safety Equip	ROLE	Ext Of Inj	AGE	Sex	Seat Pos	Safety	EQUIP	Ejected	
1F	DRVR	26	M	H	HNBD		RGT TURN	N	A	0100	-	2015	-	3	N	-	M	G	DRVR	OTH VIS	26	M	1	0	M	G
2	DRVR	25	M	B	HNBD		PROC ST	E	A	0100	-	2018	-	3	N	-	L	G								
3	PRKD	998	-				PARKED	E	D	2200	-	2000	-	3	N	-	-	-								

Primary Rd	SEPULVEDA BL	Distance (ft)	0.00	Direction		Secondary Rd	10TH ST	NCIC	1944	State Hwy?	Y	Route	1	Postmile Prefix	-	Postmile	22.770	Side of Hwy	N		
City	Manhattan Beach	County	Los Angeles	Population	4	Rpt Dist		Beat	0M1	Type	0	CalTrans	7	Badge	387	Collision Date	20230120	Time	1600	Day	FRI
Primary Collision Factor	IMPROP TURN	Violation	22107	Collision Type	REAR END	Severity	PDO	#Killed	0	#Injured	0	Tow Away?	N	Process Date	20230314						
Weather1	CLEAR	Weather2		Rdwy Surface	DRY	Rdwy Cond1	NO UNUSL CND	Rdwy Cond2		Spec Cond	0										
Hit and Run	MSDMNR	Motor Vehicle Involved With	OTHER MV	Lighting	DUSK/DAWN	Ped Action		Cntrl Dev		FUNCTNG		Loc Type	H	Ramp/Int	-						

Party Info														Victim Info												
Party	Type	Age	Sex	Race	Sobriety1	Sobriety2	Move Pre	Dir	SW Veh	CHP Veh	Make	Year	SP Info	OAF1	Viol	OAF2	Safety Equip	ROLE	Ext Of Inj	AGE	Sex	Seat Pos	Safety	EQUIP	Ejected	
1F	DRVR	998	F	W	IMP UNK	IMP UNK	CHANG LN	N	A	0100	-	2014	-	-	N	-	N	B								
2	DRVR	55	M	W	HNBD		STOPPED	N	D	2200	-	2012	-	2	N	-	M	G								

Primary Rd	SEPULVEDA BL	Distance (ft)	0.00	Direction		Secondary Rd	14TH ST	NCIC	1944	State Hwy?	N	Route		Postmile Prefix		Postmile		Side of Hwy		
City	Manhattan Beach	County	Los Angeles	Population	4	Rpt Dist	SOUTH	Beat	002	Type	0	CalTrans	Badge	316	Collision Date	20230116	Time	1427	Day	MON
Primary Collision Factor	R-O-W AUTO	Violation	21801A	Collision Type	BROADSIDE	Severity	PDO	#Killed	0	#Injured	0	Tow Away?	Y	Process Date	20230224					
Weather1	CLEAR	Weather2		Rdwy Surface	DRY	Rdwy Cond1	NO UNUSL CND	Rdwy Cond2		Spec Cond	0									
Hit and Run		Motor Vehicle Involved With	OTHER MV	Lighting	DAYLIGHT	Ped Action		Cntrl Dev		FUNCTNG		Loc Type		Ramp/Int						

Party Info														Victim Info												
Party	Type	Age	Sex	Race	Sobriety1	Sobriety2	Move Pre	Dir	SW Veh	CHP Veh	Make	Year	SP Info	OAF1	Viol	OAF2	Safety Equip	ROLE	Ext Of Inj	AGE	Sex	Seat Pos	Safety	EQUIP	Ejected	
1F	DRVR	38	M	W	HNBD		LFT TURN	N	A	0100	-	2021	-	3	N	-	L	G	PASS		2	M	6	0	L	-
2	DRVR	61	M	W	HNBD		PROC ST	S	A	0100	-	2019	-	3	N	-	M	G								

Primary Rd	SEPULVEDA BL	Distance (ft)	80.0	Direction	N	Secondary Rd	17TH ST	NCIC	1944	State Hwy?	Y	Route	1	Postmile Prefix	-	Postmile	23.110	Side of Hwy	N		
City	Manhattan Beach	County	Los Angeles	Population	4	Rpt Dist	M15	Beat	0M4	Type	0	CalTrans	7	Badge	294	Collision Date	20230322	Time	1213	Day	WED
Primary Collision Factor	PED VIOL	Violation	21954A	Collision Type	AUTO/PED	Severity	INJURY	#Killed	0	#Injured	1	Tow Away?	N	Process Date	20230414						
Weather1	CLOUDY	Weather2	RAINING	Rdwy Surface	WET	Rdwy Cond1	NO UNUSL CND	Rdwy Cond2		Spec Cond	0										
Hit and Run		Motor Vehicle Involved With	PED	Lighting	DAYLIGHT	Ped Action	IN RD,	Cntrl Dev		FUNCTNG		Loc Type	H	Ramp/Int	-						

Party Info														Victim Info												
Party	Type	Age	Sex	Race	Sobriety1	Sobriety2	Move Pre	Dir	SW Veh	CHP Veh	Make	Year	SP Info	OAF1	Viol	OAF2	Safety Equip	ROLE	Ext Of Inj	AGE	Sex	Seat Pos	Safety	EQUIP	Ejected	
1F	PED	52	M	W	HNBD		OTHER	N	N	6000	-	-	-	3	N	-	-	-	PED	MINOR	52	M	-	-	P	-
2	DRVR	37	F	W	HNBD		PROC ST	N	A	0100	-	2021	-	3	N	-	M	G								

Primary Rd	SEPULVEDA BL	Distance (ft)	13.0	Direction	S	Secondary Rd	17TH ST	NCIC	1944	State Hwy?	Y	Route	1	Postmile Prefix	-	Postmile	23.090	Side of Hwy	S		
City	Manhattan Beach	County	Los Angeles	Population	4	Rpt Dist	SOUTH	Beat	003	Type	0	CalTrans	7	Badge	316	Collision Date	20230710	Time	1557	Day	MON
Primary Collision Factor	IMPROP TURN	Violation	22107	Collision Type	REAR END	Severity	INJURY	#Killed	0	#Injured	1	Tow Away?	N	Process Date	20230802						
Weather1	CLEAR	Weather2		Rdwy Surface	DRY	Rdwy Cond1	NO UNUSL CND	Rdwy Cond2		Spec Cond	0										
Hit and Run	FELONY	Motor Vehicle Involved With	OTHER MV	Lighting	DAYLIGHT	Ped Action		Cntrl Dev	NT PRS/FCTR	Loc Type	H	Ramp/Int	-								

Party Info														Victim Info												
Party	Type	Age	Sex	Race	Sobriety1	Sobriety2	Move Pre	Dir	SW Veh	CHP Veh	Make	Year	SP Info	OAF1	Viol	OAF2	Safety Equip	ROLE	Ext Of Inj	AGE	Sex	Seat Pos	Safety	EQUIP	Ejected	
1F	DRVR	998	-		HBD-UNK		CHANG LN	S	A	0700	-	-	-	N	-	B	-									
2	DRVR	20	M	W	HNBD		PROC ST	S	C	0200	-	2004	-	3	N	-	P	W	DRVR	MINOR	20	M	1	1	P	W
3	DRVR	24	M	W	HNBD		PROC ST	S	A	0700	-	2019	-	3	N	-	M	G								

Primary Rd	SEPULVEDA BL	Distance (ft)	285.	Direction	S	Secondary Rd	18TH ST	NCIC	1944	State Hwy?	Y	Route	1	Postmile Prefix	-	Postmile	23.150	Side of Hwy	N		
City	Manhattan Beach	County	Los Angeles	Population	4	Rpt Dist	M15	Beat	0M4	Type	0	CalTrans	7	Badge	384	Collision Date	20230202	Time	2155	Day	THU
Primary Collision Factor	IMPROP TURN	Violation	22107	Collision Type	SIDESWIPE	Severity	INJURY	#Killed	0	#Injured	1	Tow Away?	N	Process Date	20230315						
Weather1	CLEAR	Weather2		Rdwy Surface	DRY	Rdwy Cond1	NO UNUSL CND	Rdwy Cond2		Spec Cond	0										
Hit and Run	FELONY	Motor Vehicle Involved With	OTHER MV	Lighting	DARK - ST	Ped Action		Cntrl Dev	NT PRS/FCTR	Loc Type	H	Ramp/Int	-								

Party Info														Victim Info												
Party	Type	Age	Sex	Race	Sobriety1	Sobriety2	Move Pre	Dir	SW Veh	CHP Veh	Make	Year	SP Info	OAF1	Viol	OAF2	Safety Equip	ROLE	Ext Of Inj	AGE	Sex	Seat Pos	Safety	EQUIP	Ejected	
1F	DRVR	27	F	B	HBD-UNK		PASSING	N	A	0700	-	2003	-	-	A	22350	-	M	B							
2	DRVR	56	F	W	HNBD		PROC ST	N	A	0700	-	2022	-	2	N	-	M	G	DRVR	COMP PN 56	F	1	0	M	G	

Primary Rd	SEPULVEDA BL	Distance (ft)	35.0	Direction	S	Secondary Rd	18TH ST	NCIC	1944	State Hwy?	Y	Route	1	Postmile Prefix	-	Postmile	23.181	Side of Hwy	N		
City	Manhattan Beach	County	Los Angeles	Population	4	Rpt Dist	M15	Beat	0M4	Type	0	CalTrans	7	Badge	3711	Collision Date	20230513	Time	1804	Day	SAT
Primary Collision Factor	UNSAFE SPEED	Violation	22350	Collision Type	REAR END	Severity	PDO	#Killed	0	#Injured	0	Tow Away?		Process Date	20230811						
Weather1	CLEAR	Weather2		Rdwy Surface	DRY	Rdwy Cond1	NO UNUSL CND	Rdwy Cond2		Spec Cond	0										
Hit and Run	MSDMNR	Motor Vehicle Involved With	OTHER MV	Lighting	DAYLIGHT	Ped Action		Cntrl Dev	FNCTNG	Loc Type	H	Ramp/Int	-								

Party Info														Victim Info													
Party	Type	Age	Sex	Race	Sobriety1	Sobriety2	Move Pre	Dir	SW Veh	CHP Veh	Make	Year	SP Info	OAF1	Viol	OAF2	Safety Equip	ROLE	Ext Of Inj	AGE	Sex	Seat Pos	Safety	EQUIP	Ejected		
1F	DRVR	19	M	H	HNBD		PROC ST	N	A	0100	-	2000	-	3	A	2800	-	M	G	PASS		17	M	6	0	M	G
																				PASS		15	M	4	0	M	G
2	DRVR	41	F	W	HNBD		STOPPED	N	A	0700	-	2022	-	3	N	-	M	G	PASS		11	F	6	0	M	G	
																				PASS		6	M	4	0	M	Q

Primary Rd	SEPULVEDA BL	Distance (ft)	0.00	Direction		Secondary Rd	27TH ST	NCIC	1944	State Hwy?	Y	Route	1	Postmile Prefix	-	Postmile	23.510	Side of Hwy	S		
City	Manhattan Beach	County	Los Angeles	Population	4	Rpt Dist	M6	Beat	0M2	Type	0	CalTrans	7	Badge	387	Collision Date	20230121	Time	1234	Day	SAT
Primary Collision Factor	UNSAFE SPEED	Violation	22350	Collision Type	REAR END	Severity	INJURY	#Killed	0	#Injured	2	Tow Away?	Y	Process Date	20230315						
Weather1	CLEAR	Weather2		Rdwy Surface	DRY	Rdwy Cond1	NO UNUSL CND	Rdwy Cond2		Spec Cond	0										
Hit and Run		Motor Vehicle Involved With	OTHER MV	Lighting	DAYLIGHT	Ped Action		Cntrl Dev	NT PRS/FCTR	Loc Type	H	Ramp/Int	-								

Party Info														Victim Info												
Party	Type	Age	Sex	Race	Sobriety1	Sobriety2	Move Pre	Dir	SW Veh	CHP Veh	Make	Year	SP Info	OAF1	Viol	OAF2	Safety Equip	ROLE	Ext Of Inj	AGE	Sex	Seat Pos	Safety	EQUIP	Ejected	
1F	DRVR	21	F	W	HNBD		PROC ST	S	A	0700	-	2004	-	1	A	23123	F	M	G	DRVR	COMP PN 21	F	1	0	M	G
2	DRVR	48	F	B	HNBD		STOPPED	S	A	0100	-	2021	-	3	N	-	M	G	DRVR	COMP PN 48	F	1	0	M	G	

Primary Rd	SEPULVEDA BL	Distance (ft)	165.	Direction	N	Secondary Rd	2ND ST	NCIC	1944	State Hwy?	Y	Route	1	Postmile Prefix	-	Postmile	22.410	Side of Hwy	N		
City	Manhattan Beach	County	Los Angeles	Population	4	Rpt Dist		Beat		Type	0	CalTrans	7	Badge	302	Collision Date	20230204	Time	0948	Day	SAT
Primary Collision Factor	UNSAFE SPEED	Violation	22350	Collision Type	REAR END	Severity	INJURY	#Killed	0	#Injured	2	Tow Away?	Y	Process Date	20230315						
Weather1	CLEAR	Weather2		Rdwy Surface	DRY	Rdwy Cond1	NO UNUSL CND	Rdwy Cond2		Spec Cond	0										
Hit and Run		Motor Vehicle Involved With	OTHER MV	Lighting	DAYLIGHT	Ped Action		Cntrl Dev	FNCTNG	Loc Type	H	Ramp/Int	-								

Party Info														Victim Info												
Party	Type	Age	Sex	Race	Sobriety1	Sobriety2	Move Pre	Dir	SW Veh	CHP Veh	Make	Year	SP Info	OAF1	Viol	OAF2	Safety Equip	ROLE	Ext Of Inj	AGE	Sex	Seat Pos	Safety	EQUIP	Ejected	
1F	DRVR	23	F	W	HNBD		PROC ST	N	A	0100	-	2011	-	3	N	-	M	G								
2	DRVR	55	F	W	HNBD		STOPPED	N	A	0100	FORD	1994	-	3	N	-	M	G	DRVR	COMP PN 55	F	1	0	M	G	

Include State Highways cases

Report Run On: 10/04/2023

3 DRVR 58 M W HNBD STOPPED N A 0700 - 2017 - 3 N - M G DRVR COMP PN 58 M 1 0 M G

Primary Rd SEPULVEDA BL Distance (ft) 0.00 Direction Secondary Rd 2ND ST NCIC 1944 State Hwy? Y Route 1 Postmile Prefix - Postmile 22.391 Side of Hwy S
City Manhattan Beach County Los Angeles Population 4 Rpt Dist M17 Beat 0M4 Type 0 CalTrans 7 Badge 367 Collision Date 20230512 Time 2313 Day FRI
Primary Collision Factor DRVR ALC|DRG Violation 23152B Collision Type BROADSIDE Severity PDO #Killed 0 #Injured 0 Tow Away? Y Process Date 20230714
Weather1 CLEAR Weather2 Rdwy Surface DRY Rdwy Cond1 NO UNUSL CND Rdwy Cond2 Spec Cond 0
Hit and Run MSDMNR Motor Vehicle Involved With OTHER MV Lighting DARK - ST Ped Action Cntrl Dev FNCTNG Loc Type I Ramp/Int 5

Party Info Party Type Age Sex Race Sobriety1 Sobriety2 Move Pre Dir SW Veh CHP Veh Make Year SP Info OAF1 Viol OAF2 Safety Equip ROLE Ext Of Inj AGE Sex Seat Pos Safety EQUIP Ejected
1F DRVR 38 M H HNBD LFT TURN N D 7200 - 2001 - 3 A 21453 - M G
2 DRVR 25 F W HBD-NUI PROC ST S A 0700 - 2006 - 3 N - M G

Primary Rd SEPULVEDA BL Distance (ft) 0.00 Direction Secondary Rd 2ND ST NCIC 1944 State Hwy? N Route Postmile Prefix Postmile Side of Hwy
City Manhattan Beach County Los Angeles Population 4 Rpt Dist SOUTH Beat 002 Type 0 CalTrans Badge 316 Collision Date 20230828 Time 1058 Day MON
Primary Collision Factor STOP SGN|SIG Violation 21453A Collision Type BROADSIDE Severity INJURY #Killed 0 #Injured 1 Tow Away? Y Process Date 20230919
Weather1 CLEAR Weather2 Rdwy Surface DRY Rdwy Cond1 HOLES Rdwy Cond2 Spec Cond 0
Hit and Run Motor Vehicle Involved With OTHER MV Lighting DAYLIGHT Ped Action Cntrl Dev FNCTNG Loc Type Ramp/Int

Party Info Party Type Age Sex Race Sobriety1 Sobriety2 Move Pre Dir SW Veh CHP Veh Make Year SP Info OAF1 Viol OAF2 Safety Equip ROLE Ext Of Inj AGE Sex Seat Pos Safety EQUIP Ejected
1F DRVR 48 M A HNBD STOPPED W D 2200 - 2023 - 3 N - L G
2 DRVR 57 M W HNBD PROC ST S D 2200 - 2021 - 3 N - L G DRVR MINOR 57 M 1 0 L G

Primary Rd SEPULVEDA BL Distance (ft) 77.0 Direction N Secondary Rd 30TH ST NCIC 1944 State Hwy? Y Route 1 Postmile Prefix - Postmile 23.610 Side of Hwy N
City Manhattan Beach County Los Angeles Population 4 Rpt Dist M7 Beat 0M4 Type 0 CalTrans 7 Badge 383 Collision Date 20230128 Time 2129 Day SAT
Primary Collision Factor R-O-W AUTO Violation 21804 Collision Type REAR END Severity INJURY #Killed 0 #Injured 2 Tow Away? Y Process Date 20230315
Weather1 CLEAR Weather2 Rdwy Surface DRY Rdwy Cond1 NO UNUSL CND Rdwy Cond2 Spec Cond 0
Hit and Run Motor Vehicle Involved With Lighting DARK - ST Ped Action Cntrl Dev NT PRS/FCTR Loc Type H Ramp/Int -

Party Info Party Type Age Sex Race Sobriety1 Sobriety2 Move Pre Dir SW Veh CHP Veh Make Year SP Info OAF1 Viol OAF2 Safety Equip ROLE Ext Of Inj AGE Sex Seat Pos Safety EQUIP Ejected
1F DRVR 20 F B HNBD ENT TRAF N A 0100 - 2020 - 3 A 22107 - M C PASS 25 F 3 0 M C
2 DRVR 21 M B HNBD PROC ST N C 0200 - 2023 - 3 A 22350 - P W DRVR SEVERE 21 M 1 1 P W
3 DRVR 21 M W HNBD PROC ST N C 0200 - 2022 - 3 A 21703 - P W DRVR SEVERE 21 M 1 1 P W

Primary Rd SEPULVEDA BL Distance (ft) 23.0 Direction S Secondary Rd 30TH ST NCIC 1944 State Hwy? Y Route 1 Postmile Prefix - Postmile 23.591 Side of Hwy S
City Manhattan Beach County Los Angeles Population 4 Rpt Dist M7 Beat 0M4 Type 0 CalTrans 7 Badge 294 Collision Date 20230712 Time 1358 Day WED
Primary Collision Factor UNSAFE SPEED Violation 22350 Collision Type REAR END Severity INJURY #Killed 0 #Injured 2 Tow Away? Y Process Date 20230802
Weather1 CLEAR Weather2 Rdwy Surface DRY Rdwy Cond1 NO UNUSL CND Rdwy Cond2 Spec Cond 0
Hit and Run Motor Vehicle Involved With OTHER MV Lighting DAYLIGHT Ped Action Cntrl Dev FNCTNG Loc Type I Ramp/Int 5

Party Info Party Type Age Sex Race Sobriety1 Sobriety2 Move Pre Dir SW Veh CHP Veh Make Year SP Info OAF1 Viol OAF2 Safety Equip ROLE Ext Of Inj AGE Sex Seat Pos Safety EQUIP Ejected
1F DRVR 39 M H HNBD PROC ST S D 2200 FORD 2000 - - F - L G DRVR POSSIBL 39 M 1 0 L G
2 DRVR 59 M HNBD STOPPED S D 2200 - 2019 - 3 N - M G DRVR POSSIBL 59 M 1 0 M G

Primary Rd SEPULVEDA BL Distance (ft) 70.0 Direction N Secondary Rd 33RD ST NCIC 1944 State Hwy? N Route Postmile Prefix Postmile Side of Hwy
City Manhattan Beach County Los Angeles Population 4 Rpt Dist SOUTH Beat 004 Type 0 CalTrans Badge 316 Collision Date 20230419 Time 0738 Day WED
Primary Collision Factor UNSAFE SPEED Violation 22350 Collision Type REAR END Severity PDO #Killed 0 #Injured 0 Tow Away? N Process Date 20230523
Weather1 CLEAR Weather2 Rdwy Surface DRY Rdwy Cond1 NO UNUSL CND Rdwy Cond2 Spec Cond 0
Hit and Run Motor Vehicle Involved With OTHER MV Lighting DAYLIGHT Ped Action Cntrl Dev NT PRS/FCTR Loc Type Ramp/Int

Party Info Party Type Age Sex Race Sobriety1 Sobriety2 Move Pre Dir SW Veh CHP Veh Make Year SP Info OAF1 Viol OAF2 Safety Equip ROLE Ext Of Inj AGE Sex Seat Pos Safety EQUIP Ejected
1F DRVR 49 M H HNBD PROC ST N A 0800 - 2006 - 3 N - L G
2 DRVR 67 F W HNBD STOPPED N A 0700 JEEP 2017 - 3 N - M G

3	DRVR	21	F	W	HNBD	STOPPED	W	A	0100	-	2016	-	3	N	-	M	G									
Primary Rd		SEPULVEDA BL		Distance (ft)	0.00	Direction	Secondary Rd		33RD ST		NCIC	1944	State Hwy?	Y	Route	Postmile Prefix	Postmile	Side of Hwy								
City		Manhattan Beach		County	Los Angeles	Population	4	Rpt Dist	M7	Beat	0M2	Type	0	CalTrans	Badge	387	Collision Date	20230818	Time	0151	Day	FRI				
Primary Collision Factor		UNKNOWN		Violation	23154A	Collision Type	BROADSIDE		Severity	PDO	#Killed	0	#Injured	0	Tow Away?	Y	Process Date	20230928								
Weather1		CLEAR		Weather2	Rdwy Surface		DRY		Rdwy Cond1	NO UNUSL CND		Rdwy Cond2	Spec Cond		0											
Hit and Run		Motor Vehicle Involved With		OTHER MV		Lighting	DARK - ST		Ped Action	Cntrl Dev		FNCTNG		Loc Type	Ramp/Int											
Party Info																	Victim Info									
Party	Type	Age	Sex	Race	Sobriety1	Sobriety2	Move Pre	Dir	SW Veh	CHP Veh	Make	Year	SP Info	OAF1	Viol	OAF2	Safety Equip	ROLE	Ext Of Inj	AGE	Sex	Seat Pos	Safety	EQUIP	Ejected	
1F	DRVR	28	F	H	HBD-UI		RGT TURN	E	A	0100	-	2015	-	3	A	22107	-	L	G							
2	DRVR	35	M	A	HNBD		PROC ST	S	A	0100	-	2022	-	3	N		-	L	G							

Primary Rd		SEPULVEDA BL		Distance (ft)	0.00	Direction	Secondary Rd		8TH ST		NCIC	1944	State Hwy?	N	Route	Postmile Prefix	Postmile	Side of Hwy								
City		Manhattan Beach		County	Los Angeles	Population	4	Rpt Dist	SOUTH	Beat	002	Type	0	CalTrans	Badge	316	Collision Date	20230411	Time	0853	Day	TUE				
Primary Collision Factor		IMPROP TURN		Violation	22107	Collision Type	HIT OBJECT		Severity	PDO	#Killed	0	#Injured	0	Tow Away?	N	Process Date	20230511								
Weather1		CLOUDY		Weather2	Rdwy Surface		DRY		Rdwy Cond1	NO UNUSL CND		Rdwy Cond2	Spec Cond		0											
Hit and Run		Motor Vehicle Involved With		FIXED OBJ		Lighting	DAYLIGHT		Ped Action	Cntrl Dev		FNCTNG		Loc Type	Ramp/Int											
Party Info																	Victim Info									
Party	Type	Age	Sex	Race	Sobriety1	Sobriety2	Move Pre	Dir	SW Veh	CHP Veh	Make	Year	SP Info	OAF1	Viol	OAF2	Safety Equip	ROLE	Ext Of Inj	AGE	Sex	Seat Pos	Safety	EQUIP	Ejected	
1F	DRVR	24	M	B	HNBD		RGT TURN	S	I	1100	-	2020	-	3	N		-	M	G							

Primary Rd		SEPULVEDA BL		Distance (ft)	21.0	Direction	S	Secondary Rd		9TH ST		NCIC	1944	State Hwy?	Y	Route	1	Postmile Prefix	-	Postmile	22.711	Side of Hwy	S			
City		Manhattan Beach		County	Los Angeles	Population	4	Rpt Dist	M22	Beat	002	Type	0	CalTrans	7	Badge	380	Collision Date	20230121	Time	1339	Day	SAT			
Primary Collision Factor		R-O-W AUTO		Violation	21801A	Collision Type	BROADSIDE		Severity	INJURY	#Killed	0	#Injured	2	Tow Away?	Y	Process Date	20230315								
Weather1		CLOUDY		Weather2	Rdwy Surface		DRY		Rdwy Cond1	NO UNUSL CND		Rdwy Cond2	Spec Cond		0											
Hit and Run		Motor Vehicle Involved With		OTHER MV		Lighting	DAYLIGHT		Ped Action	Cntrl Dev		NT PRS/FCTR		Loc Type	I	Ramp/Int 5										
Party Info																	Victim Info									
Party	Type	Age	Sex	Race	Sobriety1	Sobriety2	Move Pre	Dir	SW Veh	CHP Veh	Make	Year	SP Info	OAF1	Viol	OAF2	Safety Equip	ROLE	Ext Of Inj	AGE	Sex	Seat Pos	Safety	EQUIP	Ejected	
1F	DRVR	26	F	W	HNBD		LFT TURN	-	A	0700	FORD	2013	-	3	E	-	M	G	PASS	COMP PN	61	F	3	0	M	G
2	DRVR	19	M	H	HNBD		PROC ST	-	A	0100	FORD	2014	-	3	N	-	L	G	DRVR	COMP PN	19	M	1	0	L	G

Primary Rd		SEPULVEDA BL		Distance (ft)	0.00	Direction	Secondary Rd		ARTESIA BL		NCIC	1944	State Hwy?	Y	Route	1	Postmile Prefix	-	Postmile	21.919	Side of Hwy	N				
City		Manhattan Beach		County	Los Angeles	Population	4	Rpt Dist	SOUTH	Beat	001	Type	0	CalTrans	7	Badge	316	Collision Date	20230206	Time	0911	Day	MON			
Primary Collision Factor		STOP SGN SIG		Violation	21453A	Collision Type	BROADSIDE		Severity	INJURY	#Killed	0	#Injured	1	Tow Away?		Process Date	20230323								
Weather1		CLEAR		Weather2	Rdwy Surface		DRY		Rdwy Cond1	NO UNUSL CND		Rdwy Cond2	Spec Cond		0											
Hit and Run		Motor Vehicle Involved With		OTHER MV		Lighting	DAYLIGHT		Ped Action	Cntrl Dev		FNCTNG		Loc Type	I	Ramp/Int 5										
Party Info																	Victim Info									
Party	Type	Age	Sex	Race	Sobriety1	Sobriety2	Move Pre	Dir	SW Veh	CHP Veh	Make	Year	SP Info	OAF1	Viol	OAF2	Safety Equip	ROLE	Ext Of Inj	AGE	Sex	Seat Pos	Safety	EQUIP	Ejected	
1F	DRVR	74	M	W	HNBD		PROC ST	W	A	0100	-	2018	-	3	N	-	M	G	DRVR	OTH VIS	74	M	1	0	M	G
2	DRVR	46	F	W	HNBD		PROC ST	N	A	0100	-	2004	-	3	N	-	M	G								

Primary Rd		SEPULVEDA BL		Distance (ft)	0.00	Direction	Secondary Rd		KEATS ST		NCIC	1944	State Hwy?	Y	Route	1	Postmile Prefix	-	Postmile	22.091	Side of Hwy	N				
City		Manhattan Beach		County	Los Angeles	Population	4	Rpt Dist	M26	Beat	0M1	Type	0	CalTrans	7	Badge	383	Collision Date	20230523	Time	0820	Day	TUE			
Primary Collision Factor		R-O-W AUTO		Violation	21801A	Collision Type	BROADSIDE		Severity	INJURY	#Killed	0	#Injured	4	Tow Away?	Y	Process Date	20230705								
Weather1		CLEAR		Weather2	Rdwy Surface		DRY		Rdwy Cond1	NO UNUSL CND		Rdwy Cond2	Spec Cond		0											
Hit and Run		Motor Vehicle Involved With		OTHER MV		Lighting	DAYLIGHT		Ped Action	Cntrl Dev		FNCTNG		Loc Type	I	Ramp/Int 5										
Party Info																	Victim Info									
Party	Type	Age	Sex	Race	Sobriety1	Sobriety2	Move Pre	Dir	SW Veh	CHP Veh	Make	Year	SP Info	OAF1	Viol	OAF2	Safety Equip	ROLE	Ext Of Inj	AGE	Sex	Seat Pos	Safety	EQUIP	Ejected	
1F	DRVR	51	F	O	HNBD		LFT TURN	S	A	0700	-	2020	-	3	E	-	L	G	PASS		16	F	9	0	L	G
																			PASS	COMP PN	16	M	3	0	L	G
																			PASS		16	F	4	0	L	G
																			PASS		16	F	5	0	M	G
																			PASS	OTH VIS	16	F	6	0	L	G

Include State Highways cases

Report Run On: 10/04/2023

														PASS	OTH VIS	11	F	7	0	L	G							
2	DRVR	24	M	W	HNBD	PROC ST	N	A	0100	-	2018	-	3	A	22350	-	L	G										
														DRVR	OTH VIS	24	M	1	0	L	G							
Primary Rd SEPULVEDA BL Distance (ft) 11.0 Direction N Secondary Rd MANHATTAN NCIC 1944 State Hwy? Y Route 1 Postmile Prefix - Postmile 22.900 Side of Hwy S																												
City Manhattan Beach County Los Angeles Population 4 Rpt Dist M15 Beat 0M4 Type 0 CalTrans 7 Badge 329 Collision Date 20230609 Time 0839 Day FRI																												
Primary Collision Factor IMPROP TURN Violation 22107 Collision Type HIT OBJECT Severity PDO #Killed 0 #Injured 0 Tow Away? N Process Date 20230808																												
Weather1 CLEAR Weather2 Rdwy Surface DRY Rdwy Cond1 NO UNUSL CND Rdwy Cond2 Spec Cond 0																												
Hit and Run MSDMNR Motor Vehicle Involved With FIXED OBJ Lighting DAYLIGHT Ped Action Cntrl Dev FNCTNG Loc Type I Ramp/Int 5																												
Party Info														Victim Info														
Party	Type	Age	Sex	Race	Sobriety1	Sobriety2	Move Pre	Dir	SW Veh	CHP Veh	Make	Year	SP Info	OAF1	Viol	OAF2	Safety Equip	ROLE	Ext Of Inj	AGE	Sex	Seat Pos	Safety	EQUIP	Ejected			
1F	DRVR	63	M	A	HNBD		RGT TURN	W	F		2500	-	2023	-	3	N	-	M	G									
Primary Rd SEPULVEDA BL Distance (ft) 30.0 Direction S Secondary Rd MARINE AV NCIC 1944 State Hwy? Y Route 1 Postmile Prefix - Postmile 23.400 Side of Hwy N																												
City Manhattan Beach County Los Angeles Population 4 Rpt Dist M7 Beat 0M4 Type 0 CalTrans 7 Badge 361 Collision Date 20230308 Time 1918 Day WED																												
Primary Collision Factor DRVR ALC DRG Violation 23152G Collision Type REAR END Severity PDO #Killed 0 #Injured 0 Tow Away? Y Process Date 20230420																												
Weather1 CLEAR Weather2 Rdwy Surface DRY Rdwy Cond1 NO UNUSL CND Rdwy Cond2 Spec Cond 0																												
Hit and Run Motor Vehicle Involved With OTHER MV Lighting DARK - ST Ped Action Cntrl Dev FNCTNG Loc Type H Ramp/Int -																												
Party Info														Victim Info														
Party	Type	Age	Sex	Race	Sobriety1	Sobriety2	Move Pre	Dir	SW Veh	CHP Veh	Make	Year	SP Info	OAF1	Viol	OAF2	Safety Equip	ROLE	Ext Of Inj	AGE	Sex	Seat Pos	Safety	EQUIP	Ejected			
1F	DRVR	61	M	W	HBD-UI		PROC ST	N	D		2200	-	2001	-	-	A	22350	-	M	B								
2	DRVR	46	F	W	HNBD		STOPPED	N	A		0800	-	2013	-	-	N	-	M	B									
Primary Rd SEPULVEDA BL Distance (ft) 0.00 Direction Secondary Rd MARINE AV NCIC 1944 State Hwy? Y Route 1 Postmile Prefix - Postmile 23.421 Side of Hwy N																												
City Manhattan Beach County Los Angeles Population 4 Rpt Dist M7 Beat 0M4 Type 0 CalTrans 7 Badge 390 Collision Date 20230425 Time 1909 Day TUE																												
Primary Collision Factor DRVR ALC DRG Violation 23152A Collision Type SIDESWIPE Severity INJURY #Killed 0 #Injured 2 Tow Away? Y Process Date 20230613																												
Weather1 CLEAR Weather2 Rdwy Surface DRY Rdwy Cond1 NO UNUSL CND Rdwy Cond2 Spec Cond 0																												
Hit and Run Motor Vehicle Involved With OTHER MV Lighting DAYLIGHT Ped Action Cntrl Dev FNCTNG Loc Type I Ramp/Int 5																												
Party Info														Victim Info														
Party	Type	Age	Sex	Race	Sobriety1	Sobriety2	Move Pre	Dir	SW Veh	CHP Veh	Make	Year	SP Info	OAF1	Viol	OAF2	Safety Equip	ROLE	Ext Of Inj	AGE	Sex	Seat Pos	Safety	EQUIP	Ejected			
1F	DRVR	23	M	H	HBD-UI		WRONG WY	N	A		0100	-	2015	-	3	A	F	L	G									
2	DRVR	39	M	O	HNBD		STOPPED	S	A		0100	-	2010	-	3	-	-	M	G	DRVR	MINOR	40	M	1	0	M	G	
3	DRVR	41	F	B	HNBD		STOPPED	S	A		0700	-	2022	-	3	-	-	L	G	DRVR	MINOR	41	F	1	0	M	G	
4	DRVR	29	M	W	HNBD		STOPPED	S	A		0100	-	2018	-	3	N	-	M	G									
5	DRVR	32	M	A	HNBD		STOPPED	S	A		0100	-	2015	-	3	N	-	M	G									
Primary Rd SEPULVEDA BL Distance (ft) 0.00 Direction Secondary Rd MARINE AV NCIC 1944 State Hwy? Y Route 1 Postmile Prefix - Postmile 23.470 Side of Hwy S																												
City Manhattan Beach County Los Angeles Population 4 Rpt Dist M7 Beat 0M4 Type 0 CalTrans 7 Badge 294 Collision Date 20230619 Time 1619 Day MON																												
Primary Collision Factor UNSAFE SPEED Violation 22350 Collision Type REAR END Severity INJURY #Killed 0 #Injured 2 Tow Away? N Process Date 20230817																												
Weather1 CLEAR Weather2 Rdwy Surface Rdwy Cond1 NO UNUSL CND Rdwy Cond2 Spec Cond 0																												
Hit and Run FELONY Motor Vehicle Involved With OTHER MV Lighting DAYLIGHT Ped Action Cntrl Dev FNCTNG Loc Type H Ramp/Int -																												
Party Info														Victim Info														
Party	Type	Age	Sex	Race	Sobriety1	Sobriety2	Move Pre	Dir	SW Veh	CHP Veh	Make	Year	SP Info	OAF1	Viol	OAF2	Safety Equip	ROLE	Ext Of Inj	AGE	Sex	Seat Pos	Safety	EQUIP	Ejected			
1F	DRVR	998	M	W	IMP UNK	IMP UNK	PROC ST	S	A		0800	-	2006	-	-	N	-	-	-									
2	DRVR	20	M	H	HNBD		STOPPED	S	A		0100	-	2003	-	3	N	A	-	M	G	DRVR	POSSIBL	20	M	1	0	M	G
																				PASS	POSSIBL	24	M	3	0	M	G	

Include State Highways cases

Report Run On: 10/04/2023

Primary Rd		SEPULVEDA BL		Distance (ft)	0.00	Direction		Secondary Rd		ROSECRANS AV		NCIC	1944	State Hwy?	Y	Route	1	Postmile Prefix	-	Postmile	23.924	Side of Hwy	S				
City		Manhattan Beach		County	Los Angeles	Population	4	Rpt Dist	M7	Beat	0M4	Type	0	CalTrans	7	Badge	361	Collision Date	20230628	Time	2153	Day	WED				
Primary Collision Factor		PED VIOL		Violation	21954A	Collision Type	AUTO/PED	Severity	INJURY	#Killed	0	#Injured	1	Tow Away?	N	Process Date	20230802										
Weather1		CLEAR		Weather2		Rdwy Surface	DRY	Rdwy Cond1	NO UNUSL CND	Rdwy Cond2		Spec Cond	0														
Hit and Run				Motor Vehicle Involved With		PED		Lighting	DARK - ST	Ped Action	NOT IN X-	Cntrl Dev	FUNCTNG	Loc Type	I	Ramp/Int	5										
Party Info													Victim Info														
Party	Type	Age	Sex	Race	Sobriety1	Sobriety2	Move Pre	Dir	SW Veh	CHP Veh	Make	Year	SP Info	OAF1	Viol	OAF2	Safety Equip	ROLE	Ext Of Inj	AGE	Sex	Seat Pos	Safety	EQUIP	Ejected		
1F	PED	48	M			DRUG	OTHER	W	N	6000	-	-	-	N	-	-	-	PED	MINOR	48	M	-	-	P	-		
2	DRVR	53	M	W	HNBD		BACKING	N	A	0700	-	2021	-	3	N	-	M	G									
Primary Rd		SEPULVEDA BL		Distance (ft)	227.	Direction	S	Secondary Rd		ROSECRANS AV		NCIC	1944	State Hwy?	Y	Route		Postmile Prefix		Postmile		Side of Hwy					
City		Manhattan Beach		County	Los Angeles	Population	4	Rpt Dist	MS	Beat	0M3	Type	0	CalTrans		Badge	391	Collision Date	20230727	Time	0004	Day	THU				
Primary Collision Factor		DRVR ALC DRG		Violation	23152A	Collision Type	HIT OBJECT	Severity	PDO	#Killed	0	#Injured	0	Tow Away?	Y	Process Date	20231002										
Weather1		CLEAR		Weather2		Rdwy Surface	DRY	Rdwy Cond1	NO UNUSL CND	Rdwy Cond2		Spec Cond	0														
Hit and Run				Motor Vehicle Involved With		FIXED OBJ		Lighting	DARK - ST	Ped Action		Cntrl Dev	FUNCTNG	Loc Type		Ramp/Int											
Party Info													Victim Info														
Party	Type	Age	Sex	Race	Sobriety1	Sobriety2	Move Pre	Dir	SW Veh	CHP Veh	Make	Year	SP Info	OAF1	Viol	OAF2	Safety Equip	ROLE	Ext Of Inj	AGE	Sex	Seat Pos	Safety	EQUIP	Ejected		
1F	DRVR	70	M	B	HBD-UI		PROC ST	S	A	0700	FORD	2017	-	3	A	22107	-	L	-	PASS		48	M	3	0	L	G
Primary Rd		SEPULVEDA BL		Distance (ft)	66.0	Direction	N	Secondary Rd		VALLEY DR		NCIC	1944	State Hwy?	Y	Route	1	Postmile Prefix	-	Postmile	23.840	Side of Hwy	S				
City		Manhattan Beach		County	Los Angeles	Population	4	Rpt Dist	M7	Beat	0M4	Type	0	CalTrans	7	Badge	386	Collision Date	20230526	Time	2236	Day	FRI				
Primary Collision Factor		DRVR ALC DRG		Violation	23152A	Collision Type	SIDESWIPE	Severity	PDO	#Killed	0	#Injured	0	Tow Away?	N	Process Date	20230714										
Weather1		CLEAR		Weather2		Rdwy Surface	DRY	Rdwy Cond1	NO UNUSL CND	Rdwy Cond2		Spec Cond	0														
Hit and Run		MSDMNR		Motor Vehicle Involved With		PKD MV		Lighting	DARK - ST	Ped Action		Cntrl Dev	NT PRS/FCTR	Loc Type	H	Ramp/Int	-										
Party Info													Victim Info														
Party	Type	Age	Sex	Race	Sobriety1	Sobriety2	Move Pre	Dir	SW Veh	CHP Veh	Make	Year	SP Info	OAF1	Viol	OAF2	Safety Equip	ROLE	Ext Of Inj	AGE	Sex	Seat Pos	Safety	EQUIP	Ejected		
1F	DRVR	63	M	A	HBD-UI		CHANG LN	S	A	0700	FORD	2015	-	3	A	22107	-	M	G								
2	PRKD	998	-				PARKED	S	J	4800	FORD	2017	-	3	N	-	-	-									
Primary Rd		VALLEY DR		Distance (ft)	17.0	Direction	N	Secondary Rd		13TH ST		NCIC	1944	State Hwy?	N	Route		Postmile Prefix		Postmile		Side of Hwy					
City		Manhattan Beach		County	Los Angeles	Population	4	Rpt Dist	M12	Beat	0M3	Type	0	CalTrans		Badge	389	Collision Date	20230303	Time	2100	Day	FRI				
Primary Collision Factor		STOP SGN SIG		Violation	22450A	Collision Type	HIT OBJECT	Severity	PDO	#Killed	0	#Injured	0	Tow Away?	N	Process Date	20230501										
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Hit and Run				Motor Vehicle Involved With		FIXED OBJ		Lighting	DARK - ST	Ped Action		Cntrl Dev	FUNCTNG	Loc Type		Ramp/Int											
Party Info													Victim Info														
Party	Type	Age	Sex	Race	Sobriety1	Sobriety2	Move Pre	Dir	SW Veh	CHP Veh	Make	Year	SP Info	OAF1	Viol	OAF2	Safety Equip	ROLE	Ext Of Inj	AGE	Sex	Seat Pos	Safety	EQUIP	Ejected		
1F	DRVR	40	M	W	HBD-NUI		PROC ST	E	A	0700	-	2017	-	1	A	23123	F	L	G								
Primary Rd		VALLEY DR		Distance (ft)	47.0	Direction	N	Secondary Rd		2ND ST		NCIC	1944	State Hwy?	N	Route		Postmile Prefix		Postmile		Side of Hwy					
City		Manhattan Beach		County	Los Angeles	Population	4	Rpt Dist	M21	Beat	0M2	Type	0	CalTrans		Badge	294	Collision Date	20230606	Time	1559	Day	TUE				
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Weather1		CLEAR		Weather2		Rdwy Surface	DRY	Rdwy Cond1	NO UNUSL CND	Rdwy Cond2		Spec Cond	0														
Hit and Run				Motor Vehicle Involved With		OTHER MV		Lighting	DAYLIGHT	Ped Action		Cntrl Dev	NT PRS/FCTR	Loc Type		Ramp/Int											
Party Info													Victim Info														
Party	Type	Age	Sex	Race	Sobriety1	Sobriety2	Move Pre	Dir	SW Veh	CHP Veh	Make	Year	SP Info	OAF1	Viol	OAF2	Safety Equip	ROLE	Ext Of Inj	AGE	Sex	Seat Pos	Safety	EQUIP	Ejected		
1F	DRVR	78	M	W	HNBD		PROC ST	S	A	0100	-	2015	-	3	N	-	M	G									
2	DRVR	45	M	H	HNBD		PROC ST	S	D	7200	-	1987	-	3	N	-	M	G									
3	PRKD	998	-				PARKED	S	A	0100	-	2018	-	3	N	-	-	-									

Primary Rd VALLEY DR Distance (ft) 0.00 Direction Secondary Rd 3RD ST NCIC 1944 State Hwy? N Route Postmile Prefix Postmile Side of Hwy City Manhattan Beach County Los Angeles Population 4 Rpt Dist M19 Beat 0M2 Type 0 CalTrans Badge 294 Collision Date 20230416 Time 1516 Day SUN Primary Collision Factor R-O-W AUTO Violation 21802A Collision Type BROADSIDE Severity INJURY #Killed 0 #Injured 1 Tow Away? Y Process Date 20230517 Weather1 CLEAR Weather2 Rdwy Surface DRY Rdwy Cond1 NO UNUSL CND Rdwy Cond2 Spec Cond 0 Hit and Run Motor Vehicle Involved With OTHER MV Lighting DAYLIGHT Ped Action Cntrl Dev FNCTNG Loc Type Ramp/Int														
Party Info Victim Info Party Type Age Sex Race Sobriety1 Sobriety2 Move Pre Dir SW Veh CHP Veh Make Year SP Info OAF1 Viol OAF2 Safety Equip ROLE Ext Of Inj AGE Sex Seat Pos Safety EQUIP Ejected 1F DRVR 18 F W HNBD RGT TURN E A 0100 - 2023 - 3 N - - M G DRVR POSSIBL 22 F 1 0 M G 2 DRVR 22 F H HNBD PROC ST S A 0100 - 2018 - - F - - M G 3 PRKD 998 - PARKED S A 0100 - 2018 - 3 N - - -														
Primary Rd VALLEY DR Distance (ft) 0.00 Direction Secondary Rd LONGFELLOW AV NCIC 1944 State Hwy? N Route Postmile Prefix Postmile Side of Hwy City Manhattan Beach County Los Angeles Population 4 Rpt Dist B102 Beat HB2 Type 0 CalTrans Badge 389 Collision Date 20230605 Time 0501 Day MON Primary Collision Factor NOT DRIVER Violation Collision Type HIT OBJECT Severity INJURY #Killed 0 #Injured 1 Tow Away? Y Process Date 20230705 Weather1 CLEAR Weather2 Rdwy Surface DRY Rdwy Cond1 NO UNUSL CND Rdwy Cond2 Spec Cond 0 Hit and Run Motor Vehicle Involved With FIXED OBJ Lighting DUSK/DAWN Ped Action Cntrl Dev NT PRS/FCTR Loc Type Ramp/Int														
Party Info Victim Info Party Type Age Sex Race Sobriety1 Sobriety2 Move Pre Dir SW Veh CHP Veh Make Year SP Info OAF1 Viol OAF2 Safety Equip ROLE Ext Of Inj AGE Sex Seat Pos Safety EQUIP Ejected 1 DRVR 84 M W IMP UNK IMP UNK PROC ST E A 0100 - 2017 - - N - - M B DRVR SERIOUS 84 M 1 0 M B														
Primary Rd VALLEY DR Distance (ft) 99.0 Direction S Secondary Rd MANHATTAN NCIC 1944 State Hwy? N Route Postmile Prefix Postmile Side of Hwy City Manhattan Beach County Los Angeles Population 4 Rpt Dist M19 Beat 0M2 Type 0 CalTrans Badge 386 Collision Date 20230630 Time 2017 Day FRI Primary Collision Factor PED VIOL Violation 21954A Collision Type AUTO/PED Severity INJURY #Killed 0 #Injured 1 Tow Away? N Process Date 20230728 Weather1 CLEAR Weather2 Rdwy Surface DRY Rdwy Cond1 NO UNUSL CND Rdwy Cond2 Spec Cond 0 Hit and Run Motor Vehicle Involved With PED Lighting DARK - ST Ped Action IN RD, Cntrl Dev NT PRS/FCTR Loc Type Ramp/Int														
Party Info Victim Info Party Type Age Sex Race Sobriety1 Sobriety2 Move Pre Dir SW Veh CHP Veh Make Year SP Info OAF1 Viol OAF2 Safety Equip ROLE Ext Of Inj AGE Sex Seat Pos Safety EQUIP Ejected 1F PED 13 M W HNBD ENT TRAF S N 6000 - - 3 N - - - PED MINOR 13 M - - - V 2 DRVR 28 M B HNBD PROC ST S A 0100 - 2022 - 3 N - - M G														
Primary Rd VALLEY DR Distance (ft) 0.00 Direction Secondary Rd PACIFIC AV NCIC 1944 State Hwy? N Route Postmile Prefix Postmile Side of Hwy City Manhattan Beach County Los Angeles Population 4 Rpt Dist SOUTH Beat 003 Type 0 CalTrans Badge 316 Collision Date 20230417 Time 1702 Day MON Primary Collision Factor R-O-W PED Violation 21950A Collision Type AUTO/PED Severity INJURY #Killed 0 #Injured 1 Tow Away? N Process Date 20230517 Weather1 CLEAR Weather2 Rdwy Surface DRY Rdwy Cond1 NO UNUSL CND Rdwy Cond2 Spec Cond 0 Hit and Run Motor Vehicle Involved With PED Lighting DAYLIGHT Ped Action X-WLK AT Cntrl Dev FNCTNG Loc Type Ramp/Int														
Party Info Victim Info Party Type Age Sex Race Sobriety1 Sobriety2 Move Pre Dir SW Veh CHP Veh Make Year SP Info OAF1 Viol OAF2 Safety Equip ROLE Ext Of Inj AGE Sex Seat Pos Safety EQUIP Ejected 1F DRVR 35 M H HNBD LFT TURN N A 0100 - 2017 - 3 N - - M G 2 PED 40 M H HNBD PROC ST S N 6000 - - 1 F - - - PED MINOR 40 M - - - P														

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07-LA-Route 1-PM

EA/EFIS: 38260/0722000070

PROJECT SPONSOR AND CONSULTANT: JLL, KPFF Consulting Engineers

May 2023

Design Engineering Evaluation Report (DEER)

For Project Initiation and Approval

On Route 1 (Sepulveda Blvd)

Between 33rd Street

And 203' South of 33rd Street

I have reviewed the right of way information contained in this report and the right of way data sheet attached hereto, completed by Caltrans and/or its consultant KPFF, and find the data to be complete as to form and process:

Edward Francis, Deputy District Director, Right of Way

APPROVAL RECOMMENDED:

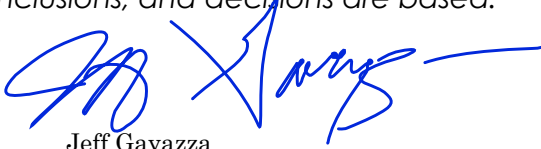
Mehdi Salehinik, Caltrans Project Manager

APPROVED:

Gloria Robert, (Acting) District Director

Date

This design engineering evaluation report has been prepared under the direction of the following registered civil engineer. The registered civil engineer attests to the technical information contained herein and the engineering data upon which recommendations, conclusions, and decisions are based.



Jeff Gavazza

5/10/2023

REGISTERED CIVIL ENGINEER

DATE



1. INTRODUCTION

Project Description:

Construction of new widened shoulder and curb ramp at southeast corner of the intersection of Sepulveda Blvd and 33rd Street, from the intersection to approximately 203 ft south of the centerline of 33rd street.

Project Limits	Route 1 LA 23.719/LA 23.662
Current Project Cost Estimate (Construction and Right-of Way)	\$101,156
Type of Facility	7-lane conventional highway
Anticipated Environmental Determination or Document	Categorically Exempt (CE) / Categorically Exempt (CE)
Legal Description	Deferred
Plans, Specifications, & Estimate Date	September 10, 2021
Ready to List Date (Permit Issue)	N/A Private Developer
Award Date	N/A Private Developer
Estimated Construction Date	TBD

2. BACKGROUND

The Sepulveda Boulevard (SR-1)) bridge seismic retrofit and widening project eliminated the northbound right turn into the Manhattan Village Shopping Center at 33rd Street. The right turn lane was converted to a 4th through lane which continues north to Rosecrans Avenue over the bridge. This right turn lane served as a deceleration area for the right turn with a volume of approximately 00 vehicles per day.

When the Manhattan Village Shopping Center development project was proposed, the City of Manhattan Beach required the applicant to conduct traffic studies to determine if additional traffic improvements would be necessary to maintain existing level of safety at the driveway entrances.

Through the planning entitlement process and extensive public testimony, the Planning Commission on July 24, 2013, and the City Council on December 2, 2014, determined that the replacement of a deceleration area for right turning traffic was necessary to prevent an increase in rear-end collisions and to maintain the same or better levels of traffic safety for pedestrians, bicyclists and motorists. The hearing process was conducted in accordance with as required by Manhattan Beach Municipal Code Chapter 10.84 for Master Use Permits and in accordance with the California Environmental Quality Act (CEQA) Guideline Section 15105

After substantial review of the development project, feedback from the public, the input from City Staff and in compliance with CEQA Guidelines, the Planning Commission certified the EIR and approved the project on July 24, 2013. The City Council re-certified the EIR and adopted a Resolution approving the project with conditions of approval on December 2, 2014. One of these conditions of approval requires the developer to construct a widened shoulder to replace the northbound right turn lane, which was eliminated by the bridge widening project.

3. PURPOSE AND NEED

Purpose:

The purpose of this project is to improve traffic operations and safety at the intersection of Sepulveda Boulevard and 33rd Street, as it existed prior to the Sepulveda Boulevard Bridge Seismic Retrofit and Widening project. The prior northbound right turn lane allowed for slowing vehicles to move out of the through lanes so as not to impede traffic flow and increase the potential for rear-end collisions. The widened shoulder will improve pedestrian and bicyclist safety by allowing drivers to make safer right turns at a slower speed in a deceleration area (shoulder), so drivers can have greater reaction time to anticipate pedestrian crossings and bicyclists crossing the intersection.

One of the main objectives of the project is to reduce the potential for collisions due to merging and slowing traffic caused by northbound right turning traffic in a main line travel lane. As described more fully in the Traffic Maintenance and Operations section, the predominant collision factors are unsafe lane changes and rear-end collisions, both a consequence of motorists avoiding slowing traffic in heavily congested conditions. The widened shoulder will reduce the congested conditions and vehicles unexpectedly slowing in mainline travel lanes, thereby reducing the collision rate.

Another objective of this project is to improve traffic flow along the northbound curb lane by providing a safe refuge area to relocate the existing transit bus stop out of the existing travel lane which is currently located approximately 450 feet south of the project location.

Ultimately, the objective of the widened shoulder is to improve bicyclist safety by dedicating additional state right-of-way for future Class II bike lanes and a full width shoulder on the State Highway. Northbound right turn volumes are expected to increase substantially after the Shopping Center is fully occupied, so the widened area can eventually be converted to a striped right turn pocket, with additional right-of-way acquisition, at the time it is warranted.

Need:

The intersection of Sepulveda Boulevard (SR-1) and 33rd Street is located on a seven-lane state highway with prevailing speed over 40 MPH and extremely heavy peak hour volumes. The removal of the northbound left turn pocket has resulted in a high-speed northbound curb lane without a shoulder or turn pocket for motorists to decelerate and prepare for a right turn at 33rd Street, the main entrance to the Shopping Center. This makes it extremely difficult for motorists to safely conduct a right turn movement while avoiding being rear-ended or colliding with a crossing pedestrians or bicyclists.

The absence of a deceleration area increases traffic congestion and the potential for collisions because right turning vehicles impede traffic flow and cause unanticipated or erratic driver behavior in a high-speed, highly saturated travel lane.

The current conditions at the intersection do not provide a paved shoulder or a deceleration zone as recommended by the Caltrans Highway Design Manual. The southeast corner does not have directional pedestrian curb ramps pursuant to current Caltrans Accessibility guidelines. These conditions would be improved by the proposed project.

While minimum right turn volumes do not presently meet minimum guidelines for a dedicated right turn pocket, the widened shoulder area will act as an auxiliary deceleration area of sufficient width to allow motorists to pass the slowing traffic safely. Since slowing vehicles impede traffic flow and increase the potential for collisions, the replacement of the right turn lane with a widened shoulder will have the effect of improving the volume/capacity ratio at the intersection.

From January 2008 to December 2010, there were 29 total accidents on northbound Sepulveda Blvd. at 33rd St. per the Caltrans's TASAS tables B and C. The addition of the widened shoulder at Sepulveda Blvd. and 33rd St. will allow better distribution of traffic across the through lanes on Sepulveda Blvd. so the intersection operation will improve. On Saturday afternoons, the predominant flow of traffic is northbound so the replacement of a right turn lane would move the 126 northbound right turning vehicles per hour out of the mainline through lane during the peak hour.

4. RIGHT OF WAY

There is an existing storm drain curb inlet catch basin on the east side of Sepulveda. When that portion of the road is widened to create the widened shoulder, and the curb line moves east, the catch basin would need to be reconstructed in the new location and the storm drain lateral extended. Additionally, conduits associated with the street lighting and signal systems will need to be relocated to account for the shifted sidewalk. No additional relocations are anticipated.

The right-of-way currently aligns with the existing back of sidewalk. When the road is widened and the sidewalk moves east, an additional 10 ft right-of-way will need to be dedicated to Caltrans to encompass the full sidewalk area. Temporary Construction Easements (TCEs) are not anticipated to be required, since the project is only proposing work adjacent to the project owner's private property.

5. TRAFFIC, MAINTENANCE AND OPERATIONS IMPACTS

The proposed Project will add a widened shoulder in the northbound direction to Sepulveda Boulevard at the signalized intersection of 33rd Street.

Morning and afternoon peak hour traffic volumes were prepared for and detailed in Manhattan Village Shopping Center: Sepulveda Boulevard & 33rd Street Traffic Volume Review (See Attachment 5). The projected traffic volumes for year 2018 were used as the basis for the traffic Maintenance and Operations Analysis detailed below and were grown by 1% per year to simulate forecasted traffic conditions for year 2035.

Collision analyses along Sepulveda Boulevard within the Project limits were prepared as part of the Sepulveda Widening Project and detailed in Supplemental Fact Sheet Exceptions to Advisory Design Standards and Supplemental Fact Sheet Exceptions to Mandatory Design Standards (See Attachments 6 and 7). The Supplemental Fact Sheets summarized that the data showed "accidents are indicative of accidents resulting from lower speeds due to heavily congested conditions during peak commute hours. As the level of service of Sepulveda deteriorates, the accident rate would be expected to increase." As detailed in the following traffic Maintenance and Operations Analysis, the Project would not worsen the level of service (LOS) operations at the intersection of Sepulveda Boulevard and 33rd Street. Furthermore, the Project would result in a decrease in intersection delay. Thus, it is anticipated that the Project will improve operations along Sepulveda Boulevard and result in potential reductions to the congestion related accident rate. The Project is not anticipated to increase the number of accidents.

The Highway Capacity Manual, 6th Edition (Transportation Research Board, 2016) (HCM) methodology, which was implemented using Synchro software, was used to calculate the intersection delay and corresponding LOS operation at the signalized intersection. The HCM signalized methodology calculates the average delay, in seconds, for each vehicle passing through the intersection. The LOS categories range from excellent, nearly free-flow traffic (i.e. LOS A) to congested, stop-and-go conditions (i.e. LOS F). An LOS analysis summary is provided for year 2018 and year 2035 conditions. As detailed below, the Project would not worsen the LOS operations at the intersection of Sepulveda Boulevard and 33rd Street in both year 2018 and year 2035 conditions. Furthermore, the Project would result in a decrease in intersection delay. Thus, it is anticipated that the Project will improve operations along Sepulveda Boulevard.

TABLE A
INTERSECTION LEVELS OF SERVICE OPERATIONS

Intersection	Peak Hour	Without Project Conditions		With Project Conditions		
		Delay	LOS	Delay	LOS	Δ in Delay
Year 2018 Conditions						
Sepulveda Boulevard & 33rd Street	AM	40.7	D	40.3	D	-0.4
	PM	37.2	D	35.3	D	-1.9
Year 2035 Conditions						
Sepulveda Boulevard & 33rd Street	AM	84.2	F	83.6	F	-0.6
	PM	43.5	D	39.7	D	-3.8

Notes:

Delay is measured in seconds per vehicle

LOS = Level of service

6. PROPOSED SCOPE OF WORK

The proposed scope of work will include adding a widened shoulder in the northbound direction of Sepulveda Boulevard at the signalized intersection at 33rd Street.

A Design Standard Decision Document will be prepared if the project is approved in concept.

7. STRUCTURES INFORMATION

No structures within the Caltrans right-of-way are proposed. Retaining wall is shown on plans for reference only but will be constructed on private property.

8. OTHER CONSIDERATIONS AS APPROPRIATE

Not applicable

9. PROJECT PERSONNEL

Jeffrey Gavazza, Civil Engineer, 213.266.5244, jeff.gavazza@kpff.com

Kristen Sharer, Civil Project Manager, 213.266.5288, kristen.sharer@kpff.com

Sarah Drobis, Traffic Consultant, 213.683.0088, sdrobis@gibsontrans.com

Mike Johnstone, Traffic Engineer, 213.261.3067, M.Johnstone@fehrrandpeers.com

Mark Cermak, Owner's Representative, 937.620,6977, Mark.Cermak@jll.com

Nick Zaharov, Owner, 415.547.9098, Nicholas.Zaharov@dws.com

10. ATTACHMENTS (78 Pages)

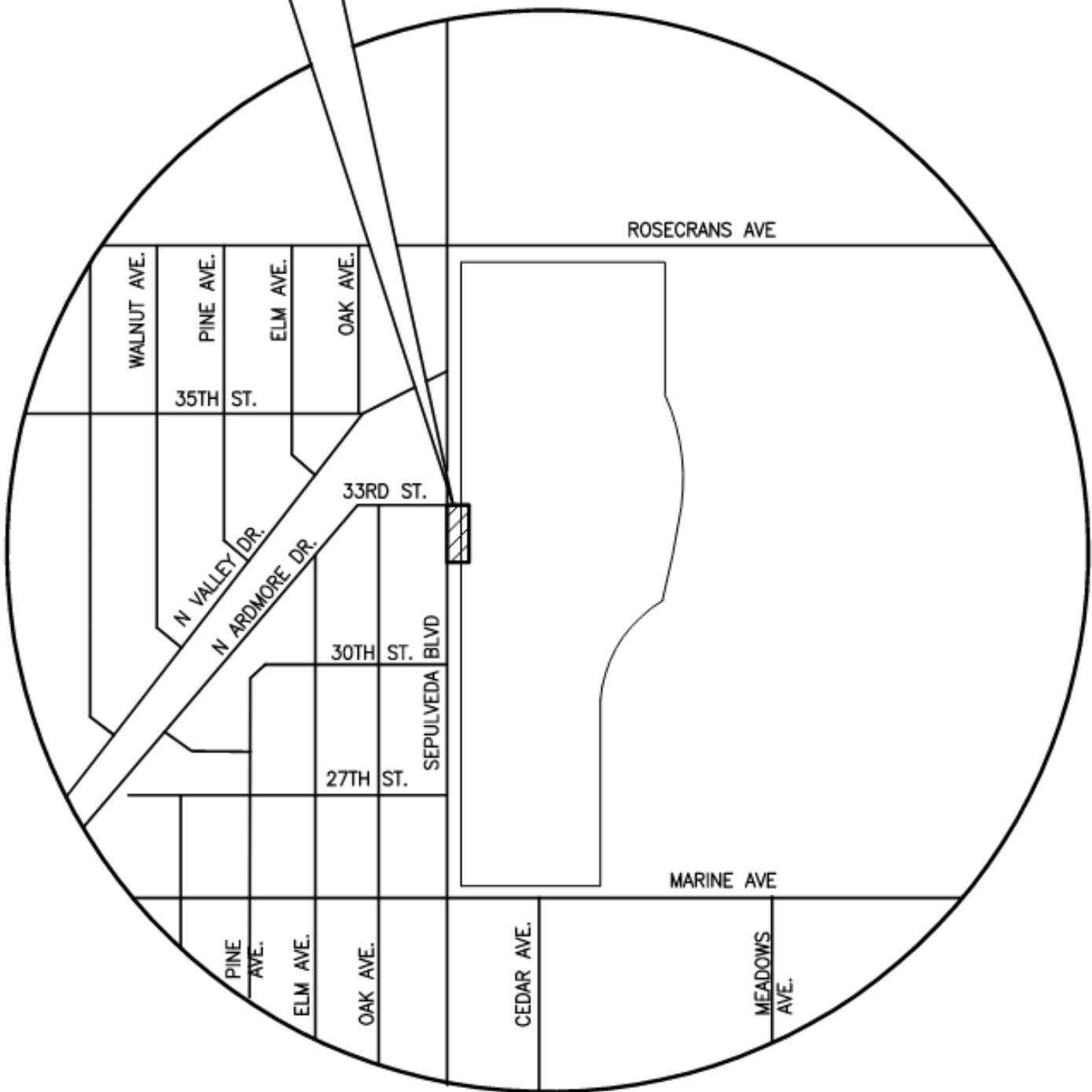
ATTACHMENT 1: VICINITY AND LOCATION MAP (2)

ATTACHMENT 2: PROJECT DRAWINGS (8)

ATTACHMENT 3: ENVIRONMENTAL DOC (68)

Attachment 1
Vicinity and Location Map

PROJECT SITE
MANHATTAN VILLAGE SHOPPING CENTER
3300 N SEPULVEDA BLVD.,
MANHATTAN BEACH, CA 90266



VICINITY MAP

NOT TO SCALE
THOMAS GUIDE LOS ANGELES EDITION
PAGE: 732 GRID: H4

Attachment 2
Plan Set

GENERAL NOTES

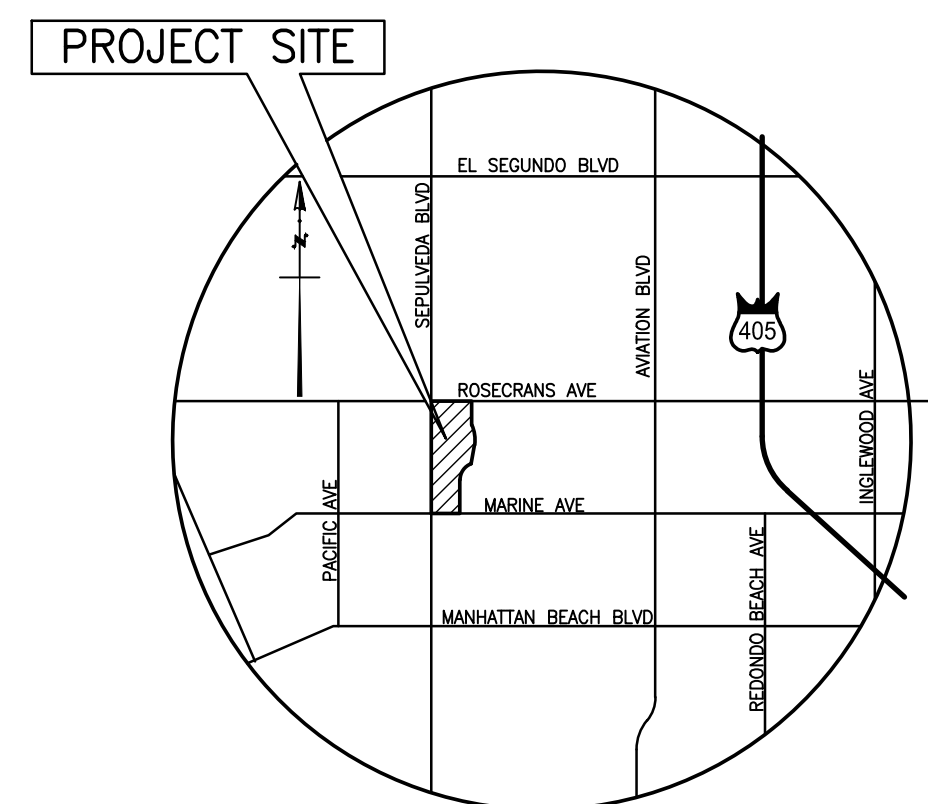
- WORK SHOWN HEREON SHALL BE DONE IN ACCORDANCE WITH THE "STANDARD SPECIFICATIONS FOR PUBLIC WORKS CONSTRUCTION," LATEST EDITION AND SUPPLEMENTS, THE CITY OF MANHATTAN BEACH STANDARDS AND THIS PLAN.
- ALL WORK WITHIN STATE R/W SHALL BE PER LATEST MUTCD, STANDARD PLANS AND SPECIFICATIONS. ALL GRADING WORK SHALL BE PERFORMED IN ACCORDANCE WITH THE REQUIREMENTS AND RECOMMENDATIONS CONTAINED IN THE GEOTECHNICAL REPORT, "PRELIMINARY GEOTECHNICAL REPORT PROPOSED MANHATTAN VILLAGE SHOPPING CENTER EXPANSION AND RENOVATION 3200-3600 SEPULVEDA BOULEVARD, MANHATTAN BEACH, CALIFORNIA" BY GEOTECHNOLOGIES, INC., DATED OCTOBER 1, 2015.
- EXISTING TOPOGRAPHY SHOWN HEREON WAS TAKEN FROM A SURVEY BY HAHN AND ASSOCIATES, INC., DATED APRIL 11, 2016.
- THE CONTRACTOR SHALL ASSUME SOLE AND COMPLETE RESPONSIBILITY FOR THE JOB SITE CONDITIONS, INCLUDING SAFETY OF ALL PERSONS AND PROPERTY, DURING THE COURSE OF CONSTRUCTION OF THIS PROJECT. THIS REQUIREMENT SHALL APPLY CONTINUOUSLY, AND SHALL NOT BE LIMITED TO NORMAL WORKING HOURS.
- PRIOR TO COMMENCING CONSTRUCTION, THE CONTRACTOR SHALL VERIFY ALL JOIN CONDITIONS FOR GRADING, DRAINAGE AND UNDERGROUND FACILITIES INCLUDING LOCATION AND ELEVATION OF EXISTING UNDERGROUND FACILITIES AT CROSSINGS WITH PROPOSED UNDERGROUND FACILITIES. IF CONDITIONS DIFFER FROM THOSE SHOWN ON THE PLANS, THE CONTRACTOR SHALL NOTIFY THE ENGINEER AND SHALL NOT BEGIN CONSTRUCTION UNTIL THE CHANGED CONDITIONS HAVE BEEN EVALUATED.
- ALL DRAWINGS ARE CONSIDERED TO BE A PART OF THE CONTRACT DOCUMENTS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE REVIEW AND COORDINATION OF ALL DRAWINGS AND SPECIFICATIONS PRIOR TO THE START OF CONSTRUCTION. ANY DISCREPANCIES THAT OCCUR SHALL BE BROUGHT TO THE ATTENTION OF THE ARCHITECT PRIOR TO THE START OF CONSTRUCTION SO THAT A CLARIFICATION CAN BE ISSUED. ANY WORK PERFORMED IN CONFLICT WITH THE CONTRACT DOCUMENTS OR ANY CODE REQUIREMENTS SHALL BE CORRECTED BY THE CONTRACTOR AT THEIR OWN EXPENSE AND AT NO EXPENSE TO THE OWNER OR ARCHITECT.
- THE ENGINEER PREPARING THESE PLANS WILL NOT BE RESPONSIBLE FOR, OR LIABLE FOR, UNAUTHORIZED CHANGES TO OR USES OF THESE PLANS. ALL CHANGES TO THE PLANS MUST BE IN WRITING AND MUST BE APPROVED BY THE PREPARER OF THESE PLANS.
- THE EXISTENCE, LOCATION AND CHARACTERISTICS OF UNDERGROUND UTILITY INFORMATION SHOWN ON THESE PLANS HAVE BEEN OBTAINED FROM A REVIEW OF AVAILABLE RECORD DATA. NO REPRESENTATION IS MADE AS TO THE ACCURACY OR COMPLETENESS OF SAID UTILITY INFORMATION. THE CONTRACTOR SHALL TAKE PRECAUTIONARY MEASURES TO PROTECT THE UTILITY LINES SHOWN AND ANY OTHER LINES NOT OF RECORD OR NOT SHOWN ON THESE PLANS.
- IF AT ANY TIME DURING GRADING OPERATIONS, ANY UNFAVORABLE GEOLOGICAL CONDITIONS ARE ENCOUNTERED, GRADING IN THAT AREA WILL STOP UNTIL APPROVED CORRECTIVE MEASURES ARE OBTAINED.
- THE PROPOSED GRADE IS THE FINAL GRADE AND NOT THE ROUGH GRADE. THE CONTRACTOR SHALL SUBTRACT THE THICKNESS OF THE PAVED SECTION AND/OR LANDSCAPE TOPSOIL SECTION TO ARRIVE AT THE ROUGH GRADE ELEVATION.
- STRAIGHT GRADES SHALL BE MAINTAINED BETWEEN SPOT ELEVATIONS UNLESS OTHERWISE SHOWN ON THE PLANS.
- ALL DEBRIS AND FOREIGN MATERIAL SHALL BE REMOVED FROM THE SITE AND DISPOSED OF AT APPROVED DISPOSAL SITES. THE CONTRACTOR SHALL OBTAIN NECESSARY PERMITS FOR THE TRANSPORTATION OF MATERIAL TO AND FROM THE SITE.
- ALL FILL SOILS OR SOILS DISTURBED OR OVER-EXCAVATED DURING CONSTRUCTION SHALL BE COMPACTED PER THE REQUIREMENTS OF THE SOILS REPORT BUT NOT LESS THAN 90% MAXIMUM DENSITY AS DETERMINED BY A.S.T.M. SOIL COMPACTION TEST D-1557.
- THE CONTRACTOR SHALL OBTAIN AN O.S.H.A. PERMIT FROM THE CALIFORNIA DIVISION OF INDUSTRIAL SAFETY PRIOR TO THE CONSTRUCTION OF TRENCHES OR EXCAVATIONS WHICH ARE FIVE FEET OR DEEPER.
- DIMENSIONS TO PIPELINES ARE TO CENTERLINE UNLESS OTHERWISE NOTED.
- CONSTRUCTION STAKING FOR IMPROVEMENTS SHOWN ON THESE PLANS SHALL BE PERFORMED BY A LICENSED LAND SURVEYOR.
- THE CONTRACTOR SHALL REPLACE ALL EXISTING IMPROVEMENTS DAMAGED DURING CONSTRUCTION TO MATCH EXISTING, INCLUDING PERMANENT TRENCH RESURFACING.
- ALL DIMENSIONS ARE IN FEET OR DECIMALS THEREOF.
- ALL CURB DIMENSIONS AND RADII ARE TO PAVEMENT FACE OF CURB.
- CONTRACTOR TO BE AWARE OF ALL OVERHEAD LINES AT ALL TIMES, SO AS NOT TO DISTURB THEM.
- WATER SHALL BE PROVIDED ONSITE AND USED TO CONTROL DUST DURING CONSTRUCTION OPERATIONS.
- CONTRACTOR SHALL OBTAIN ANY NECESSARY PERMITS FROM THE CITY OF MANHATTAN BEACH FOR ALL WORK WITHIN THE PUBLIC RIGHT-OF-WAY.
- STORM DRAINAGE SYSTEMS SHOWN ON THESE PLANS HAVE BEEN DESIGNED FOR THE FINAL SITE CONDITION AT COMPLETION OF THE PROJECT. THE CONTRACTOR IS RESPONSIBLE FOR MAINTAINING ADEQUATE DRAINAGE OF THE SITE, DURING INTERIM CONDITIONS OF CONSTRUCTION.
- CONTRACTOR SHALL OBTAIN ALL REQUIRED PERMITS, INCLUDING NPDES, FROM THE APPROPRIATE JURISDICTIONAL AGENCIES FOR DISCHARGE OF GROUNDWATER THAT MAY BE NECESSARY TO ACCOMPLISH EXCAVATIONS SHOWN ON THESE PLANS.
- CONTRACTOR SHALL COORDINATE WITH UTILITY OWNERS AFFECTED BY THE IMPROVEMENTS BEFORE START OF WORK.
- ALL DESIGN ENGINEERING, SURVEYING, LABOR, EQUIPMENT AND MATERIAL (EXCEPT AS NOTED) REQUIRED FOR OFF-SITE IMPROVEMENTS ARE THE RESPONSIBILITY OF THE DEVELOPER.
- AT LEAST 72 HOURS SHALL ELAPSE BETWEEN THE TIME CONCRETE IS PLACED AND VEHICULAR TRAFFIC IS PERMITTED.
- CONTRACTOR SHALL ADJUST ALL EXISTING MANHOLES, VAULTS, VALVE BOXES, METER BOXES, TO CONFORM TO FINISHED GRADES AS CONSTRUCTED WHETHER INDICATED ON PLANS OR NOT.
- FORTY-EIGHT HOURS PRIOR TO REMOVAL OF ANY EXISTING PAVEMENTS, CONTRACTOR SHALL NOTIFY UTILITY OWNERS, ETC. CONTRACT UNDERGROUND SERVICE ALERT AT 1-800-422-4133.
- SURVEYING AND GRADE SHEETS SHALL BE PROVIDED TO CITY BY CIVIL ENGINEER OR LAND SURVEYORS.
- ASPHALT CONCRETE PAVEMENTS SHALL BE TYPE IC AR 4000 WITH 6.5% ASPHALT BINDER.
- TACK COAT OF SS-1H ASPHALTIC EMULSION SHALL BE APPLIED TO ALL EXISTING PAVEMENT AGAINST WHICH NEW ASPHALT PAVEMENT IS TO BE PLACED.
- CROSS SLOPE FOR ALLEYS SHALL BE BETWEEN 2% AND 5%.
- REDLINE PLANS WITH CORRECTED ORIGINAL SHEETS FOR CITY ENGINEER'S SIGNATURE.
- APPLICATIONS FOR CLOSING OF THE ALLEY SECTIONS DURING CONSTRUCTION SHALL BE OBTAINED FROM THE CITY OF MANHATTAN BEACH. IF CLOSURE OF AN ALLEY SECTION IS APPROVED BY THE PARKING DIVISION, THE TIMES MUST BE COORDINATED WITH THE INDIVIDUAL PROPERTY OWNERS.

CITY OF MANHATTAN BEACH GENERAL NOTES

- ALL WORK SHALL BE DONE IN ACCORDANCE WITH THE "STANDARD SPECIFICATIONS FOR PUBLIC WORKS CONSTRUCTION" LATEST EDITION, AND ALL APPLICABLE STANDARDS AND SPECIFICATIONS TO THE SATISFACTION OF THE ENGINEER.
- THE CONTRACTOR SHALL NOTIFY UNDERGROUND SERVICE ALERT (U.S.A.) AS REQUIRED PRIOR TO THE START OF WORK. UPON EXPOSING ANY UTILITY'S UNDERGROUND FACILITY THE CONTRACTOR SHALL NOTIFY THAT UTILITY IMMEDIATELY.
- IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO LOCATE ALL SUBSTRUCTURES WITHIN THE ALIGNMENT OF THE PROPOSED IMPROVEMENTS, AND IN THE EVENT OF THE SUBSTRUCTURE DAMAGE, HE SHALL BEAR THE TOTAL COST OF THE REPAIR OR REPLACEMENT.
- THE CONTRACTOR SHALL NOT CONDUCT ANY OPERATIONS OR PERFORM ANY WORK PERTAINING TO THE PROJECT BETWEEN 5:00PM AND 7:30AM ON ANY DAY NOR ON SATURDAY, SUNDAY, HOLIDAY AT ANY TIME EXCEPT AS APPROVED BY THE ENGINEER.
- TREES, FOLIAGE, WALLS FENCES, AND OTHER IMPROVEMENTS SHALL BE PROTECTED IN PLACE AND ANY DAMAGE TO EXISTING IMPROVEMENTS SHALL BE REPLACED IN KIND.
- THE CONTRACTOR IS ADVISED THAT ALL EXCAVATED MATERIALS SHALL BECOME HIS PROPERTY AND SHALL BE REMOVED FROM THE JOBSITE UNLESS INSTRUCTED BY THE ENGINEER TO DO OTHERWISE.
- THE CONTRACTOR SHALL BE RESPONSIBLE DURING ALL PHASES OF THE WORK TO PROVIDE FOR PUBLIC SAFETY AND CONVENIENCE. THE CONTRACTOR SHALL ESTABLISH ADEQUATE ACCESS TO DRIVEWAYS AT THE END OF EACH WORKING DAY TO THE SATISFACTION OF THE ENGINEER.
- THE CONTRACTOR SHALL ADJUST MANHOLE AND VALVE COVERS TO FINISH GRADE
- THE CONTRACTOR SHALL PROVIDE A TRAFFIC CONTROL PLAN FOR THE COMPLETION OF THE PROPOSED IMPROVEMENTS PER THE "WORK AREA TRAFFIC CONTROL HANDBOOK" (I.A.C.H.) TO THE SATISFACTION OF THE ENGINEER. THE CONTRACTOR MAY BE ALLOWED TO CLOSE STREETS WITHIN THE PROJECT AREA TO TRAFFIC FROM 8:00AM TO 4:30PM, EXCEPT THAT NO TWO ADJACENT STREETS SHALL BE CLOSED AT ANY ONE TIME.
- THE CONTRACTOR SHALL PROVIDE A 72-HOUR NOTIFICATION TO THE AFFECTED PROPERTIES, POLICE DEPARTMENT, AND FIRE DEPARTMENT IN THE EVENT OF A CHANGE IN STREET CLOSURE TO TRAFFIC, AND/OR PUBLIC SAFETY VEHICLE, PARKING RESTRICTIONS, AND ON EACH MONDAY MORNING DURING THE CONSTRUCTION PERIOD.
- AS REQUIRED BY THE ENGINEERING, THE CONTRACTOR SHALL FURNISH AND OPERATE A SELF-LOADING MOTOR SWEEPER WITH SPRAY NOZZLES AT LEAST TWICE EACH WORKING DAY TO KEEP PAVED AREAS ACCEPTABLY CLEAN WHEREVER CONSTRUCTION, INCLUDING RESTORATION, IS INCOMPLETE.
- ALL PORTLAND CEMENT CONCRETE (P.C.C.) AND ASPHALTIC CONCRETE (A.C.) SHALL BE REMOVED TO A SAW CUT, SAW CUT SLURRY SHALL BE REMOVED WITH A VACUUM MACHINE SYSTEM. DISPOSED OF PROPERLY. NO SLURRY SHALL BE ALLOWED TO ENTER THE STORM DRAIN SYSTEM.
- SHOULD ANY TRENCH LINE JOINT OR PATCH BE LOCATED WITHIN 36' OF AN EXISTING CURB/GUTTER LINE, THE PAVEMENT SECTION MUST BE REMOVED AND REPLACED WITH THE ADJACENT TRENCH PAVING.
- ALL EXCAVATIONS SHALL BE BACKFILLED AT THE END OF EACH WORKING DAY AND ROADS OPEN TO VEHICULAR TRAFFIC UNLESS OTHERWISE APPROVED BY THE ENGINEER.

ABBREVIATION

AC	ASPHALTIC CONCRETE	MH	MANHOLE
BCR	BEGIN CURVE RETURN	(N)	NORTH
BW	BACK OF WALK	NTS	NOT TO SCALE
BLDG	BUILDING	PA	PLANTER AREA
BM	BENCH MARK	POC	POINT OF CONNECTION
BOS	BOTTOM OF STAIRS	PIV	POST INDICATOR VALVE
BMP	BEST MANAGEMENT PRACTICES	PCC	POINT OF COMPOUND CURVE
CB	CATCH BASIN	PRC	POINT OF REVERSE CURVE
CI	CAST IRON	PRV	PRESSURE REDUCING VALVE
CL	CENTER LINE	PVC	POLYVINYL CHLORIDE
CMU	CONCRETE MASONRY UNIT	R	RADIUS
CO	CLEANOUT	RCIP	RECTANGULAR CAST IRON PIPE
CONC	PORTLAND CEMENT CONCRETE	RD	ROOF DRAIN
CF	CURB FACE	RW	RIGHT-OF-WAY
DW	DOMESTIC WATER	(S)	SOUTH
(E)	EAST	S=	SLOPE EQUALS
ECR	END CURVE RETURN	SD	STORM DRAIN
EG	EDGE OF GUTTER	SSMH	SANITARY SEWER MANHOLE
EL. OR ELEV	ELEVATION	SS	SANITARY SEWER
ELEC	ELECTRIC, ELECTRICAL	STD	STANDARD
EX. OR EXIST.	EXISTING	SDMH	STORM DRAIN MANHOLE
FDC	FIRE DEPARTMENT CONNECTION	TC	TOP OF CURB
FF	FINISHED FLOOR	TEL	TELEPHONE
FG	FINISHED GRADE (LANDSCAPE)	TG	TOP OF GRATE
FS	FINISHED SURFACE (HARDSCAPE)	TOS	TOP OF STAIRS
FH	FIRE HYDRANT	TW	TOP OF WALL
FL	FLOW LINE	TS	TRAFFIC SIGNAL
FT	FOOT OR FEET	TSB	TRAFFIC SIGNAL BOX
FU	FIXTURE UNITS	TYP	TYPICAL
FW	FIRE WATER	TV	TELEVISION
GPM	GALLONS PER MINUTE	VIF	VERIFY IN FIELD
GV	GATE VALVE	VL	VAULT
HDPE	HIGH DENSITY POLYETHYLENE	VCP	VITRIFIED CLAY PIPE
HP	HIGH POINT	(W)	WEST
INV.	INVERT	W	WATER
LP	LOW POINT	WM	WATER METER
MAX.	MAXIMUM	WV	WATER VALVE
MIN.	MINIMUM		



VICINITY MAP
NOT TO SCALE
THOMAS GUIDE LOS ANGELES EDITION
PAGE: 732 GRID: H4

STORMWATER POLLUTION NOTES

ATTACHMENT "A" NOTES

- EVERY EFFORT SHOULD BE MADE TO ELIMINATE THE DISCHARGE OF NON-STORMWATER FROM THE PROJECT SITE AT ALL TIMES.
- ERODED SEDIMENTS AND OTHER POLLUTANTS MUST BE RETAINED ON-SITE AND MAY NOT BE TRANSPORTED FROM THE SITE VIA SHEET FLOW, SWALES, AREA DRAINS, NATURAL DRAINAGE COURSES OR WIND.
- STOCKPILES OF EARTH AND OTHER CONSTRUCTION RELATED MATERIALS MUST BE PROTECTED FROM BEING TRANSPORTED FROM THE SITE BY THE FORCES OF WIND OR WATER.
- FUELS, OILS, SOLVENTS, AND OTHER TOXIC MATERIALS MUST BE STORED IN ACCORDANCE WITH THEIR LISTING AND ARE NOT TO CONTAMINATE THE SOIL AND SURFACE WATERS. ALL APPROVED STORAGE CONTAINERS ARE TO BE PROTECTED FROM THE WEATHER. SPILLS MUST BE CLEANED UP IMMEDIATELY AND DISPOSED OF IN A PROPER MANNER. SPILLS MAY NOT BE WASHED INTO THE DRAINAGE SYSTEM.
- EXCESS OR WASTE CONCRETE MAY NOT BE WASHED INTO PUBLIC WAY OR ANY OTHER DRAINAGE SYSTEM. PROVISIONS SHALL BE MADE TO RETAIN CONCRETE WASTES ON-SITE UNTIL THEY CAN BE DISPOSED OF SOLID WASTE.
- TRASH AND CONSTRUCTION RELATED SOLID WASTES MUST BE DEPOSITED INTO A COVERED RECEPTACLE TO PREVENT CONTAMINATION OF RAINWATER AND DISPERSAL BY WIND.
- SEDIMENTS AND OTHER MATERIALS MAY NOT BE TRACKED FROM THE SITE BY VEHICLE TRAFFIC. THE CONSTRUCTION ENTRANCE ROADWAYS MUST BE STABILIZED SO AS TO INHIBIT SEDIMENTS FROM BEING DEPOSITED INTO PUBLIC WAY. ACCIDENTAL DEPOSITIONS MUST BE SWEEPED UP IMMEDIATELY AND MAY NOT BE WASHED DOWN BY RAIN OR OTHER MEANS.
- ANY SLOPES WITH DISTURBED SOILS OR DENUDED OF VEGETATION MUST BE STABILIZED SO AS TO INHIBIT EROSION BY WIND OR WATER.
- AS PROJECT OWNER OR AUTHORIZED AGENT OF THE OWNER, I HAVE READ AND UNDERSTAND THE REQUIREMENTS LISTED ABOVE, NECESSARY TO CONTROL STORM WATER POLLUTION FROM SEDIMENTS, EROSION, AND CONSTRUCTION MATERIALS, AND I CERTIFY THAT I WILL COMPLY WITH THESE REQUIREMENTS.

PRINT NAME _____
(OWNER OR AUTHORIZED AGENT OF THE OWNER)

SIGNATURE _____ DATE _____
(OWNER OR AUTHORIZED AGENT OF THE OWNER)

LEGEND

---	PROPERTY LINE
---	EASEMENT
---	EXISTING DOMESTIC WATER
---	EXISTING SANITARY SEWER
---	EXISTING STORM DRAIN
---	EXISTING GAS LINE
---	EXISTING ELECTRICAL LINE
⊙	EXISTING MANHOLE
---	PROPOSED DOMESTIC WATER (PER SEPARATE PERMIT)
---	PROPOSED SANITARY SEWER (PER SEPARATE PERMIT)
---	PROPOSED STORM DRAIN
---	PROPOSED GAS LINE (PER SEPARATE PERMIT)
---	PROPOSED ELECTRICAL LINE (PER SEPARATE PERMIT)
⊙	PROPOSED MANHOLE
⊙	PROPOSED THRUST BLOCK (PER SEPARATE PERMIT)
---	EASEMENT LINE

JOB ADDRESS

MANHATTAN VILLAGE MALL — BOUNDED BY SEPULVEDA BLVD., ROSECRANS AVE., VILLAGE DR. AND MARINE AVE., MANHATTAN BEACH, CA

OWNER

RREEF AMERICA LLC
222 SOUTH RIVERSIDE PLAZA, 26TH FLOOR
CHICAGO, IL 60606

BASIS OF BEARING

THE BEARING OF N 89°58'45" ALONG THE CENTERLINE OF ROSECRANS BLVD. AS SHOWN ON PARCEL MAP NO. 12219, BOOK 122, PAGES 33-35 OF MAPS, IN THE OFFICE OF THE COUNTY RECORDER, WAS USED AS THE BASIS OF BEARINGS FOR THIS SURVEY.

BENCH MARK

THE ELEVATION OF 144.024 ON BENCH MARK ID L346 (LEAD RIVET AT ECR, NW COR. MARINE AND MEADOWS) NAVD 1988 DATUM, 2005 ADJUSTMENT, AS SHOWN IN CITY OF MANHATTAN BEACH WAS USED AS ELEVATION DATUM FOR THIS SURVEY.

SHEET INDEX

- GENERAL NOTES, VICINITY AND KEY MAP
- TYPICAL CROSS SECTIONS
- SEPULVEDA BOULEVARD PLAN AND PROFILE
- STORM DRAIN PLAN AND PROFILE
- OFFSITE TRAFFIC SIGNAL MODIFICATIONS
- OFFSITE TRAFFIC SIGNAL MODIFICATIONS
- OFFSITE STREET SIGNING AND STRIPING PLAN

STANDARD PLANS

CITY OF MANHATTAN BEACH DEPARTMENT OF PUBLIC WORKS

ST-2 STANDARD SIDEWALK

STANDARD PLANS FOR PUBLIC WORKS CONSTRUCTION

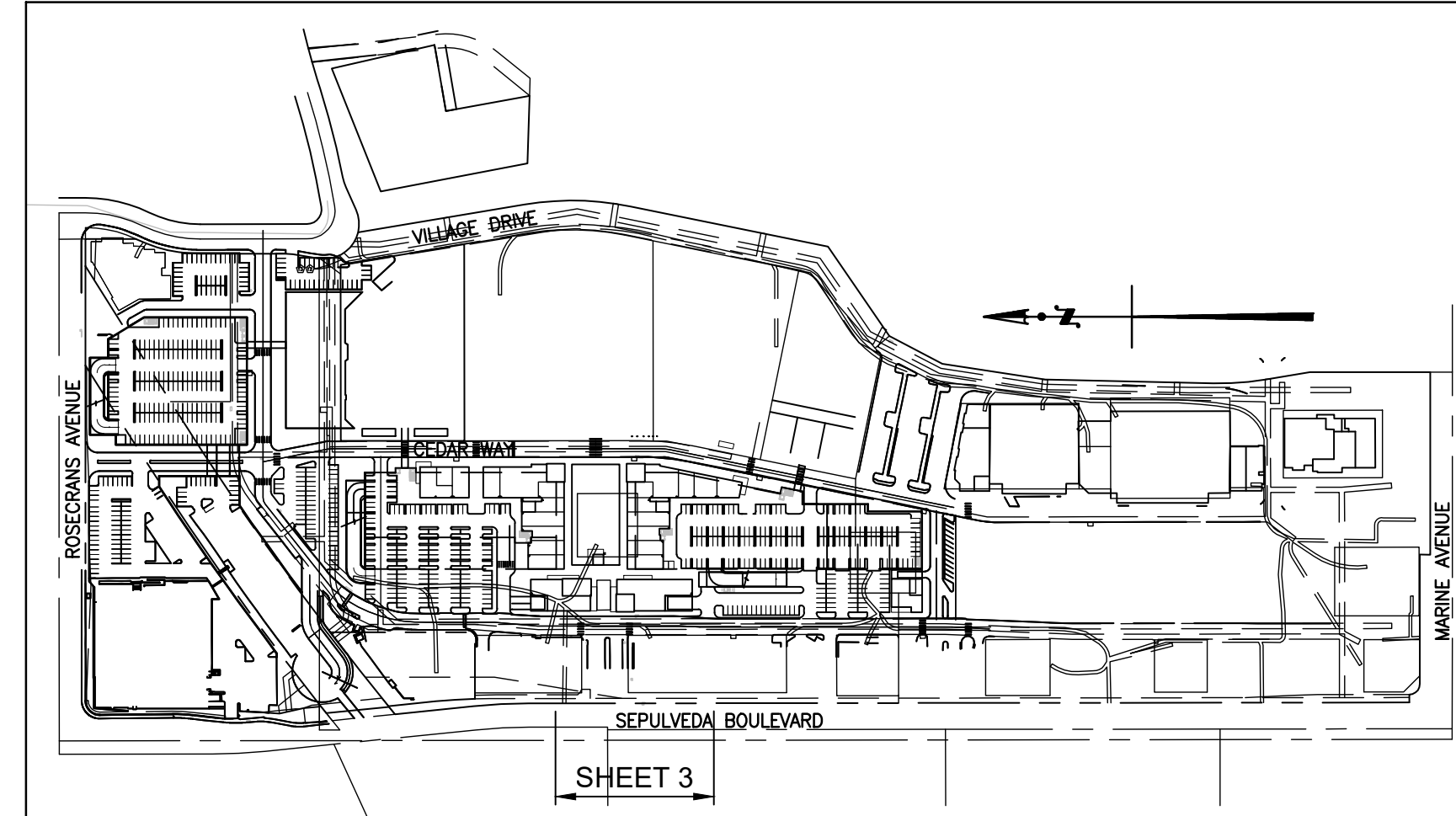
120-2 CURB AND GUTTER — BARRIER
380-4 CONCRETE COLLAR FOR RCP 12" THROUGH 72"

LOS ANGELES COUNTY DEPARTMENT OF PUBLIC WORKS

3080-2 PIPE BEDDING IN TRENCHES

STANDARD PLANS FOR STATE OF CALIFORNIA DEPARTMENT OF TRANSPORTATION

A20D PAVEMENT MARKERS AND TRAFFIC LINES — TYPICAL DETAILS
A24F PAVEMENT MARKINGS — CROSSWALKS
A87A CURBS AND DRIVEWAYS
A68A CURB RAMPS
D72 DRAINAGE INLETS
D78A GUTTER DEPRESSIONS



CITY OF MANHATTAN BEACH
PUBLIC WORKS DEPARTMENT — ENGINEERING DIVISION

GENERAL NOTES, VICINITY MAP AND KEY MAP

NO.	BY	DATE

CHECKED BY _____ DATE _____
CITY ENGINEER

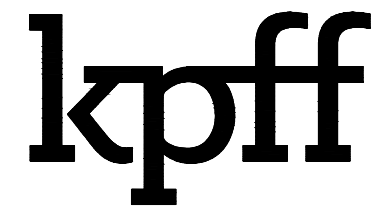
DESIGNED BY _____ DATE _____
CITY ENGINEER

DRAWN BY _____ DATE _____
CITY ENGINEER

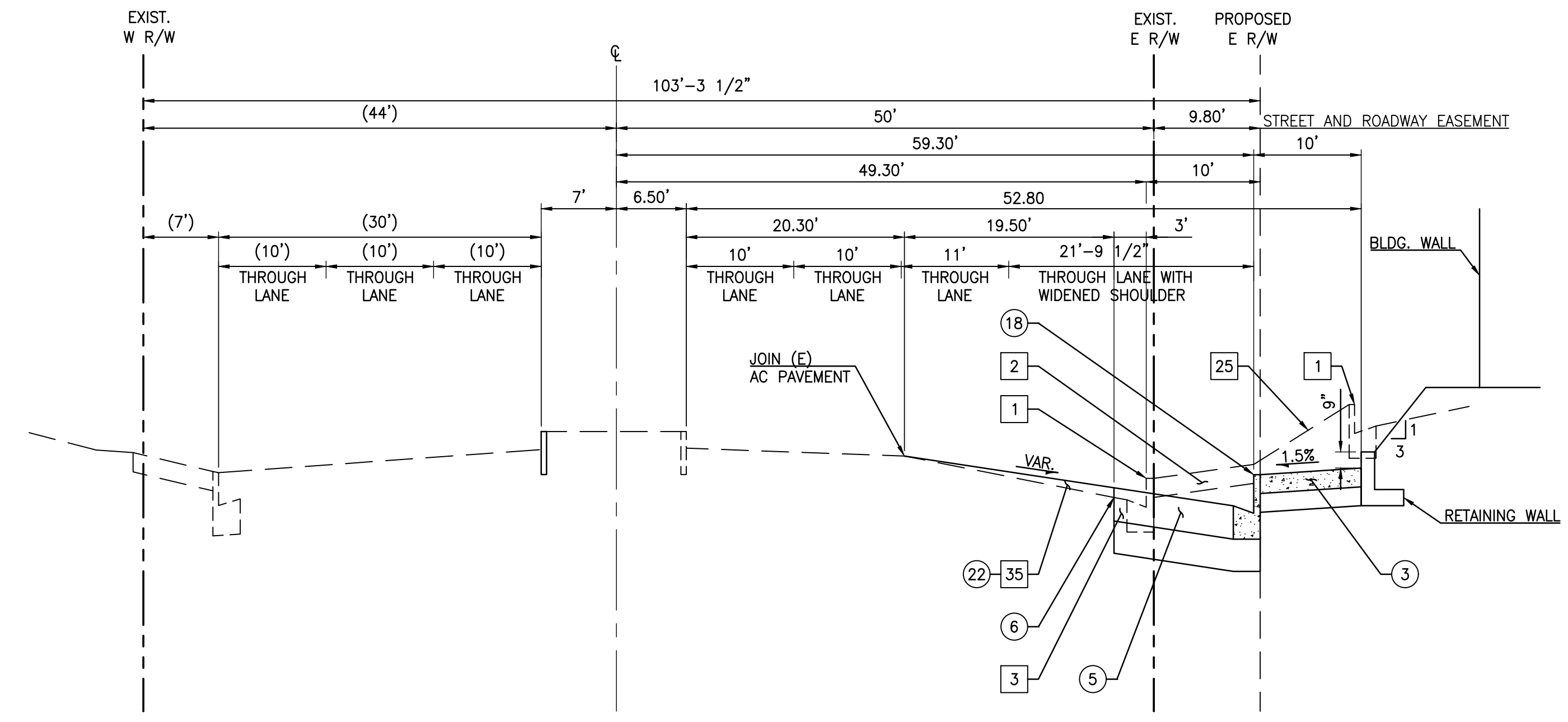
APPROVED BY _____ DATE _____
DIRECTOR OF PUBLIC WORKS

SCALE: PER PLAN DATE: 1-13-2021 DRAWING NO. _____

DATE: XX-XX-XX SHEET 1 of 7



700 South Flower Street
Suite 2100
Los Angeles, CA 90017
O: 213.418.0201
F: 213.266.5294
www.kpff.com



SEPULVEDA BOULEVARD TYPICAL SECTION

NOT TO SCALE

DEMOLITION NOTES:

REMOVE & DEMOLISH

- | | |
|---|---|
| 1 REMOVE CONCRETE CURB AND GUTTER. | 19 NOT USED. |
| 2 REMOVE CONCRETE SIDEWALK. | 20 NOT USED. |
| 3 REMOVE ASPHALT CONCRETE PAVEMENT. | 21 NOT USED. |
| 4 NOT USED. | 22 NOT USED. |
| 5 REMOVE AND RELOCATE STREET LIGHTING. SEE STREET LIGHTING PLAN FOR NEW LOCATION. | 23 NOT USED. |
| 6 REMOVE STREET LIGHTING PULL BOX. | 24 REMOVE CONCRETE CURB RAMP. |
| 7 REMOVE TREE. | 25 REMOVE LANDSCAPE AREA. |
| 8 NOT USED. | 26 NOT USED. |
| 9 NOT USED. | 27 NOT USED. |
| 10 NOT USED. | 28 NOT USED. |
| 11 NOT USED. | 29 REMOVE AND RELOCATE STREET LIGHTING/TRAFFIC SIGNAL POLE. SEE TRAFFIC SIGNAL PLAN FOR NEW LOCATION. |
| 12 NOT USED. | 30 NOT USED. |
| 13 NOT USED. | 31 REMOVE AND RELOCATE CALTRANS COMMUNICATION BOX. SEE TRAFFIC SIGNAL PLAN FOR NEW LOCATION. |
| 14 NOT USED. | 32 REMOVE INLET TYPE "OL". |
| 15 REMOVE TRAFFIC SIGNAL PULL BOX. | 33 REMOVE AND RELOCATE PEDESTRIAN CROSSWALK BUTTON POST. SEE TRAFFIC SIGNAL PLAN FOR NEW LOCATION. |
| 16 NOT USED. | 34 NOT USED. |
| 17 NOT USED. | 35 COLDPLANE (MILL) ASPHALT CONCRETE PAVEMENT, t=2" MIN. |
| 18 REMOVE CONCRETE CURB. | 36 REMOVE RCP STORM DRAIN LINE. SEE PLAN FOR SIZE. |

PROTECT-IN-PLACE

- | |
|---|
| 1 CONCRETE CURB AND GUTTER. |
| 2 CONCRETE SIDEWALK. |
| 3 NOT USED. |
| 4 NOT USED. |
| 5 NOT USED. |
| 6 NOT USED. |
| 7 NOT USED. |
| 8 NOT USED. |
| 9 NOT USED. |
| 10 NOT USED. |
| 11 NOT USED. |
| 12 REINFORCED CONCRETE PIPE. SIZE PER PLAN. |
| 13 NOT USED. |
| 14 NOT USED. |
| 15 NOT USED. |
| 16 NOT USED. |
| 17 NOT USED. |
| 18 CONCRETE CURB. |
| 19 COLORED CONCRETE SIDEWALK. |
| 20 TREE. |
| 21 MANHOLE NO. 1. |
| 22 LANDSCAPE AREA. |

CONSTRUCTION NOTES:

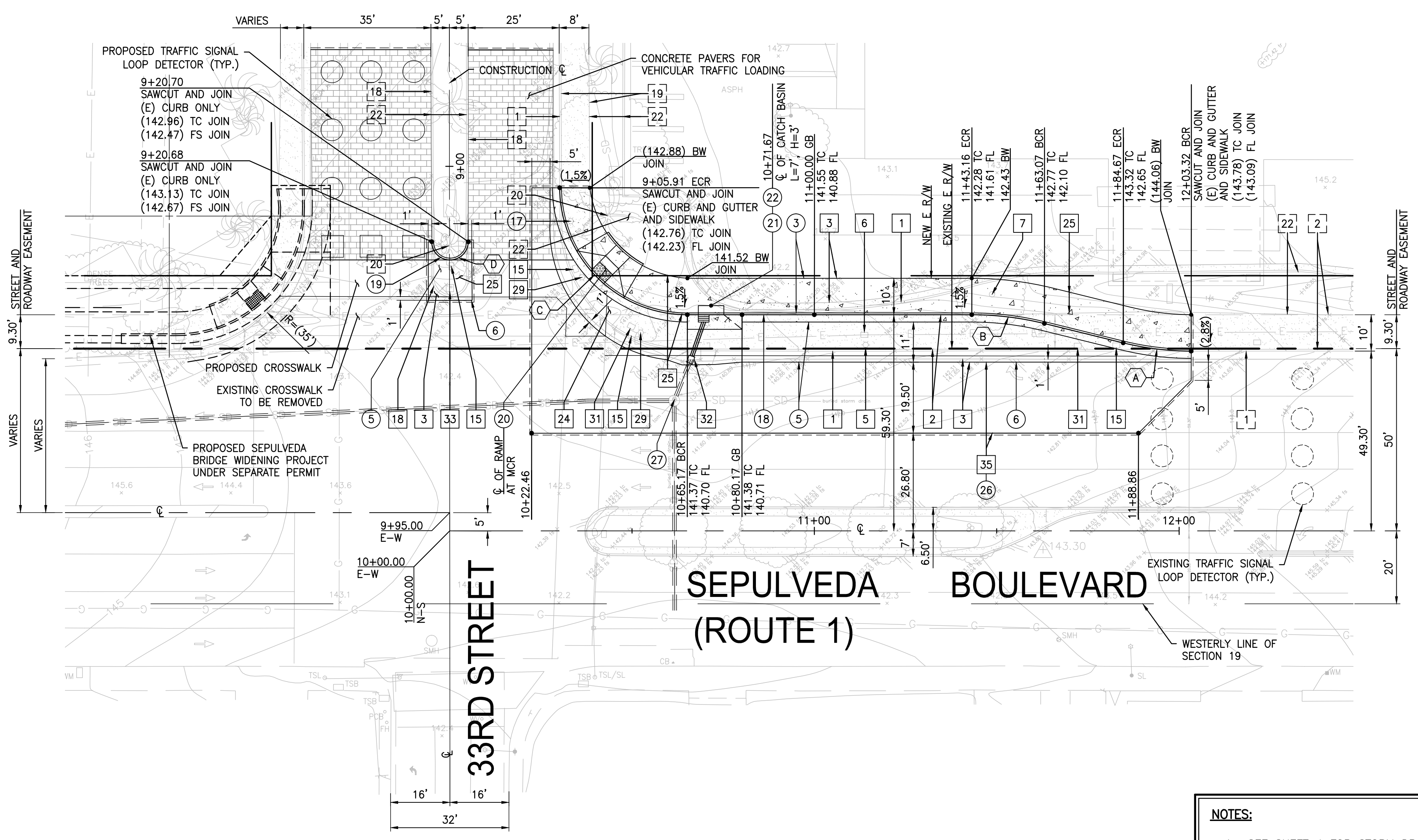
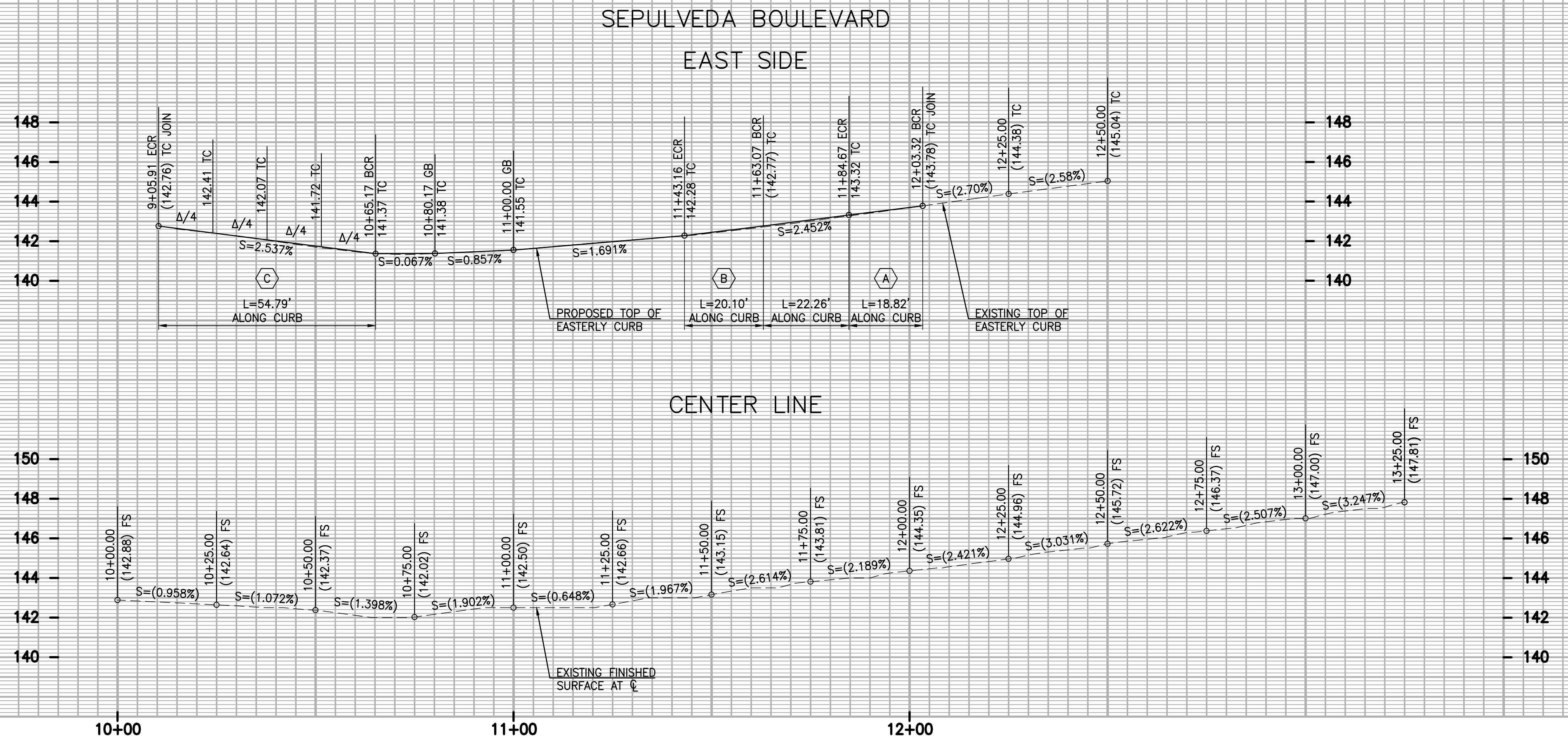
- | | |
|--|---|
| 1 NOT USED. | 14 NOT USED. |
| 2 NOT USED. | 15 NOT USED. |
| 3 CONSTRUCT 4" THICK SIDEWALK ON 4" THICK CRUSHED AGGREGATE BASE PER CITY OF MANHATTAN BEACH STANDARD PLAN NO. ST-2. | 16 NOT USED. |
| 4 NOT USED. | 17 ADJUST PEDESTRIAN LIGHT TO NEW GRADE. |
| 5 CONSTRUCT 8" THICK AC PAVEMENT ON 6" THICK CRUSHED AGGREGATE BASE. | 18 CONSTRUCT CURB AND GUTTER, TYPE A2-8 PER CALTRANS STANDARD PLAN A87A ON 6" THICK CRUSHED AGGREGATE BASE. |
| 6 SAWCUT LINE AND JOIN. | 19 CONSTRUCT CURB ONLY, TYPE A1-6 PER SPPWC STANDARD PLAN NO. 120-2 ON 6" THICK CRUSHED AGGREGATE BASE. |
| 7 NOT USED. | 20 CONSTRUCT CURB RAMP, CASE B PER CALTRANS STANDARD PLAN A88A. |
| 8 NOT USED. | 21 CONSTRUCT DRAINAGE INLET, TYPE GOL, L=7', T=6", H=3' PER CALTRANS STANDARD PLAN D72. |
| 9 NOT USED. | 22 CONSTRUCT GUTTER DEPRESSIONS, TYPE GOL INLET ON GRADE, W=4', D=1 1/4" PER CALTRANS STANDARD PLAN D78A. |
| 10 CONSTRUCT CONCRETE COLLAR FOR RCP 12" THROUGH 72" PER SPPWC STANDARD PLAN NO. 380-4, D1=D2=18", L=12", T=5". | 23 INSTALL 12" RCP, D-2000 STORM DRAIN LINE PER LOS ANGELES COUNTY DEPARTMENT OF PUBLIC WORKS STANDARD PLAN NO. 3080-2, CASE 3. SEE PLAN FOR SLOPE. |
| 11 NOT USED. | 24 NOT USED. |
| 12 NOT USED. | 25 NOT USED. |
| 13 NOT USED. | 26 CONSTRUCT VARIABLE THICKNESS ASPHALT CONCRETE OVERLAY, t=2" MIN. |
| | 27 ADJUST STORM DRAIN MANHOLE TO GRADE. |



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CITY OF MANHATTAN BEACH			
PUBLIC WORKS DEPARTMENT - ENGINEERING DIVISION			
TYPICAL CROSS SECTIONS			
REVISIONS NO. BY DATE		CHECKED BY _____ DATE _____ CITY ENGINEER	
APPROVED BY _____ DATE _____ DIRECTOR OF PUBLIC WORKS		SCALE PER PLAN DATE 1-13-2021 DRAWING NO.	
REFERENCES REVIEWED BY _____ DATE _____ DESIGNED BY _____ DATE _____ DRAWN BY _____ DATE _____	SHEET 2 of 7		



DEMOLITION NOTES:

- REMOVE & DEMOLISH**
- 1 REMOVE CONCRETE CURB AND GUTTER.
 - 2 REMOVE CONCRETE SIDEWALK.
 - 3 REMOVE ASPHALT CONCRETE PAVEMENT.
 - 5 REMOVE AND RELOCATE STREET LIGHTING. SEE STREET LIGHTING PLAN FOR NEW LOCATION.
 - 6 REMOVE STREET LIGHTING PULL BOX.
 - 7 REMOVE TREE.
 - 15 REMOVE TRAFFIC SIGNAL PULL BOX.
 - 18 REMOVE CONCRETE CURB.
 - 24 REMOVE CONCRETE CURB RAMP.
 - 25 REMOVE LANDSCAPE AREA.
 - 29 REMOVE AND RELOCATE STREET LIGHTING/TRAFFIC SIGNAL POLE. SEE TRAFFIC SIGNAL PLAN FOR NEW LOCATION.
 - 31 REMOVE AND RELOCATE CALTRANS COMMUNICATION BOX. SEE TRAFFIC SIGNAL PLAN FOR NEW LOCATION.

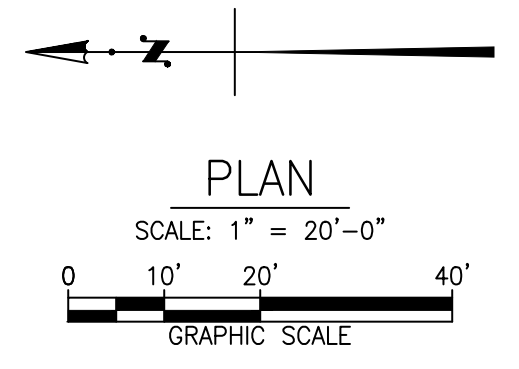
- PROTECT-IN-PLACE**
- 1 CONCRETE CURB AND GUTTER.
 - 2 CONCRETE SIDEWALK.
 - 18 CONCRETE CURB.
 - 19 COLORED CONCRETE SIDEWALK.
 - 20 TREE.
 - 22 LANDSCAPE AREA.

CONSTRUCTION NOTES:

- 3 CONSTRUCT 4" THICK SIDEWALK ON 4" THICK CRUSHED AGGREGATE BASE PER CITY OF MANHATTAN BEACH STANDARD PLAN NO. ST-2.
- 5 CONSTRUCT 8" THICK AC PAVEMENT ON 6" THICK CRUSHED AGGREGATE BASE.
- 6 SAWCUT LINE AND JOIN.
- 17 ADJUST PEDESTRIAN LIGHT TO NEW GRADE.
- 18 CONSTRUCT CURB AND GUTTER, TYPE A2-8 PER CALTRANS STANDARD PLAN A87A ON 6" THICK CRUSHED AGGREGATE BASE.
- 19 CONSTRUCT CURB ONLY, TYPE A1-6 PER SPPWC STANDARD PLAN NO. 120-2 ON 6" THICK CRUSHED AGGREGATE BASE.
- 20 CONSTRUCT CURB RAMP, CASE B PER CALTRANS STANDARD PLAN A88A.
- 21 CONSTRUCT CURB AND GUTTER, TYPE GOL, L=7', T=6", H=3' PER CALTRANS STANDARD PLAN D72.
- 22 CONSTRUCT GUTTER DEPRESSIONS, TYPE GOL INLET ON GRADE, W=4", D=1 1/4" PER CALTRANS STANDARD PLAN D78B.
- 26 CONSTRUCT VARIABLE THICKNESS ASPHALT CONCRETE OVERLAY, t=2" MIN.
- 27 ADJUST STORM DRAIN MANHOLE TO GRADE.

CURVE DATA

Symbol	Angle	R	L	T	REMARKS
A	13°50'11"	77.95'	18.82'	9.46'	CURB RETURN
B	13°50'11"	83.25'	20.10'	10.10'	CURB RETURN
C	89°41'39"	35'	54.79'	34.81'	CURB RETURN
D	180°00'00"	5'	15.71'	-	MEDIAN NOSE



NOTES:
1. SEE SHEET 4 FOR STORM DRAIN PLAN AND PROFILE.



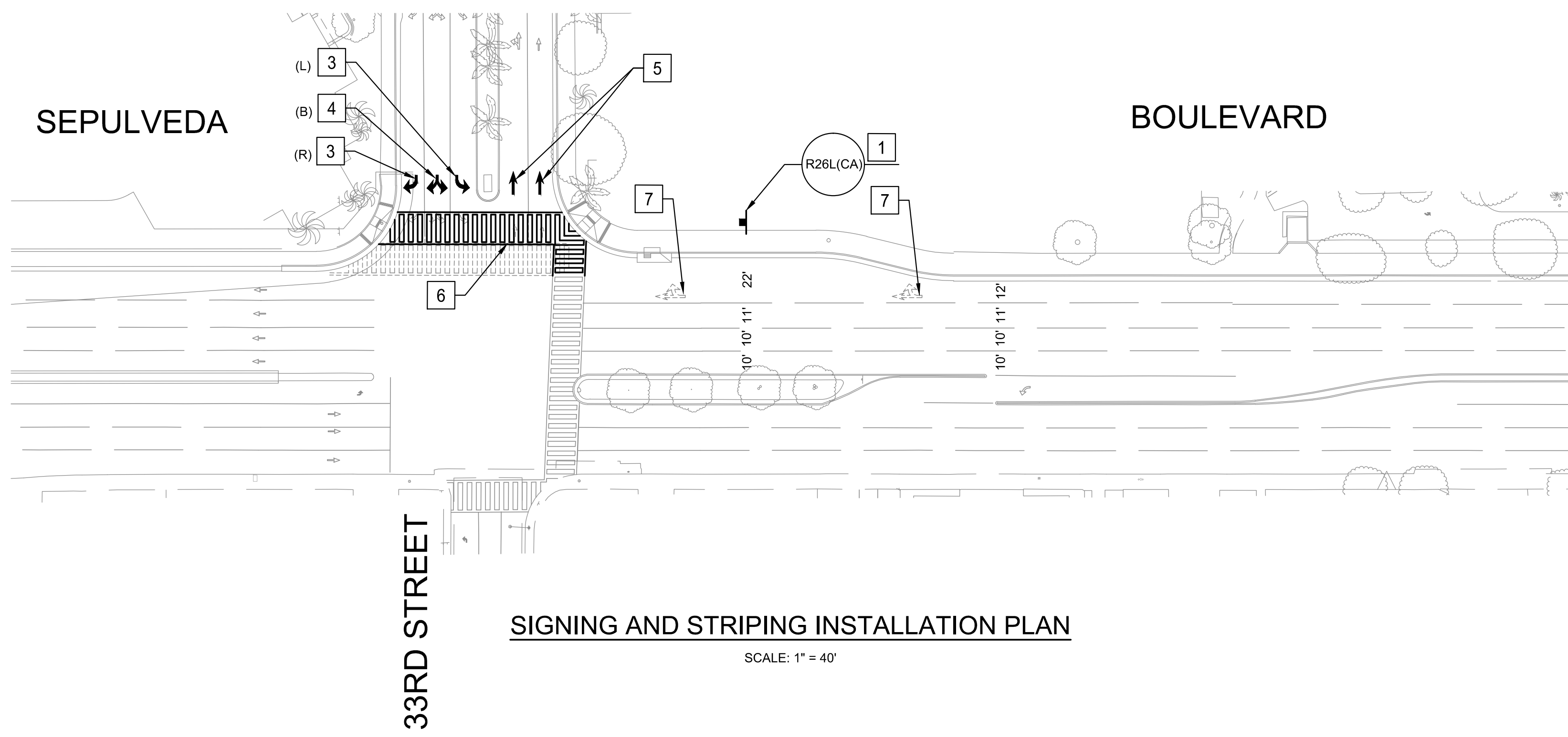
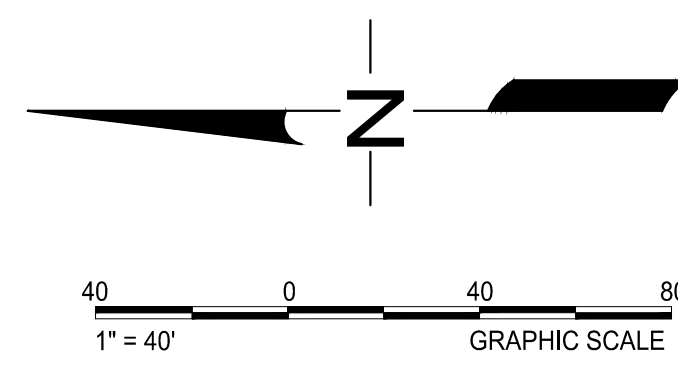
CITY OF MANHATTAN BEACH
PUBLIC WORKS DEPARTMENT - ENGINEERING DIVISION

OFFSITE STREET PLAN AND PROFILE
SEPULVEDA BOULEVARD
FROM 33RD STREET TO 200 S/O 33RD STREET

REVISIONS		CHECKED BY		APPROVED BY	
NO.	DATE	NAME	DATE	NAME	DATE

REFERENCES		DESIGNED BY		SCALE	

DATE: 1-13-2021
DRAWING NO.:
SHEET 3 of 7



SIGNING AND STRIPING INSTALLATION PLAN

SCALE: 1" = 40'

GENERAL NOTES FOR SIGNING & STRIPING

1. ALL TRAFFIC LINES AND PAVEMENT MARKINGS SHOWN SHALL BE THERMOPLASTIC UNLESS OTHERWISE NOTED.
2. ALL TRAFFIC LINES AND PAVEMENT MARKINGS SHALL CONFORM TO CALTRANS STANDARD PLANS AND SPECIFICATIONS (LATEST EDITION).
3. ALL CONFLICTING LINES AND MARKINGS SHALL BE REMOVED BY WET SANDBLASTING OR GRINDING (THEN PAINTED BLACK) AND INCLUDES REMOVAL OF RAISED PAVEMENT MARKERS.
4. ALL TURN ARROW MARKINGS SHALL BE TYPE IV (L OR R) UNLESS OTHERWISE NOTED.
5. LANE WIDTHS SHALL BE MEASURED BETWEEN THE CENTERLINES OF EACH ADJACENT SINGLE OR DOUBLE STRIPE OR TOP OF CURB AS APPROPRIATE.
6. ALL NEW SIGNING SHALL CONFORM TO THE CALIFORNIA MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES, LATEST EDITION.
7. ALL SIGNING SHOWN HEREON SHALL BE INSTALLED, RELOCATED, OR REMOVED BY THE CONTRACTOR, UNLESS OTHERWISE NOTED.

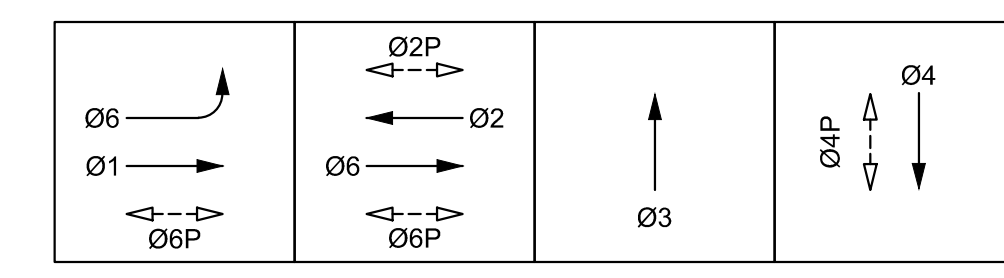
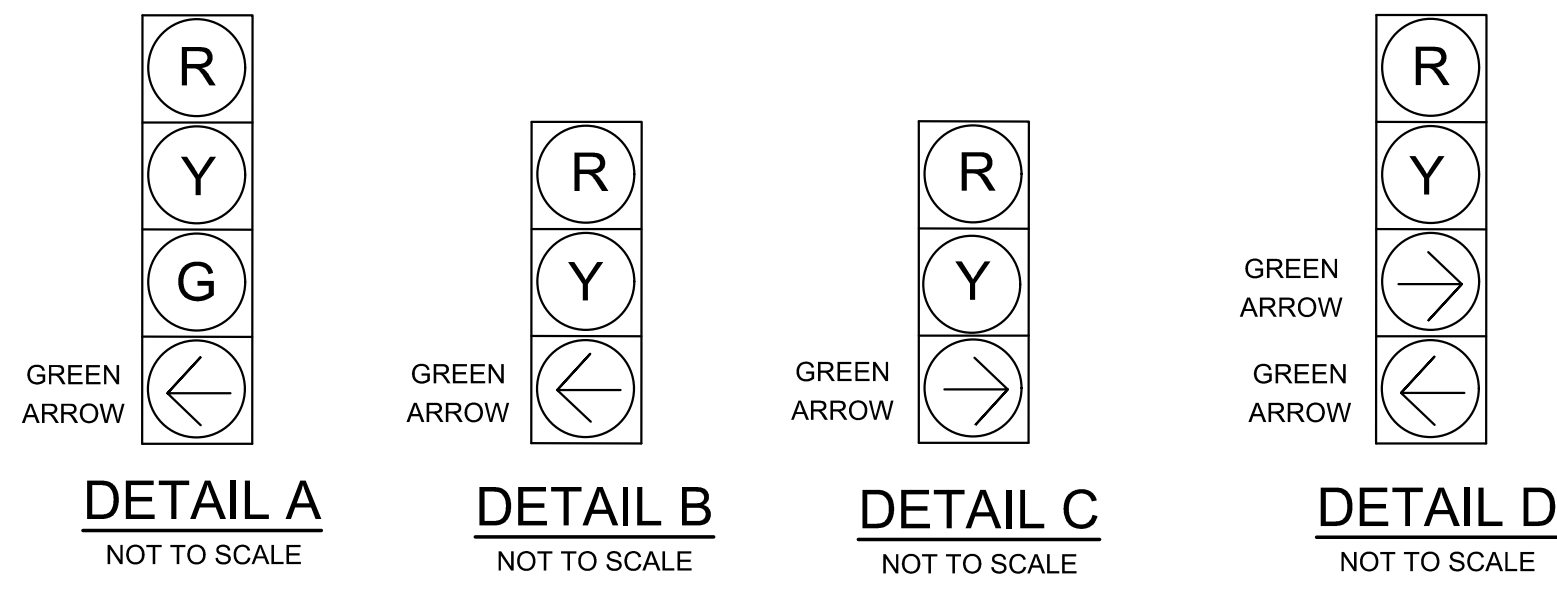
CONSTRUCTION NOTES (THIS SHEET)

- 1 INSTALL SIGN(S) AS SHOWN.
- 2 INSTALL DETAIL 27B. LENGTH IS AS SHOWN ON PLAN.
- 3 INSTALL TYPE IV (L)/(R) ARROW.
- 4 INSTALL TYPE IV (B) ARROW.
- 5 INSTALL TYPE I 10' ARROW.
- 6 INSTALL 14" WIDE LADDER CROSSWALK PER CALTRANS STD. PLAN A24F WITH 2" BAR MARKINGS, 1" OUTSIDE EDGE, AND 2" SPACE. REMOVE CONFLICTING STRIPING AND PAVEMENT MARKINGS.
- 7 REMOVE EXISTING PAVEMENT MARKING.



FEHR & PEERS
 600 Wilshire Boulevard Los Angeles, CA 90017
 Suite 1050 (213) 261-3050

CITY OF MANHATTAN BEACH																																																		
PUBLIC WORKS DEPARTMENT – ENGINEERING DIVISION																																																		
OFFSITE STREET SIGNING AND STRIPING PLAN SEPULVEDA BOULEVARD & 33RD STREET																																																		
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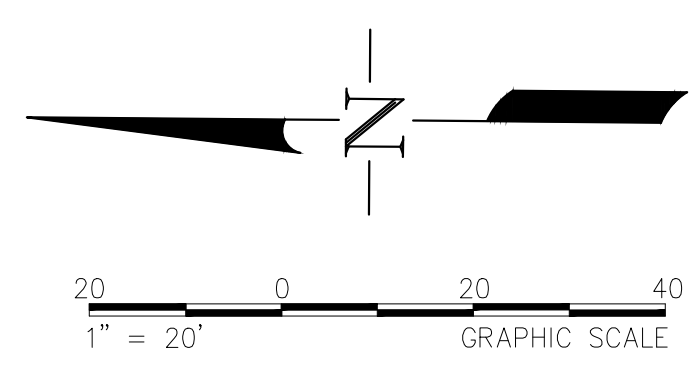
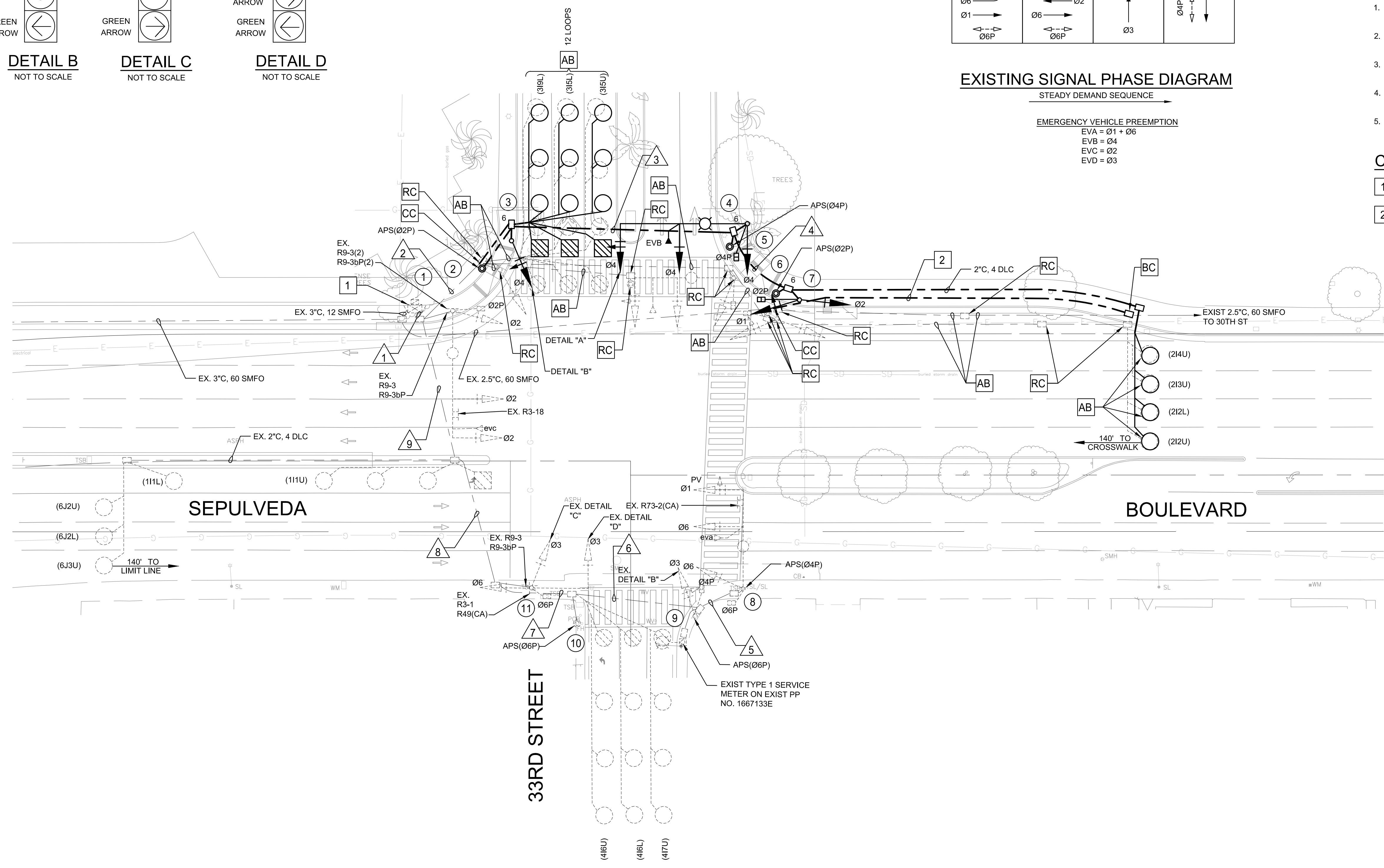


GENERAL NOTES FOR SIGNING & STRIPING

1. TRAFFIC SIGNAL SYSTEM SHALL CONFORM TO CALTRANS STANDARD PLANS AND SPECIFICATIONS (LATEST EDITION).
2. FOR ACCURATE RIGHT OF WAY DATA, CONTACT RIGHT OF WAY ENGINEERING AT THE DISTRICT OFFICE.
3. ALL EXISTING SIGNAL EQUIPMENT SHALL BE PROTECTED IN PLACE UNLESS OTHERWISE.
4. CONTRACTOR MUST CALL UNDERGROUND SERVICE ALERT (USA) AT 811 48 HOURS PRIOR TO ANY EXCAVATION.
5. ALL NEW SIGNS SHALL CONFORM TO THE CALIFORNIA MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES, LATEST EDITION.

CONSTRUCTION NOTES (THIS SHEET)

1. EXISTING MODEL 332 CONTROLLER CABINET ASSEMBLY AND MODEL 2070L CONTROLLER TO REMAIN.
2. DISCONNECT EXISTING 60 SMFO FROM THE 33RD ST./SEPULVEDA BLVD. CONTROLLER CABINET AND REMOVE FROM EXISTING CONDUIT. INTERCEPT EXISTING CONDUIT AND INSTALL NEW PULL BOX AND 3" CONDUIT. RE-PULL EXISTING 60 SMFO THROUGH NEW AND EXISTING CONDUIT AND RECONNECT TO 33RD ST./SEPULVEDA BLVD. CONTROLLER CABINET.



FEHR & PEERS
600 Wilshire Boulevard
Suite 1050 Los Angeles, CA 90017
(213) 261-3050

CITY OF MANHATTAN BEACH			
PUBLIC WORKS DEPARTMENT - ENGINEERING DIVISION			
OFFSITE TRAFFIC SIGNAL MODIFICATION SEPULVEDA BOULEVARD & 33RD STREET			
DESIGNED BY J. SCHMIDT		APPROVED BY DIRECTOR OF PUBLIC WORKS	
DATE 12/30/20		DATE 1-13-2021	
DRAWN BY J. SCHMIDT		DATE 12/30/20	
DATE 12/30/20		SHEET 6 OF 7	
DRAWING NO. P-906			

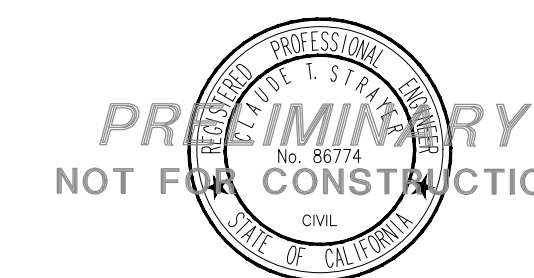
N:\Jobs\Active\2709\2792_Manhattan Village SSS\CAD\33rd Signal Modification\Calttrans et Submission\2792-E1-EG.dwg, Dec 30, 2020 - 10:17am

CONDUCTOR SCHEDULE																
CONDUCTOR DESIGNATION			NUMBER OF CONDUCTORS													
CABLE TYPE	S T D	PHASE	RUN NUMBER													
			1	2	3	4	5	6	7	8	9					
VEH-PED 12CSC	①	2,2P	1	-	-	-	-	-	-	-	-	-				
	②	-	2P	1(N)	1(N)	-	-	-	-	-	-	-				
	③	4	-	1(N)	1(N)	-	-	-	-	-	-	-				
	④	4,4P	-	1(N)	1(N)	1(N)	-	-	-	-	-	-				
	⑤	-	4P	1(N)	1(N)	1(N)	-	-	-	-	-	-				
	⑥	-	2P	1(N)	1(N)	1(N)	1(N)	-	-	-	-	-				
	⑦	1,2,2P	-	1(N)	1(N)	1(N)	1(N)	-	-	-	-	-				
	⑧	1,6,6P	4P	1	1	1	1	1	1	1	1	1				
	⑨	3,4P	6P	1	1	1	1	1	1	1	1	1				
	⑩	-	6P	1	1	1	1	1	1	1	1	1				
PPB 3CSC	⑪	3,6,6P	1	-	-	-	-	-	-	-	-	-				
TOTAL 12/3 CABLES			7	6	3	3	2	2	1	1	1	2	2	3	3	3
#14	ILLUMINATED SIGNS															
		PEU	3													
	TOTAL		3													
#10	LUMINAIRE		-	2	2		2	2	2	2	2	2	2			
#6	CONTROLLER POWER		2				2	2	2	2	2	2				
DLC	Ø1		3													2
	Ø2		4(N)	4(N)	4(N)	4(N)										
	Ø3		3(N)	3(N)												
	Ø4		3							3	3	3				
	Ø6		3													3
	TOTAL		6(7)	7(N)	4(N)	4(N)				3	3	3			8	
EVP			3					1	1	1	1	1	1	1		
NEW/EXISTING			E	E	N	N	E	E	E	E	E	E	E	E		
CONDUIT SIZE			2-3"	3"	3"	3"	2"	2"	2"	2.5"	3"					

ALL CONDUCTOR AND OR CABLES ARE EXISTING UNLESS OTHERWISE NOTED.
(N) = INDICATES NEW CONDUCTOR OR CABLE.

POLE SCHEDULE												
POLE			SIGNAL M.A. LENGTH	SIGNAL MOUNTING			LUMINAIRE		PED PUSH BUTTON			REMARKS
NO.	TYPE	HEIGHT		M.A.	POLE	PED	M.A. LENGTH	LED WATTS	NO.	QUAD.	PHASE	
①	26-4-100	30'	45'	MAS MAS	SV-1-T	SP-1-T	12'	137	-	-	-	
②	PPB POST(N)	5'-4"	-	-	-	-	-	-	1	W	2	
③	1-A(N)	10'	-	-	TV-1-T(N)	-	-	-	-	-	-	
④	26-4-100(N)	30'	45'(N)	MAS(N) MAS(N)	SV-1-T(N)	SP-1-T(N)	12'(N)	137(N)	-	-	-	
⑤	PPB POST(N)	5'-4"	-	-	-	-	-	-	1	S	4	
⑥	PPB POST(N)	5'-4"	-	-	-	-	-	-	1	E	2	
⑦	1-A	10'	-	-	TV-2-T(N)	SP-1-T(N)	-	-	-	-	-	
⑧	24-4-70	30'	35'	MAT MAS	SV-1-T	SP-1-T	12'	137	1	N	4	
⑨	1-A	10'	-	-	TV-1	SP-1-T	-	-	1	E	6	
⑩	PPB POST	5'-4"	-	-	-	-	-	-	1	E	6	
⑪	17-1-70	30'	20'	MAS	SV-2-TA	SP-1-T	-	-	-	-	-	

ALL EQUIPMENT IS EXISTING UNLESS OTHERWISE NOTED.
(N) = INDICATES NEW EQUIPMENT



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CITY OF MANHATTAN BEACH

PUBLIC WORKS DEPARTMENT - ENGINEERING DIVISION

OFFSITE TRAFFIC SIGNAL MODIFICATIONS
SEPULVEDA BOULEVARD & 33RD STREET

REVISIONS		
NO.	BY	DATE

REFERENCES		

CHECKED BY CITY ENGINEER J. SCHMIDT DATE 12/18/20	APPROVED BY DIRECTOR OF PUBLIC WORKS DATE 1-13-2021
DESIGNED BY J. SCHMIDT DATE 12/18/20	DRAWING NO. P-906
DRAWN BY J. SCHMIDT DATE 12/18/20	SHEET 7 OF 7